



NOTICE OF PUBLIC MEETING
CITY OF ALBANY
CITY COUNCIL
Council Chambers
 333 Broadalbin Street SW
 Wednesday, January 13, 2016
 7:15 p.m.

OUR MISSION IS
"Providing quality public services for a better Albany community."

OUR VISION IS
"A vital and diversified community that promotes a high quality of life, great neighborhoods, balanced economic growth, and quality public services."

AGENDA

Rules of Conduct for Public Meetings

1. No person shall be disorderly, abusive, or disruptive of the orderly conduct of the meeting.
2. Persons shall not testify without first receiving recognition from the presiding officer and stating their full name and residence address.
3. No person shall present irrelevant, immaterial, or repetitious testimony or evidence.
4. There shall be no audience demonstrations such as applause, cheering, display of signs, or other conduct disruptive of the meeting.

1. CALL TO ORDER
2. PLEDGE OF ALLEGIANCE TO THE FLAG
3. ROLL CALL
4. SPECIAL PRESENTATION

a. Fire Department Civilian Awards. [verbal]
 Action: _____

5. SCHEDULED BUSINESS

a. Communications

- 1) Accepting Mark Azevedo's resignation from the City Tree Commission. [Page 3]

Action: _____

- 2) Accepting Chuck Leland's resignation from the Albany Arts Commission. [Page 4]

Action: _____

- 3) Albany Industry Partnership Update. [Page 5]

Action: _____

b. Quasi-Judicial Public Hearings

- 1) VC-02-15, CU-07-15, and RL-10-15; development of a new Albany Police Station. [Pages 6-127]

Action: _____ ORD. NO. _____

- 2) VC-03-15, CU-08-15, and RL-11-15; development of a new Albany Fire Station. [Pages 128-262]

Action: _____ ORD. NO. _____

c. Business from the Public

d. Second Reading of Ordinance

- 1) Amending Albany Municipal Code 2.04.060, Voting. [Pages 263-264]

Action: _____ ORD. NO. _____

e. First Reading of Ordinance

- 1) Amending Albany Municipal Code 2.25, Albany Human Relations Commission. [Pages 265-267]

Action: _____ ORD. NO. _____

f. Adoption of Resolution

- 1) Approving an Intergovernmental Agreement with Lebanon and Tangent Fire Districts for a shared medic unit. [Pages 268-270]

Action: _____ RES. NO. _____

g. Adoption of Consent Calendar

1) Approval of Minutes

- a) October 14, 2015, Regular Session minutes. [Pages 271-275]
- b) October 26, 2015, Work Session minutes. [Pages 276-278]
- c) October 28, 2015, Regular Session minutes. [Pages 279-281]
- d) November 2, 2015, 4:00 p.m., Work Session minutes. [Pages 282-283]
- e) November 2, 2015, 7:15 p.m., Work Session minutes. [Pages 284-286]
- f) November 9, 2015, Work Session minutes. [Pages 287-294]

- 2) Approving application and acceptance of the FY 2015-16 State Hazardous Materials Emergency Preparedness Grant on behalf of the Mid-Valley Local Emergency Planning Committee. [Pages 295-296]
RES. NO. _____

Action: _____

h. Award of Contract

- 1) Approving a contract with InterCommunity Health Network and appropriating funds for a one-year pilot Community Paramedic Program. [Pages 297-299]

Action: _____ RES. NO. _____

i. Award of Bid

- 1) MS-16-01, Dave Clark Riverfront Trail Lighting. [Pages 300-302]

Action: _____

j. Appointments

- 1) City Boards, Commission, and Committees. [Pages 303-323]

Action: _____

- 2) Council Audit Committee. [Pages 324-327]

Action: _____

- 3) Hospital Facility Authority. [Pages 328-330]

Action: _____

- 4) City Council representatives. [Page 331]

Action: _____

k. Report

- 1) Airport Advisory Commission discussion regarding revenue from the sale of the Armory property. [Pages 332-333]

Action: _____

6. BUSINESS FROM THE COUNCIL

7. NEXT MEETING DATE: Work Session: January 25, 2016
Regular Session: January 27, 2016

8. ADJOURNMENT

Ed,

This letter is to inform you that effective immediately, I will no longer be able to serve as a member of the Albany Tree Commission. This resignation has not been taken lightly and is in no way a reflection of any discontent towards the City or any issues related to the Commission. As you are aware, my time and energy for the last year or more has been consumed by extended family and work related obligations. After 8 years on the Commission, I think that it is time for me to step back from my community outreach activities so that I do not burn out and loose the excitement and joy that I feel for volunteer community involvement.

I have thoroughly enjoyed getting to know you and your staff, particularly the brainstorming sessions to create a vision for the management and appreciation of Albany's unique and special natural resources.

I am proud of the accomplishments that were made during my tenure on the Commission, especially in bringing public awareness for and commitment to best management practices to Albany's natural resources and in our efforts to provide positive and meaningful experiences to our youth through the Sawing for Schools and WIN internship programs. None of this could have been accomplished without the involvement and dedication of you, your staff, the wood shop teachers at West and South and the many, many citizens in the community who believe that together we CAN build a better community.

Thank you.

Best,

Mark Azevedo

Charles Leland
330 S W. 8th Avenue
Albany, Oregon 97321

October 17, 2015

Sharon Konopa – Mayor
333 Broadalbin SW
City of Albany, Oregon 97321

Dear Sharon:

As we talked on the 15th, I no longer can maintain my appointment to the Albany Art Commission. I find commitments outside of Albany now takes up more time than I believed. That plus my age has made me re consider my position and unfortunately I must cut back on my volunteer work. Effective December 31, 2015 I resign my appointment.

I have enjoyed my time on the commission and especially knowing and working with all of the capable, friendly and caring people I have come in contact with over these past years. Please convey my sentiments to Ed and the other commissioners.

Sincerely,



Charles J. Leland

Cc: Ed Hodney ✓

Linda Herd



TO: Albany City Council
VIA: Wes Hare, City Manager
FROM: Kate Porsche, Economic Development & Urban Renewal Director
DATE: December 3, 2015, for the January 13, 2016, City Council Meeting
SUBJECT: Albany Industry Partnership Update

RELATES TO STRATEGIC PLAN THEME: ● Goal III – A Healthy Economy

RELATES TO:

- Prosperity: – Maintain and grow the income levels with a focus on living-wage jobs, training and education opportunities of Albany residents, consistent with Oregon and national trends. Work to increase the community's assessed value while working to achieve a healthy balance of housing and jobs.

Action Requested:

None.

Discussion:

In the spring, Council approved the request for funding by the Albany Industries for workforce education. Since that time, the contract has been executed and the business partners have been working in consultation with LBCC to get the program underway.

Since it has been a little while since the partnership was approved, I thought it would be a good idea to have our partners come back and report to you the status of the partnership. Jim Merryman, President and COO of OFD Foods, will be coming before you to give an update on where things are at, the ordering of equipment, and the status of the LBCC programs.

We look forward to seeing you then.

KP:hr

G:\Economic Development\PROJECTS\LBCC Partnership\Funding Request and Meetings\LBCC City Council update memo 2016.01.13.docx



TO: Albany City Council
VIA: Wes Hare, City Manager
Jeff Blaine, P.E., Public Works Engineering and Community Development Director *JB*
FROM: Bob Richardson, Planning Manager *BAR*
David Martineau, Project Planner *DM*
DATE: January 6, 2016, for the January 13, 2016, City Council Meeting
SUBJECT: Public Hearing (Planning Files VC-02-15, CU-07-15, and RL-10-15)

RELATES TO STRATEGIC PLAN THEME:

- A Safe City
- Great Neighborhoods
- Healthy Economy

Action Requested:

Staff recommends that the City Council approve with conditions the proposed land use applications to construct a new Albany Police Station as presented in the attached staff report, and adopt an ordinance vacating portions of Willetta Street and the Cathey Creek drainage right-of-way as proposed by the applicant and described in the ordinance exhibits.

Discussion:

During the January 13, 2016, City Council meeting, the Council will hold a public hearing on a proposal to construct a new 40,367 square-foot police station at 2600 Pacific Boulevard SW. As described in detail in the attached staff report to the Planning Commission, the following three land use applications are associated with the subject proposal.

1. A Street Vacation for a portion of Willetta Street and a portion of the Cathey Creek drainage right-of-way;
2. A Conditional Use Review for a Community Service land use (police station) in the Office Professional (OP) and Residential Single Family (RS-5) zone; and,
3. A Replat to remove property lines and combine two parcels, plus the vacated portions of Willetta Street and the Cathey Creek drainage right-of-way into one parcel.

The applications have been processed concurrently through the Type IV application review process in accordance with ADC 1.200(3). This process is required for Vacation proposals and includes review and a recommendation by the Planning Commission prior to a final local decision made by the City Council. This process also provides the most opportunity for public participation in the decision-making process.

Following the Type IV process and procedures outlined in Oregon Revised Statutes Chapter 271, on July 8, 2015, the City Council initiated proceedings to vacate a portion of Willetta Street, and on July 22, 2015, the Council initiated proceedings to vacate a portion of the Cathey Creek drainage right-of-way. On November 16, 2015, the Planning Commission held a duly advertised public hearing and voted 7-0 to recommend that the City Council approve with conditions, the land use applications noted above.

Budget Impact:

None

DM:rk
Attachments (3)

ORDINANCE NO. _____

AN ORDINANCE VACATING A PORTION OF THE WILLETTA STREET RIGHT-OF-WAY NORTH OF 29TH AVENUE, AND A PORTION OF THE CATHEY CREEK DRAINAGE RIGHT-OF-WAY WEST OF PACIFIC BOULEVARD, IN ALBANY, OREGON; AND ADOPTING FINDINGS.

WHEREAS, the Albany City Council initiated proceedings to vacate a portion of Willetta Street on July 8, 2015, and the Albany City Council initiated proceedings to vacate a portion of the Cathey Creek drainage right-of-way on July 22, 2015; and

WHEREAS, the vacated street right-of-way will revert to the property immediately west of the Willetta Street SW right-of-way, and the southern half of the Cathey Creek drainage right-of-way will revert to the properties to the south; and

WHEREAS, notices of public hearing were mailed on November 2, 2015; published in the *Albany Democrat-Herald* on November 2, 2015, and November 9, 2015; and posted on the site, as described in Section 1 below, on October 28, 2015 as required by state and local law; and

WHEREAS, the Albany Planning Commission held a public hearing on November 16, 2015, and recommended that the City Council approve the proposed street and right-of-way vacation; and

WHEREAS, the Albany City Council held a public hearing on January 13, 2016, reviewed the testimony presented at the public hearing and the findings in the staff report, and deliberated on the vacation requests.

NOW, THEREFORE, THE PEOPLE OF THE CITY OF ALBANY DO ORDAIN AS FOLLOWS:

Section 1: Subject Property. A portion of the Willetta Street right-of-way north of 29th Avenue and a portion of the Cathey Creek drainage right-of-way west of Pacific Boulevard (File VC-02-15) (see legal descriptions on attached Exhibit A and map Exhibit B), are hereby vacated.

Section 2: Findings. The Findings, Conclusions, and Conditions in the Staff Report concerning VC-02-15 are hereby adopted in support of this decision as presented in Exhibit C of this Ordinance.

Section 3: Easement Retained. The City retains a public utility easement and storm drainage easement over the areas being vacated.

Section 4: The City Recorder shall, within 10 days of the effective date of this ordinance, file a certified copy of the ordinance with the Linn County Clerk, Linn County Assessor, and Linn County Surveyor (ORS 271.150). The petitioner for the vacation shall bear the recording costs.

Passed by the Council: _____

Approved by the Mayor: _____

Effective Date: _____

Mayor

ATTEST:

City Clerk

**Legal Descriptions
VC-02-15
Right-of-Way Vacations**

Vacation Area #1

The northern portion of the Willetta Street right-of-way that was created by deeds recorded in Linn County, Oregon Deed Records Book 339, Page 355 and Book 340, Page 617, more particularly described as follows:

Beginning at the Southeast corner of Lot 10 of Winfield Way subdivision, a subdivision of record located in the NE ¼ of Section 13, T11S, R4W, W.M., said Southeast Corner of Lot 10 also lying on the west right-of-way line of Willetta Street; thence North 0°26'00" East 343.76 feet along the west right-of-way line of Willetta Street to the northeast corner of Lot 10, said northeast corner also lying on the south right-of-way line of Cathey Creek, a right-of-way recorded in Linn County, Oregon deed records Book 159, Page 174; thence east 50.01 feet along the south right-of-way line of the Cathey Creek right-of-way to the northwest corner of Parcel 1 of Linn County, Oregon, Partition Plat No. 2008-01, said corner also lying on the east right-of-way line Willetta Street; thence South 0°26'00" West 343.76 feet along the east right-of-way line of Willetta Street to a point 8.03 feet north of the southwest corner of said Parcel 1; thence West 50.00 feet to the point of beginning, as shown on attached Exhibit B.

Vacation Area #2

The southern 20 feet of the eastern 453.11 feet of the drainage right-of-way created by a deed recorded in Linn County, Oregon deed records Book 159, Page 174, more particularly described as follows:

Beginning at the Northeast corner of Parcel 1 of Linn County, Oregon, Partition Plat No. 2008-01; thence south 89°23' west 293.19 along the north property line of said Parcel 1 to the northwest corner of said Parcel 1; thence continuing south 89°23' west 50.01 feet to the northeast corner of Lot 10 of Winfield Way subdivision; thence continuing south 89°23' west 109.91 feet along the north property line of said Lot 10 to the northwest corner of Lot 10; thence north 0°37' west 20 feet to the centerline of the Cathey Creek right-of-way; thence north 89°23' east 453.11 feet parallel with and 20 feet north of the south right-of-way line of Cathey Creek right-of-way to a point on the west right-of-way line of Pacific Highway, said point lying North 0°37' west 20 feet from the northeast corner of Parcel 1; thence south 0°37' east 20 feet along the west right-of-way line of Pacific Highway to the point of beginning, as shown on attached Exhibit B.

EXHIBIT B

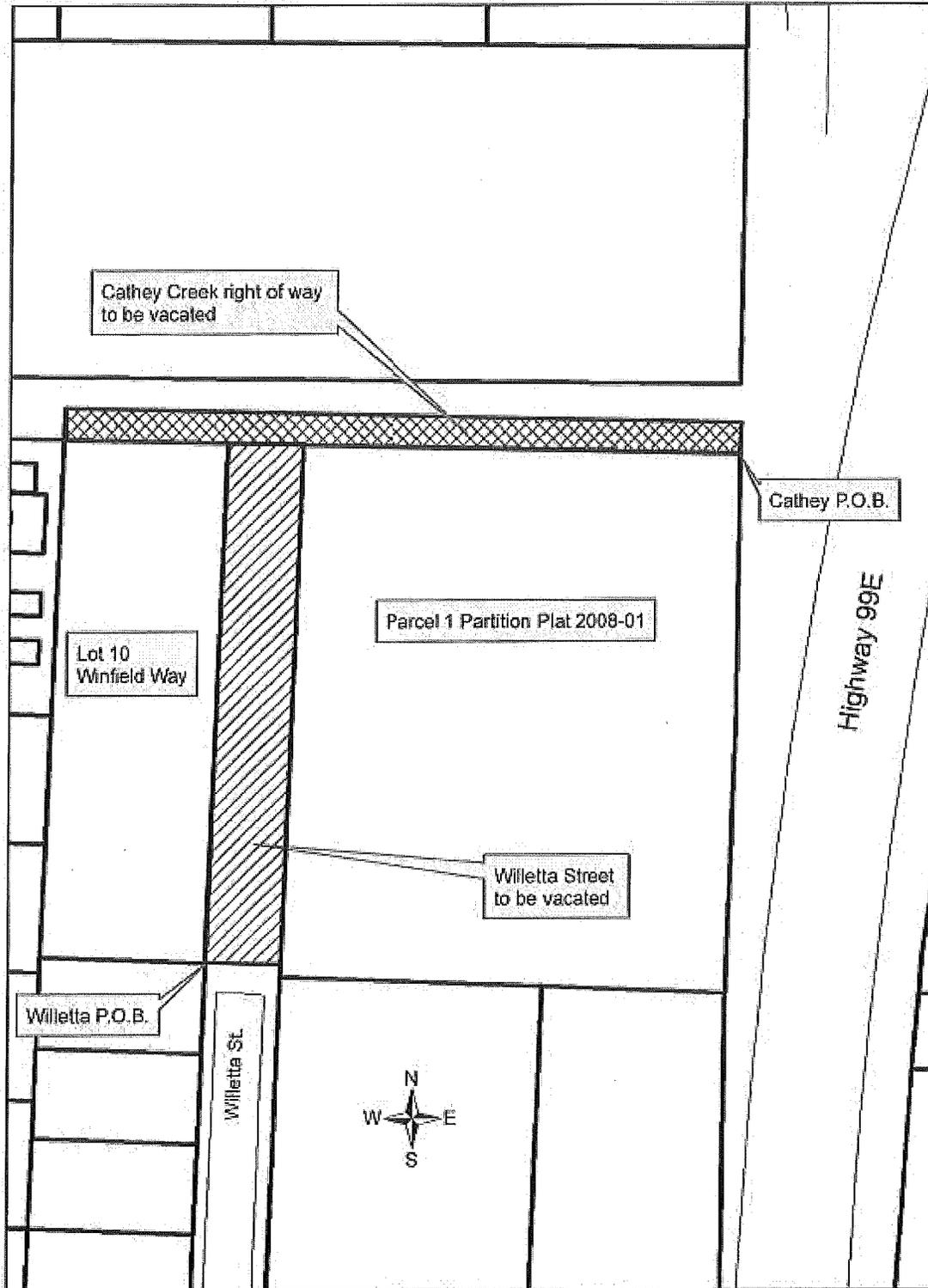


EXHIBIT C

See Staff Report:

Albany Police Station
Street Vacation, Conditional Use Review and Replat
Planning Files VC-02-15, CU-07-15, and RL-10-15

Dated November 9, 2015

CITY OF ALBANY
PLANNING COMMISSION
Municipal Court Room, 333 Broadalbin Street SW
Monday, November 16, 2015
5:15 p.m.

MINUTES

Planning Commissioners present: Linsey Godwin (arrived at 5:20 p.m.), Sue Goodman, Roger Phillips, Cordell Post, Larry Tomlin, Dala Rouse, Dan Sullivan

Planning Commissioners absent: Wendy Ezell (excused), one vacancy

Staff present: Bob Richardson, Planning Manager; David Martineau, Lead Planner; Melissa Anderson, Planner II; Jim Delapoer, City Attorney; Staci Belcastro, City Engineer; Ron Irish, Transportation Systems Analyst; Jeff Blaine, Public Works Engineering and Community Development Director; John Bradner, Fire Chief; Mario Lattanzio, Police Chief; Marilyn Smith, Public Information Officer; Rachel Kutschera, Administrative Assistant I

CALL TO ORDER

Chair Tomlin called the meeting to order at 5:15 p.m.

COMMENTS FROM THE PUBLIC

None

QUASI-JUDICIAL PUBLIC HEARING #1

Development of a New Albany Police Station (VC-02-15, CU-07-15, and RL-10-15):

- 1) Street Vacation of the abutting portion of Willetta Street SW where it terminates at the site, plus the south half of the Cathey Creek right-of-way abutting the site;
- 2) Replat to combine two parcels plus the vacated portions of Willetta Street and the Cathey Creek drainage right-of-way into one parcel under the same ownership
- 3) Conditional Use Review for new construction of a 40,367 square-foot police headquarters; and

Chair Tomlin opened the public hearing at 5:18 p.m.

Declarations by the Commission:

Conflicts of Interest: None
Ex Parte Contact: None
Site Visit: None
Abstain: None
Challenges: None

Staff Report:

Lead Planner David Martineau provided a summary of the case. Staff-proposed conditions of approval were included for the Commission's consideration (see Agenda Packet).

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Questions from the Commission:

Commissioner Rouse asked how high fences are allowed to be on residential property. Planner Martineau said they are typically allowed to be six feet high, and four feet in front yard setbacks. Commissioner Rouse asked why an eight-foot fence is allowed on this property. Richardson said staff would look in the development Code and respond.

Commissioner Phillips asked if a fence would be built at a setback from the creek. Martineau said the fence is proposed to be about five feet north of the developed portion of the property where the parking lot is. The creek is not a riparian corridor or protected at this location; it serves as more of a drainage ditch, and there are no plans to build a fence there at this time.

Commissioner Goodman asked if there were any seismic conditions applied to the police and fire stations; Martineau said those conditions are in the building code and will be addressed during that phase of development.

Commissioner Phillips then asked about building height limits at this location. Martineau said the Code allows building height exceptions for public buildings as long as it is declared in the notice of public hearing beforehand, and in this case, it was.

Commissioner Rouse asked if parking lots are allowed on residentially zoned property, and Martineau said they are allowed when tied to an approved use, accomplished via a replat or a mutual agreement. Rouse asked if the parking lot design meets the commercial or residential landscaping criteria, and Martineau said it meets the stricter section of the code, requiring a 25-foot setback. Rouse said in the past, residential zones were not allowed to have parking lots, so why isn't a zone change proposed for this case? Martineau said a zone change isn't needed since approved uses do allow parking lots; Planning Manager Richardson said the use is permitted in the residential zone as long as a Conditional Use Permit is obtained. The proposed police station use is a single property, single owner, and single use. Rouse asked if the code allows a parking lot in a residential zone with a conditional use permit. Richardson said parking is permitted with the use as long as the Conditional Use application is approved.

Applicant Testimony:

Mario Lattanzio, Police Chief for the City of Albany, 1117 Jackson St. SE. Chief Lattanzio thanked the Commission for their time. He summarized the police station history and current status, noting that it was originally built when the population of Albany was much smaller than today. They have made many changes to the building to make it work for the current environment, but there is an overcrowding issue. The property for the new station was purchased by the City in 2009, and the City has worked to determine the correct size, location, and funding sources for the building to convince the public that it is needed. He provided justification for the proposed site and building size, and addressed questions previously asked by the public, such as access on Willetta Street and potential traffic volumes.

Commissioner Rouse commented that she was on City Council when the current station was built, which was better than the one before, and she hopes the new one will be even better than the current one.

Testimony in Favor:

Helene Becker, 1100 30th Pl. SW. Ms. Becker shared her concerns with safety and traffic issues, access from Pacific Boulevard, and periodic flooding on the proposed site. She was told at a previous meeting that traffic would be monitored and reported back to her as to how it would be dealt with, but she isn't sure that has been done. There are also a lot of walkers in the area; disabled people, middle-schoolers, and the cross-country team. She wonders why an access wasn't considered from the current road in front of the nearby industrial building.

Linn Benton also has a lot of traffic during the school year. She feels no information was given to address the traffic issues.

Regarding the flooding, she is glad there will not be a fence to back up water and cause more flooding. The field where the station will be has been used to hold floodwaters, and she is concerned where that water will go if a building is there.

She is also concerned about building setbacks in the neighborhood and light from the building or vehicles shining into homes. She was wondering if the building could be rotated 90 degrees so the majority of the light would not face homes, but at the industrial plaza instead.

She appreciates the maps and explanation of how far back the building would be built, but she was told at a previous meeting that a siren test is performed by the police, and she would prefer it not to face the neighbors.

Commissioner Rouse asked how high Cathey Creek gets in the winter; Ms. Becker didn't know, but does know it goes over the bank into the field where the building is proposed. She said residences constructed along Willetta were going to be multi-family, but due to flooding and the capacity of the sewer system, the homes were designed to be single-family instead. She said the current system doesn't adequately drain the water. Rouse asked to clarify if she meant storm drain, and Ms. Baker said yes, she meant the storm water drain.

Testimony in Opposition:

Dean Williams, 1008 30th Pl. SW. Mr. Williams is against the Conditional Use Permit application because he doesn't believe it satisfies the review criterion that says the transportation system can support the proposed use. He said the development would cause undue congestion on Umatilla, Willetta, and 28th Avenue. Like Mrs. Becker, he was also under the impression that a traffic study was going to be conducted, but hasn't seen one. It appears the majority of the parking lot is directly accessed from Willetta, causing congestion on that street. He said there are adequate sidewalks in some areas, but not on Umatilla or 28th, which is used heavily by students, bicyclists, and walkers. If a secondary access is truly needed, the one from 24th Street would be a safer route, leaving the neighborhood as it is now.

Neutral Testimony: None

Rebuttal Testimony:

Brian Varricchione, Mackenzie, 1515 SE Water Ave. Portland. Mr. Varricchione would like to assure the public and the Planning Commission that the City of Albany has a very robust permit and plan review process where all the issues raised will be reviewed by staff, such as the storm drainage design and plan. Preliminary stormwater treatment plans have been entered into the record.

Regarding traffic impacts, he noted that the access on Pacific will be the primary access, and on the site plans a safe place to stop and look for pedestrians has been designed. Access on Willetta is secondary access only. He noted that there is more control over use of the site because it will be used primarily by City staff. The applications meet all code requirements.

Rouse asked what the soil types are on the property and why it floods. Mr. Varricchione said he wasn't familiar with the soil types, but project staff have surveyed the site, and his civil engineers have incorporated the results into their design. Rouse asked if the exits near Shoen Electric are too close to the site, and Mr. Varricchione said a certain separation between driveways is required and has been met.

Staff Response:

Ron Irish, Transportation Systems Analyst for the City of Albany. Irish said at the time the neighborhood meeting was held, the number of trips generated by the proposed police station was unknown. The applicant submitted a trip generation analysis with the application, which is referenced in the application. It would generate 31 to 41 peak hour trips. He said if development were allowed under the current office-professional zoning, three to four times more traffic would be generated than for the current proposal. A traffic study was not conducted because the peak hour trip estimated didn't require it. He explained how access to and from Pacific and Willetta would be accomplished. They had looked at access via the industrial park, but there are ODOT and Elm Street right-of-ways there, and wetland mitigation to cross Cathey Creek would be required. Willetta has a better situation for signals, right of ways, parking, and access, and is actually one of the wider residential streets in town.

Regarding lack of sidewalks in the area, those streets without them do need sidewalks, as do many other areas in town, but this level of development doesn't require sidewalks. Ultimately, responsibility for sidewalks in those neighborhoods falls to the property owners there.

Phillips noted that the City did a good job of assessing possible traffic impacts. Rouse asked how traffic would be affected if Stone Forest would be developed across the street (a vacant warehouse for sale across the road). Irish said the study looked at parcels not currently in use and an optimized location for the proposed use. Driveways are proposed according to that analysis. If Stone Forest is redeveloped, those access points may be changed based on future analysis.

Procedural questions: None

Chair Tomlin closed the hearing at 6:19 p.m.

Discussion and Decision:

MOTION: Commissioner Post moved to recommend that the City Council approve the applications as proposed by the applicant. Commissioner Sullivan seconded. A vote was taken and the motion passed 7-0.

QUASI-JUDICIAL PUBLIC HEARING #2

Development of a New Albany Fire Station (VC-03-15, CU-08-15, and RL-11-15):

- 1) Street Vacation for the westernmost 194.12 feet of 6th Avenue, east of Lyon Street;
- 2) Conditional Use Review for a Community Service land use (Fire Station); and,
- 3) Replat to remove property lines and combine six parcels into one

Chair Tomlin opened the public hearing at 6:27 p.m.

Declarations by the Commission:

Conflicts of Interest:	None
Ex Parte Contact:	None
Site Visit:	None
Abstain:	None
Challenges:	None

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Staff Report:

Planner Melissa Anderson provided a summary of the case. Staff-proposed Conditions of Approval were included for the Commission's consideration (see Agenda Packet).

Questions from the Commission: None

Applicant Testimony:

John Bradner, Fire Chief for the City of Albany, 110 SE 6th Ave. Mr. Bradner thanked the Commission, and summarized the current status of the Fire Station, which is past the end of its life span and needs to be replaced. There are seismic, space, and other issues present that have been discussed over the past several years. The last large earthquake in 1993 didn't cause extensive damage but did highlight the need to upgrade the building in case another earthquake occurs. A seismic evaluation was conducted in 2002 and presented to the City in 2003, but no funding was identified to address the issues. In 2009, a committee of department members and the public was put together to start looking for new sites for the Fire Station. It was narrowed down to the current site including acquiring some neighboring properties. He summarized the current status of the station and land compared to what is needed, which led to the current proposal and the process of purchasing neighboring properties. Outreach to neighbors is ongoing to mitigate concerns. The Fire Department will continue to be a good neighbor, and maintain compatibility with the downtown core.

Commissioner Rouse asked if soil and groundwater testing would be done on the former Richard's Cleaners site. Bradner said extensive testing has already been conducted and they are working with the DEQ (Department of Environmental Quality).

Rouse asked how future expansion needs would be accommodated. Bradner said there is space built into the design of the new building for extra staff, but as the City grows, additional stations would likely be located at different sites to accommodate response times. Current fire department staff at City Hall will be moved to the new station when it is complete.

Commissioner Sullivan asked how building around Hasty Freeze and traffic would be accommodated. Bradner said access would be separated from the Station, and access from Lyon Street to the station would not be provided.

Testimony in Favor: None

Testimony in Opposition: None

Neutral Testimony: None

Rebuttal Testimony: None

Staff Response: None

Procedural questions: None

Chair Tomlin closed the hearing at 6:55 p.m.

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Discussion and Decision:

MOTION: Commissioner Rouse moved to recommend that the City Council approve the applications as proposed by the applicant. Commissioner Goodman seconded. A vote was taken and the motion passed 7-0.

APPROVAL OF MINUTES

No minutes were ready to approve.

ACTIVITY UPDATE

No updates were provided.

NEXT PLANNING COMMISSION MEETING DATE

No future meetings are scheduled at this time.

ADJOURN

Hearing no further business, Chair Tomlin adjourned the meeting at 6:58 p.m.

Respectfully submitted,

Signature on File

Rachel Kutschera
Administrative Assistant

Reviewed by,

Signature on File

Bob Richardson
Planning Manager



Community Development Department

333 Broadalbin Street SW, P.O. Box 490
Albany, OR 97321

Phone: 541-917-7550 Facsimile: 541-917-7598
www.cityofalbany.net

STAFF REPORT

ALBANY POLICE STATION

Street Vacation, Conditional Use, and Replat Planning Files VC-02-15, CU-07-15, and RL-10-15

<u>HEARING BODY</u>	PLANNING COMMISSION	CITY COUNCIL
<u>HEARING DATE</u>	Monday, November 16, 2015	Wednesday, January 13, 2016
<u>HEARING TIME</u>	5:15 p.m.	7:15 p.m.
<u>HEARING LOCATION</u>	Albany City Hall Municipal Court Room, 333 Broadalbin Street SW	

SUMMARY

This report reviews and evaluates a consolidated Street Vacation, Conditional Use, and Replat application submitted by Mackenzie on behalf of the City of Albany Police Department, for conformance with the Albany Development Code (ADC). The property is located at 2600 Pacific Boulevard SW, and is currently vacant (Attachment A). The site is located within two different zoning districts. The east side of the property is zoned Office Professional (OP) and the west side is zoned Residential Single Family (RS-5). Police stations are classified as a "Community Service" use, according to ADC Section 22.190. Community service uses require conditional use approval in the RS-5 zone, and either site plan review or conditional use approval in the OP zone.

The development proposal is contingent upon the vacation of the abutting portion of Willetta Street SW where it terminates at the site, plus the south half of the Cathey Creek right-of-way abutting the site (Attachment B.1 – B.6). Oregon Revised Statute (ORS) Chapter 271 governs the process for vacating streets and right-of-ways within incorporated cities, and Albany Development Code (ADC) Section 2.630 lists review criteria, which must be met for the vacations to be approved. The Albany City Council agreed to initiate the vacation proceedings for Willetta Street on July 8, 2015, and the south half of Cathey Creek right-of-way on July 22, 2015, and directed staff to prepare applications for both vacations. Public utility easements will be retained where public utilities exist.

The Conditional Use application (file CU-07-15) is for new construction of a 40,367 square-foot police headquarters to serve the residents of Albany. The total site area is 163,272 square feet (3.75 acres). Of that, 15 percent will be building area, 52 percent will be paved, and 33 percent will be landscaped (Attachment D.9). Development will include a total of 165 on-site parking spaces, 133 of which will be dedicated to police station use in a secure area. Of the 32 parking spaces open to the public, four spaces will be accessible spaces. Bicycle parking will be provided near the public entrance. Site plans show a combination of landscaped buffer and screening to protect neighboring properties on all sides, including the residential uses to the south and west. The proposed building will be two stories, with a maximum height of 39 feet. The building will meet commercial and institutional design standards through the use of architectural features such as ground-level transparency, varying building materials featuring masonry veneer, structural masonry, and metal panel walls.

The replat application (file RL-10-15), if approved, will combine two parcels, plus the vacated portions of Willetta Street and the Cathey Creek drainage right-of-way, into one parcel under the same ownership (Attachment D.18). The proposed police station development will utilize the entire site and would also dedicate the proposed Willetta Street cul-de-sac bulb.

Staff Recommendation to the Planning Commission: **APPROVAL** with the **FOLLOWING CONDITIONS:**

Vacation

- 1.1 Public utility easements shall be retained over the areas where public utilities exist. The required easements shall be centered over the utility mains and must be 20 feet wide for sewer, and 15 feet wide for water. A public utility easement must be provided over the entire 20-foot wide area to be vacated along the Cathey Creek right-of-way.

Compatibility

- 2.1 **Prior to issuance of a certificate of occupancy**, a minimum of 101 parking spaces shall be provided. The public parking area shall include a minimum of 32 parking spaces, including parking for the disabled.
- 2.2 **Prior to issuance of a building permit**, a revised curb stop detail shall be submitted to the Community Development Department showing a 2½- to 3-foot bumper overhang on parking spaces where curb stops are provided.
- 2.3 A total of 12 bicycle parking spaces are required, 6 of which must be covered. Bicycle parking shall be provided which meets the standards for dimensional requirements in ADC 9.120(13)(g), and in the amount shown in accordance with submitted site plans prior to occupancy.
- 2.4 In accordance with ADC 3.390, the refuse disposal area must be screened from view by placement of a sight-obscuring fence, wall, or hedge at least six feet in height. The applicant shall provide the method of screening that will be used for the access gates on a detail drawing or on a revised site plan prior to building permit issuance and installed prior to occupancy.
- 2.5 **Prior to issuance of a building permit**, a revised landscape plan showing compliance with parking lot and planter bay standards will be required for review and approval by the Planning Division.
- 2.6 **Prior to issuance of a certificate of occupancy**, the applicant shall install buffering and screening in the locations and amounts shown on submitted landscape plans. Any changes to approved plans must be reviewed and approved by the Albany Planning Division.
- 2.7 **Prior to issuance of a certificate of occupancy**, the irrigation system and landscaping must be installed per the approved plans. Any changes to approved plans must be reviewed and approved by the Albany Planning Division. ADC 9.190 allows occupancy of a development prior to the complete installation of all required landscaping and irrigation only if occupancy is requested between December 1 and March 1 and other requirements are met.

Utilities

- 4.1 **Prior to issuance of building permits** for the proposed project, the applicant shall construct an 18-inch public storm drainage main through the site, generally as shown on the applicant's preliminary utility plan. Public utility easements shall be provided over these facilities as required in the City's Standard Construction Specifications and Design Standards documents.
- 4.2 **Prior to issuance of building permits** for the proposed project, the applicant must obtain a Storm Water Quality permit from the City's Engineering Department, and the permit must be approved. Alternatively, the applicant may provide financial assurances for the required public infrastructure in order to obtain building permits for the project.
- 4.3 **Prior to issuance of an occupancy permit** for the proposed development, the applicant shall construct the required storm water quality facilities, generally as shown on the applicant's preliminary utility plan.

Fire Services

- 4.4 Civil site plan submittals for all future building and planning permit approvals shall show fire protection details as required on the "Fire Site Plan Submittal Checklist" located on the City of Albany's web site.
- 4.5 **Before the City will issue building permits**, plans showing the location and placement of fire access routes and hydrants serving the development shall be reviewed and approved by the City of Albany Fire Marshal. Adequate water supply must be available on site prior to construction, or as otherwise approved by the Fire Marshal.

Design Standards

DS 1. **Before the City approves occupancy of the building**, a minimum of three pedestrian amenities from the list in ADC 8.360(2) shall be installed.

You must comply with state, federal, and local law. The issuance of this permit by the City of Albany does not eliminate the need for compliance with other federal, state, or local regulations. It is the applicant's responsibility to contact other federal, state, or local agencies or departments to assure compliance with all applicable regulations.

Expiration of Approval: Final approval is valid for three years from the date of final approval unless: (a) the applicant has installed all of the required public infrastructure related to the development and the infrastructure has been accepted by the city, or the applicant has provided financial assurance for all required public infrastructure per Section 12.600, or the first phase, if the development was approved for phased construction; or (b) A valid building permit exists for new construction or improvements, and work has commenced. If (a) or (b) has been done, then the project may continue to completion.

APPLICATION INFORMATION

DATE OF REPORT: November 9, 2015

STAFF REPORT PREPARED BY: David Martineau, Planner III

PROPERTY OWNER: City of Albany, 333 Broadalbin St. SE, Albany, OR 97321

APPLICANT: Police Chief Mario Lattanzio, City of Albany; PO Box 490, Albany, OR 97321

PROJECT ARCHITECT: Jeff Humphreys, Mackenzie, 1515 SE Water Avenue, Suite 100, Portland, Oregon 97214

PROJECT PLANNER: Brian Varricchione, Mackenzie, 1515 SE Water Avenue, Suite 100, Portland, Oregon 97214

ADDRESS: 2600 Pacific Boulevard SW

MAP/TAX LOT: Linn County Assessor's Map No. 11S-04W-13AB; Tax Lots 6900 and 7003

ZONING: Office Professional (OP) and Residential Single Family (RS-5)

TOTAL LAND AREA: 3.75 acres, following replat to combine property and right-of-way dedication the for cul-de-sac bulb

EXISTING LAND USE: Undeveloped land

NEIGHBORHOOD: West Albany

SURROUNDING ZONING: North: Office Professional (OP)
South: OP and Residential Single Family (RS-6.5)
East: Light Industrial (LI) across Pacific Boulevard SW
West: RS-6.5 and Residential Medium Density (RM)

SURROUNDING USES: North: Cathey Creek ditch; automobile dealership and parts supply
South: Medical and professional offices east of Willetta Street; single family residences west of Willetta Street
East: Vacant Light Industrial-zoned property
West: Six-unit condominium development; single family residences

PRIOR HISTORY: PA-06-02: Land Division (Partition) to create three lots from one lot was approved and recorded January 3, 2003. Two of the resultant lots were developed with medical and professional offices. The remainder, Parcel 1, is the easterly parcel (Tax Lot 7003), presently zoned OP.

M1-01-90: Preliminary subdivision (Winfield Way Subdivision) to create 10 lots. The westerly parcel (Tax Lot 6900) was Lot 10, with an area of 37,687 square feet.

Files also contain several other cases associated with the westerly property: CU-06-79, M1-17-76, PD-05-76, ZC-08-76, and ZC-08-78.

NOTICE INFORMATION

A Notice of Public Hearing was posted in the Albany Democrat-Herald on November 2, 2015 and November 9, 2015, in accordance with Oregon Revised Statute (ORS) 271.110 for right-of-way vacations. On November 2, 2015, a Notice of Public Hearing was mailed to property owners within 1,000 feet of the subject properties together with persons who attended the August 13, 2015 neighborhood meeting in accordance with ADC 1.360. The site was posted on October 28, 2015, in accordance with Section 1.410 of the Albany Development Code. As of November 3, 2015, no written comments were received.

APPEAL

Within five days of final action on this application, the Community Development Director will provide written notice of the decision to the applicant and any other parties entitled to notice.

If the Planning Commission's recommendation is for approval of this application, the application will be forwarded to the City Council for a public hearing and decision.

If the Planning Commission's decision is to deny this application, the applicant may appeal the decision to the City Council by submitting a completed Notice to Appeal application and the associated filing fee to the Planning Division within 10 days from the date the City mails the Notice of Decision.

STAFF ANALYSIS

Vacation File VC-02-15

Section 2.630 of the Albany Development Code (ADC) contains the following review criteria, which must be met for this application to be approved. Code criteria are written in *bold italics* and are followed by findings, conclusions, and conditions where conditions are necessary to meet the review criteria.

- (1) *The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan city transportation or public facility plan.*

FINDINGS OF FACT

- 1.1 The following Comprehensive Plan policies have been identified as relevant to this review criterion:

GOAL 11: Public Facilities and Services

- a. Prohibit the construction of structures over public water lines and easements.
 - b. Prohibit the construction of structures over drainage improvements and easements.
 - c. Prohibit the construction of structures over public wastewater lines and easements.
- 1.2 Sanitary Sewer. Sanitary sewer utility maps indicate that an 8-inch public sanitary sewer main lies within the southernmost 35 feet of the portion of the Willetta Street right-of-way proposed for vacation.
- 1.3 Water. Water utility maps indicate that a 24-inch public water main lies along the entire length of the portion of the Willetta Street right-of-way proposed for vacation.
- 1.4 Storm Drainage. Storm drainage utility maps indicate that Cathey Creek in this area lies within an approximately 40-foot wide right-of-way.
- 1.5 Easements. Public utility easements over sanitary sewer mains must be at least 20 feet wide and centered over the main. Public utility easements over water mains must be at least 15 feet wide and centered over the main. A public utility easement must be provided over the entire 20-foot wide right-of-way to be vacated along Cathay Creek.
- 1.6 Transportation. Neither of these right-of-ways appear in the Albany Transportation System Plan (TSP).

CONCLUSIONS

- 1.1 City utilities (sanitary sewer, water, and storm drainage) exist within the areas proposed for vacation.
- 1.2 Public utility easements will be needed over the existing public sewer and water mains, and storm drainage facilities (Cathey Creek) that lie within the areas to be vacated. These easements will provide access rights to the utilities for maintenance, and will prohibit the construction of permanent structures over the utilities.
- 1.3 Retaining public utility easements over the existing public utilities within the areas to be vacated will protect the utilities from having structures built over the utilities, and will provide for access to the utilities.
- 1.4 Vacating these right-of-ways is not inconsistent with the Albany Transportation System Plan.

CONDITION

- 1.1 Public utility easements shall be retained over the areas where public utilities exist. The required easements shall be centered over the utility mains and shall be 20 feet wide for sewer, and 15 feet wide for water. A public utility easement shall be provided over the entire 20-foot wide area to be vacated along the Cathay Creek right-of-way.
- (2) *The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.*

FINDINGS OF FACT

- 2.1 Existing public utilities currently exist within the areas to be vacated. The existing public rights-of-way currently provide access to these utilities.
- 2.2 Vacating the rights-of-way as proposed would result in public utilities that lie within private property.
- 2.3 In order to maintain access rights to these public utilities, public utility easements are needed.
- 2.4 The section of Willetta Street being sought for vacation has never been constructed and never been open to traffic. All existing properties in the area have access to an existing improved street. The section of Cathey Creek is a drainage facility, not a public street, and it does not provide access to any properties.

CONCLUSIONS

- 2.1 Retaining public utility easements over the existing public utilities within the areas to be vacated will protect the utilities from having structures built over the utilities, and will provide for access to the utilities.
- 2.2 All adjacent properties currently have access to public streets. Vacating these right-of-ways will not have a negative impact on any property's access to public streets.

CONDITION

- 2.1 See Condition 1.1 above.
- (3) *The requested vacation will not have a negative effect on traffic circulation or emergency service protection.*

FINDING OF FACT

- 3.1 The right-of-ways proposed to be vacated do not contain any constructed roadways.

CONCLUSION

- 3.1 The requested vacation will not have a negative effect on traffic circulation or emergency vehicle access.
- (4) *The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.*

FINDINGS OF FACT

- 4.1 The portion of Willetta Street being vacated equals approximately 17,190 square feet, and the portion of Cathey Creek drainage right-of-way being vacated is approximately 9,060 square feet.
- 4.2 The right-of-ways will be converted into a portion of the new City of Albany Police Station. The Police Station will be required to meet Albany Development Code standards.

CONCLUSION

- 4.1 The portions of right-of-ways being vacated that are proposed for development must comply with Albany Development Code requirements; as such the above criterion is satisfied.

- (5) *The public interest, present and future, will be best served by approval of the proposed vacation.*

FINDING OF FACT

- 5.1 The vacated right-of-ways will be converted into a portion of the new City of Albany Police Station.

CONCLUSION

- 5.1 It is in the public interest to have a new police station. Approving these right-of-way vacations will allow the police station to be completed.

STAFF ANALYSIS

Conditional Use File CU-07-15

Section 2.250 of the ADC contains the following review criteria, which must be met for this application to be approved. Code criteria are written in *bold italics* and are followed by findings, conclusions and conditions where needed to meet the criteria.

Criterion (1) The proposed use is consistent with the intended character of the base zone and the operating characteristics of the neighborhood.

FINDINGS OF FACT

- 1.1 Proposed use. The proposed use for the property is a new police headquarters to serve the residents of Albany. The original plans called for a 41,749 square-foot building; however, the building size was reduced to 40,367 square feet at the request of the Police Department, as explained in correspondence from Mackenzie dated October 14, 2015 (Attachment C.1). The total site area is 163,272 square feet (3.75 acres). Of that, 15 percent will be building area, 52 percent will be paved, and 33 percent will be landscaped. Development will include a total of 165 on-site parking spaces, 133 of which will be dedicated to police station use in a secure area. Of the 32 parking spaces open to the public, four spaces will be accessible spaces. Bicycle parking will be provided near the public entrance. The applicant proposes extensive landscaping within the parking lots and fronting Pacific Boulevard, and a vegetated buffer and an 8-foot CMU-block wall in the western portion of the site to separate the development from adjoining residences. The building itself is proposed to have a brick and metal façade with a generous amount of windows to be an inviting and attractive addition to the Pacific Boulevard streetscape (Attachments D.1 – D.2).
- 1.2 Intended character of the RS-5 zoning district. The western third of the property (about 1.22 acres) is zoned RS.5 – Single Family Residential (see Attachment D.7). ADC 3.020 states that the RS-5 district is intended primarily for low- to moderate-density single-family residential development. The average minimum lot size for single-family detached dwelling units is 5,000 square feet. Front yard setbacks are 15 feet; interior setbacks vary depending on use. The maximum building height is 30 feet and the maximum lot coverage is 60%. All yards adjacent to streets must be fully landscaped. Community service uses are considered through the Conditional Use process in the RS-5 zone.
- 1.3 Intended character of the OP zoning district. The eastern two-thirds of the property (about 2.52 acres) is zoned OP – Office Professional (see Attachment D.7). The OP zoning district is intended to provide a vertical or horizontal mix of professional offices, personal services, live-work, residential and limited related commercial uses in close proximity to residential and commercial districts. The limited uses allowed in this district are selected for their compatibility with residential uses and the desired character of the neighborhood. The OP zone is typically appropriate along arterial or collector streets as a transitional or buffer zone between residential and more intense commercial or industrial districts. Community service uses, such as public safety facilities, may be considered through the Conditional Use process in the OP zone.

- 1.4 Operating Characteristics of the Neighborhood. The 3.75-acre site is bordered by Cathey Creek drainage ditch to the north, Pacific Boulevard and Light Industrial-zoned properties to the east, medical and professional offices to the south, single-family residences to the southwest and residential condominiums to the west. A total of three single-family dwellings and six single-family attached (condominium) dwellings directly abut the property to the south and west. Lot sizes for nearby single-family residential uses average about 8,000 square feet. Vehicle trips generally are related to work, school, recreation and shopping activities. Residential properties also have private outdoor spaces on the sides, front and rear portions of the property that are used for recreation, landscaping, storage buildings, and parking. Abutting residences have direct access to Umatilla Street or Willetta Street. The Light Industrial zoned property east of the site across Pacific Boulevard is presently vacant. A commercial warehouse is located on property further to the south. The medical and professional offices to the south are served by two points of access off Willetta Street and one point of access from 29th Avenue.
- 1.5 Operating Characteristics of the Proposed Police Headquarters. According to the applicant, the new police station will be a 24-hour operation in a building containing office and public safety institutional uses. The site will have driveways, parking, lighting, utilities, and landscaping. Site plans show a combination of landscaped buffer and screening to protect neighboring properties on all sides, including the residential uses to the south and west. The proposed building will be two stories, with a maximum height of 39 feet. The building will meet commercial and institutional design standards through the use of architectural features such as ground-level transparency, varying building materials featuring masonry veneer, structural masonry and metal panel walls (Attachments D.1 – D.6).
- 1.6 Public Notification and Participation. A neighborhood meeting was conducted by the applicant at 6:00 p.m. on Thursday, August 13, 2015, at the Albany First United Methodist Church, 1115 28th Avenue SW. Notice of the neighborhood meeting was sent at least ten days in advance to a total of 150 property owners within 1,000 feet of the proposed development. The neighborhood meeting was attended by 52 people, including two people from Mackenzie, and nine City representatives. A summary of the meeting together with neighbor's concerns and applicant responses is attached to this staff report (Attachments C.49 – C.57).
- 1.7 Conditional Uses. According to ADC 2.230, "Certain uses are conditional uses instead of being allowed outright, although they may have beneficial effects and serve important public interests. They are subject to the conditional use regulations because they may have significant adverse effects on the environment, overburden public services, change the desired character of an area, or create major nuisances. A review of these proposed uses is necessary due to the potential individual or cumulative impacts they may have on the surrounding area or neighborhood. The conditional use process provides an opportunity to allow the use when there are minimal impacts, to allow the use but impose conditions to address identified concerns, or to deny the use if the concerns cannot be resolved."

CONCLUSIONS

- 1.1 The proposed use is a new City of Albany police headquarters facility on a 3.75-acre site.
- 1.2 The use is allowed conditionally in both the RS-5 and the OP zoning districts.
- 1.3 The proposed police station site is bordered by a drainage ditch to the north, industrial properties to the east, professional offices to the south and single family detached and single family attached dwellings to the south and west.
- 1.4 As described above, the proposed use is consistent with the intended character of the base zone and the operating characteristics of the neighborhood because of its location along an arterial street, the use of structures and vegetation to buffer the site from adjacent uses, and as discussed later in this report, its compliance with ADC Article 8 – Design standards. Conditions of approval will be proposed in

subsequent sections of this report that will ensure the facility will be consistent with the intended character of the base zones.

Criterion (2) The proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, setbacks, and landscaping or the proposal calls for mitigation of differences in appearance or scale through such means as setbacks, screening, landscaping or other design features.

FINDINGS OF FACT

- 2.1 Definition of Compatible. “Compatible” does not mean “the same.” *Merriam Webster’s Collegiate Dictionary*, Eleventh Edition, defines “compatible” as “(1) capable of existing together in harmony.”
- 2.2 Existing and Anticipated Uses. The property is currently undeveloped. The proposed use is a new 40,367 square-foot police headquarters building together with 32 public parking spaces, 133 secure area parking spaces, and landscaping. The building will be two stories. It will be oriented toward Pacific Boulevard and the public parking area, as opposed to the residentially zoned lots to the south and west. The applicant proposes extensive landscaping within the parking lots and fronting Pacific Boulevard and a vegetated buffer in the western portion of the site to separate the development from adjoining residences.
- 2.3 Scale of Single-Family Homes. One single family home abuts the subject property to the south (2620 Willetta Street SW). It is two stories in height and approximately 2,263 square feet in size. It is zoned Residential Single Family (RS-6.5), which has a maximum building height of 30 feet. Two single family residences and six single family attached condominiums abut the subject property to the west and are served by access to Umatilla Street. The homes at 2629 and 2619 Umatilla Street SW are single story, approximately 1,300 square feet in size, and zoned RS-6.5. The condominiums at 2601 Umatilla Street SW range in area from 1,100 to 1,500 square feet in size. Three of the units are one story, and the other three are two stories. The condominium property is zoned Residential Medium Density (RM). The maximum allowable height in the RM district is 45 feet.
- 2.4 Scale of the Proposed Police Station. The police station will be completely built within the OP-zoned portion of property. ADC Section 4.090, Table 4-2, shows that the maximum building height allowed in the OP zoning district is 30 feet. The proposed two-story facility will be 39 feet. ADC 4.240(2) contains a building height exception that states where churches and certain public and semi-public buildings require Conditional Use approval, the height restrictions may be waived as a part of the Conditional Use proceedings, provided that a request for such has been noted in the public hearing notice. The proposed 39-foot building height was noted in the public hearing notice. The footprint of the building is 23,391 square feet. The applicant states that because the site is in a transition area between land use zones, there is no established scale or standards to mimic. The proposal is for a two-story structure designed to a human scale with facades incorporating recesses and off-sets and surrounded by landscaping. A large portion of the lot will be used as parking for police vehicles and official use, all screened by landscaping. Structures on property abutting residential districts and/or uses require 1 foot of setback for each foot of finished wall height with a minimum setback of 10 feet, according to ADC 4.090(5). Additionally, Section 4.210 of the ADC states that any new construction of a school, church, or public or semi-public building must be set back at least 25 feet from any property line adjoining or directly across the public right-of-way from any residential district. Minimum required building setbacks will be met as shown in Table 1 below:

Table 1: Required and Proposed Building Setbacks

Yard	Minimum Required Setback - Public Buildings	Proposed Building Setback
North (interior)	5'	54.8'
East (front)	10'	93.8'
South (interior)	5'	45.9'
West (interior) abutting RS-5 zone	25'	85.3'

Based on setbacks being provided, the use, as proposed, fits within the context of existing abutting single-family development.

- 2.5 Intensity and Lot Coverage of the Proposed Development. The maximum lot coverage in the OP district is 70%; in the RS-5 district it is 60%. Lot coverage includes buildings, structures, pavement, and other areas not vegetated or in a naturally permeable state. According to the applicant, the proposed development covers exactly 70% in the portion of property zoned OP, and 59.8% of the property zoned RS-5. All of the 40,367 square-foot building will be located on the portion of property zoned OP. Approximately 60% of the secured parking area with associated traffic aisles will be located on the RS-5-zoned property, while the remaining 40% of secured parking, traffic aisles, and all of the public parking will be constructed on the OP side.

The square footage for all parking and access areas will be 31,597 square feet for the RS-5 side, and 53,413 square feet for the OP side, totaling 85,010 square feet (1.95 acres). The building footprint is 23,391 square feet. Together, buildings and paved areas add up to 108,401 square feet (2.49 acres). This amounts to a total lot coverage of 66.4% of the 3.75-acre combined parcel. The development, as proposed, meets lot coverage standards.

- 2.6 Architectural Style of Proposed Police Station. According to the applicant, the building is proposed to have a brick and metal façade with a generous amount of windows to be an inviting and attractive addition to the Pacific Boulevard streetscape. The east and south elevations, most visible from public streets, will feature masonry veneer, structural masonry, and metal panel walls as well as window and storefront systems. The front of the building (facing Pacific Boulevard) will provide pedestrian-level transparency with an aluminum-framed storefront system, with transparency of approximately 546 square feet or 35.5% of the 1,537 square-foot pedestrian-level area. The upper floor windows mirror the style and locations of those on the first floor to the extent possible. Figures submitted with the application reveal a color palette consisting of silver metallic metal panels, frames and coping with “blue thunder” accents. Structural concrete masonry units (CMUs) are a speckled gray pattern called “midnight,” and sill bricks will be “Redondo grey” (Attachments D.1 – D.6).

- 2.7 Vehicle Parking – Required Number. Table 9-1 of ADC Section 9.020 does not identify the minimum number of spaces required for police stations. ADC 9.020(4) allows the Director to approve alternative parking standards when the use is not specifically listed in the Code, or when the applicant produces documentation that demonstrates a different parking demand. The applicant provided parking standards in cities of comparable size to Albany (see Table 1 in Attachment C.21). St. Louis Park, MN; Bloomington, IN; and Racine, WI all have parking standards specifically for police stations. As shown, their ratios vary widely from 1 per 1,000 square feet to 4 per 1,000 square-foot of floor area (1 per 250 square feet). The applicant also provided standards for similar uses from four Oregon cities with similar populations per the 2014 city population estimates produced by the Population Research Center at Portland State University (Bend, Corvallis, Keizer, and Tigard). Parking for government/public safety services uses in the selected Oregon cities ranges from 1 space per 333 square feet to 1 per 500 square feet. The applicant proposes a standard of one (1) parking space per 400 square feet of gross floor area, which equates to a minimum of 101 spaces. The proposed site plan would provide 32 parking spaces for public/visitor use and 69 secure spaces for police parking, totaling 101 spaces. The remainder of the secure area will be used for impound and storage. Staff finds this proposal acceptable.

- 2.8 Parking Lot Design and Construction. Parking lots must be paved, landscaped, and provided with approved drainage. ADC 9.120(3) states that all areas of a parking lot shall have a durable, dust-free surface of asphalt, cement concrete, or other materials approved by the Director of Public Works. ADC 9.130, Table 1, includes dimensional standards for parking lots. The ADC requires a stall depth of 18.5 feet for spaces that are 9 feet wide and angled at 90 degrees. A 3-foot parking overhang with wheel bumpers or 7.5-foot sidewalk must be provided at the front of each space fronting a sidewalk, and can be subtracted to determine actual stall depth. Dead-end back up areas shown on the site plan are at least 5 feet deep. The applicant’s pre-cast curb stop detail (see Exhibit 10 of Attachment D.12) shows wheel

stops located at a minimum of 18 inches from the sidewalk edge, in places where provisions for a 2½ to 3-foot parking overhang would otherwise be needed. This detail must be revised to reflect the Code requirement.

Parking areas shall be divided into bays of not more than 12 parking spaces, according to ADC Section 9.150(1). Parking spaces shown on the site plan (Attachment D.9) are at least 9.0 feet wide by 15.5 to 16.5 feet deep, with a 3-foot bumper overhangs except where they face a 7.5-foot wide sidewalk with extruded 6-inch curb. The width of the two-way travel aisles throughout the property is a minimum of 26 feet, except a travel aisle providing access to law enforcement vehicle parking just west of the building is 24 feet in width. The applicant proposes bays of not more than 12 parking spaces together with landscaped planter bays in the public parking lot conforming to minimum Code requirements. Planter bays are provided in the secure parking area as well.

- 2.9 Loading Standards. The proposed police station is an institutional office use, so it does not require a designated loading facility. Any deliveries to the site can be accommodated via the front entrance area or within the confines of the sally port, which are designed for police vehicles to pull through in an east to west direction.
- 2.10 Bicycle Parking. ADC 9.120(13)(a) states that commercial or office development must provide at least two (2) bicycle parking spaces, plus one space for every 10 automobile spaces required. ADC 9.120(13)(h) says at least one-half of required bicycle parking spaces must be sheltered. Under this standard, 12 bicycle spaces, six (6) that are covered, would be required for the development. Site Plans show that a total of 22 bicycle parking spaces are being provided, 18 of which are going to be covered. Four of the covered spaces will be located near the front entrance to the building underneath the entryway canopy designed for public use. A detail drawing that shows that the dimensional requirements listed in ADC 9.120(g) can be met is required for review and approval by the Planning Division prior to occupancy.
- 2.11 Lighting. ADC Section 9.120(14) requires that any lights provided to illuminate any public or private parking area must be arranged to reflect the light away from any abutting or adjacent residential district. According to the applicant, all overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors. Light pollution will be mitigated by utilizing appropriate shielding on light fixtures and planting trees along the borders of the parking lots. The applicant provided a photometric plan listing light fixtures proposed around the site (Attachment D.13). Based on the plan, staff finds that the proposed exterior lighting will not result in glare upon the public right-of-way or beyond the facility's property line.
- 2.12 Outside Storage. The proposed refuse disposal area will be located on a portion of police department property that lies in the RS-5 zone. The minimum front setback in the RS-5 zone is 15 feet. ADC Section 3.390 states that any refuse container or refuse disposal area which would otherwise be visible from a public street, customer or resident parking area, any public facility, or any residential area, shall be screened from view by placement of a sight-obscuring fence, wall or hedge at least 6 feet in height. All refuse materials shall be contained within the screened area. Refuse disposal areas may not be located in required setbacks or buffer yards and must be placed at least 15 feet from any dwelling window. The applicant submitted a revised detail on October 30, 2015 showing the location of the refuse disposal area 15 feet back from the property line that will be accessed from the northwest side of the cul-de-sac bulb (Attachment D.14). The chosen location is more than 60 feet away from the nearest single family residential property line and will be screened by a six-foot sight-obscuring masonry wall. The applicant did not provide the method of screening that will be used for the access gates. This will be required as a condition of approval.
- 2.13 Buffering and Screening. ADC Section 9.210 requires buffering and screening in order to reduce the impacts on adjacent uses which are of a different type, buffering and screening is required in accordance with a matrix contained within the section. Commercial developments require 10 feet of buffering and

screening when the abutting uses are single-family dwellings. In addition, parking lots containing at least five (5) spaces require 10 feet of buffering and screening from abutting single-family uses. Commercial developments abutting arterial streets require 10 feet of buffering, but not screening. Commercial developments do not require buffering or screening when the use abuts other commercial or light industrial uses or zones. Buffering and screening, therefore, is required along the property lines to the south and west where the proposed police station site development borders residential uses.

According to ADC 9.240, the minimum improvements within a buffer area consist of the following:

- (1) At least one row of trees. These trees will be not less than 10 feet high at time of planting for deciduous trees and spaced not more than 30 feet apart and 5 feet high at time of planting for evergreen trees and spaced not more than 15 feet apart. This requirement may be waived by the Director when it can be demonstrated that such trees would conflict with other purposes of this Code (e.g. solar access).
- (2) At least five, 5-gallon shrubs or ten 1-gallon shrubs for each 1,000 square feet of required buffer area.
- (3) The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, evergreen shrubs).

According to ADC 9.250, where screening is required or provided, the following standards apply in addition to conditions (1) and (3) above:

- (1) One row of evergreen shrubs that will grow to form a continuous hedge at least 4 feet tall within two years of planting, or
- (2) A fence or masonry wall at least 5 feet tall constructed to provide a uniform sight-obscuring screen, or
- (3) An earth berm combined with evergreen plantings or a fence that forms a sight and noise buffer at least 6 feet tall within two years of installation.

A portion of property to the south is zoned RS-6.5, and properties to the west are zoned RS-6.5 and RM. These properties are characterized by residential uses. The applicant's plans (see Attachment D.16) show one row of parking lot trees along the north and south lot lines. Perimeters of two rows of trees will be provided on the eastern lot line and three rows of trees will be provided on the western lot line. In addition, an 8-foot high fence (or CMU block wall) will be provided around the site boundary on the north, west, and southwestern lot lines, and a continuous evergreen hedge will surround the parking area. More than five, 5-gallon shrubs will be provided for each 1,000 square feet of buffer area. All other landscape areas will be covered in ground cover.

2.14 Landscaping. All front setbacks (exclusive of accessways and other permitted intrusions) are required to be landscaped before an occupancy permit will be issued or final building permit approved. In all commercial districts, the minimum landscaping acceptable for every 30 lineal feet of street frontage (or portion thereof, deducting the width of the driveway) is:

- (a) One tree at least six feet tall.
- (b) Four 1-gallon shrubs or accent plants.
- (c) The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, and evergreen shrubs).

The front setback of the subject property abuts Pacific Boulevard and is located in the OP zoning district. It is about 377 feet in length; therefore, 13 trees are required. According to landscaping plans

(Attachment D.16), the applicant proposes to plant about 26 trees in the front setback area. The required setback area, including the 5-foot interior setback and 10-foot front setback areas, is 6,725 square feet. Thus, the minimum 5-gallon shrubs required are 5 per 1,000 square feet, or 34. As shown in the attached plans, upwards of 100 5-gallon shrubs will be planted in the required setback area, and the remaining areas of the proposed improvements will be treated with ground cover. Therefore, this standard is met.

2.15 Parking Lot Landscaping. ADC 9.150 requires the following:

- (1) Planter Bays. Parking areas shall be divided into bays of not more than 12 parking spaces. Between or at the end of each parking bay there shall be curbed planters at least 5 feet wide. Each planter shall contain one tree at least 10 feet high and decorative groundcover containing at least two shrubs for every 100 square feet of landscape area.
- (2) Entryway Landscaping. Entryways into parking lots shall be bordered by a minimum 5-foot wide landscape planter strip meeting the same landscaping provisions as for planter bays, except that no sight-obscuring trees or shrubs are permitted.
- (3) Parking Space Buffers. Parking areas shall be separated from the exterior wall of a structure by pedestrian entranceways or loading areas or by a 5-foot strip of landscaping materials.

The parking lot entrances are bordered by wide planter strips and will have compliant plantings and ground cover. Due to minor changes in the project site plans since the original submittal (e.g. refuse container location and parking space refinements); a revised landscape plan will be required for review and approval by the Planning Division prior to issuance of a building permit.

2.16 Landscaping and Vision Clearance Areas. No trees, shrubs, fences, or signs may be located within any vision clearance area, which prohibits structures, or planting that would impede visibility between the heights of two feet and eight feet. A clear vision area consists of a triangular area, two sides of which are lot lines or a driveway, and the third side of which is a line across the corner of the lot joining the non-intersecting ends of the two sides. For this use, the measurement along the drive edge and commercial district driveways shall be 20 feet, in accordance with ADC 12.180(2). Landscape plans submitted with the application show adherence to these standards for both driveways (Attachment D.16).

2.17 Irrigation. ADC 9.160 requires that all required landscape areas be provided with a piped underground irrigation system, unless a licensed landscape architect or certified nurseryman submits written verification that the proposed plant materials do not require irrigation. Irrigation systems installed in the public right-of-way require an encroachment permit. Irrigation plans were provided with this submittal, and show all landscaped areas on site being irrigated using rotary spray nozzles. Said landscaping and irrigation (if applicable) shall be installed and approved prior to issuance of an occupancy permit. Any changes to approved plans must be reviewed and approved by the Albany Planning Division. The applicant's submittal includes an irrigation plan that appears acceptable (Attachment D.17).

2.18 ADC 9.200 requires that all required landscaped areas must be maintained in an attractive manner free of weeds and noxious vegetation. The minimum amount of required living landscape materials shall be maintained.

2.19 Design Standards. See review of Design Standards for Commercial and Institutional Development following this Conditional Use section in the staff report below.

CONCLUSIONS

- 2.1 Due to the nature of the use, the new police station will be larger in size and scale than surrounding single-family residential dwellings; however, the size of the lot and the orientation of the development toward the east side of the property facing Pacific Boulevard adequately mitigates size and scale. Architectural style, building setbacks, landscaping and screening will be used to enhance compatibility with surrounding residential areas as required to satisfy criterion 2, above.

- 2.2 The proposed development will comply with lot coverage and setback standards of the OP and RS-5 zoning district. The two-story building will be 39 feet in height. According to the Development Code, building height restrictions may be waived for public buildings requiring conditional use approval.
- 2.3 The applicant provided parking standards from other cities that demonstrate a parking demand for police stations and public safety facilities. Based on those standards, 101 parking spaces would be required.
- 2.4 The curb stop detail on the applicant's site plans needs to be revised to reflect dimensions that provide a 2½- to 3-foot bumper overhang where curb stops are provided.
- 2.5 Planter bays meeting minimum Development Code standards are required throughout both parking lots.
- 2.6 The proposed development requires 12 bicycle parking spaces. The applicant intends to provide 22 bicycle parking spaces, 18 of which will be covered.
- 2.7 Proposed landscaping, buffering and screening onsite meets or exceeds Development Code requirements.
- 2.8 Any adverse impacts associated with the use of the property can be mitigated through such means as increased setbacks, buffering and screening and landscaping. Required landscaping and irrigation will be installed prior to issuance of an occupancy permit.
- 2.9 Based on the observations above, the proposed development will be compatible with existing or anticipated uses in terms of size, building style, intensity, setbacks, and landscaping when the following conditions are met:

CONDITIONS

- 2.1 **Prior to issuance of a certificate of occupancy**, a minimum of 101 parking spaces shall be provided. The public parking area shall include a minimum of 32 parking spaces, including parking for the disabled.
- 2.2 **Prior to issuance of a building permit**, a revised curb stop detail shall be submitted to the Community Development Department showing a 2½- to 3-foot bumper overhang on parking spaces where curb stops are provided.
- 2.3 A total of 12 bicycle parking spaces are required, 6 of which must be covered. Bicycle parking shall be provided which meets the standards for dimensional requirements in ADC 9.120(13)(g), and in the amount shown in accordance with submitted site plans prior to occupancy.
- 2.4 In accordance with ADC 3.390, the refuse disposal area must be screened from view by placement of a sight-obscuring fence, wall, or hedge at least six feet in height. The applicant shall provide the method of screening that will be used for the access gates on a detail drawing or on a revised site plan prior to building permit issuance and installed prior to occupancy.
- 2.5 **Prior to issuance of a building permit**, a revised landscape plan showing compliance with parking lot and planter bay standards will be required for review and approval by the Planning Division.
- 2.6 **Prior to issuance of a certificate of occupancy**, the applicant shall install buffering and screening in the locations and amounts shown on submitted landscape plans. Any changes to approved plans must be reviewed and approved by the Albany Planning Division.
- 2.7 **Prior to issuance of a certificate of occupancy**, the irrigation system and landscaping must be installed per the approved plans. Any changes to approved plans must be reviewed and approved by the Albany Planning Division. ADC 9.190 allows occupancy of a development prior to the complete installation of

all required landscaping and irrigation only if occupancy is requested between December 1 and March 1 and other requirements are met.

Criterion (3) The transportation system is capable of supporting the proposed use in addition to the existing uses in the area. Evaluation factors include street capacity and level of service, on-street parking impacts, access requirements, neighborhood impacts and pedestrian safety.

FINDINGS OF FACT

- 3.1 The proposed development will construct a 40,367 square foot Police Station. The project is located on the west side of Pacific Boulevard on the south side of Cathey Creek.
- 3.2 Access to the development will be provided by a direct driveway connection to Pacific Boulevard and from a proposed cul-de-sac bulb at the north end of Willetta Street.
- 3.3 Willetta Street is classified as local road and is constructed to city standards. Improvements include curb, gutter, and sidewalk; a vehicle travel lane in each direction; and on street parking.
- 3.4 Pacific Boulevard as a principal arterial road, is under the jurisdiction of ODOT, and is constructed to city standards. Improvements include curb, gutter, and sidewalk; two vehicle travel lanes in each direction; a two-way center left turn lane; and on street bike lanes.
- 3.5 The proposed development would result in the following changes to the street system:
 - *Construction of a new driveway approach to Pacific Boulevard approximately 145 feet south of the site's north property line.* The new driveway will be the facility's primary access connection for both APD staff and the general public. ODOT staff have reviewed the location of the driveway and indicated a willingness to issue the required access and construction permits.
 - *The dedication of public right of way and construction of a cul-de-sac bulb and two driveways on the south boundary of the site at the end of Willetta Street.* The construction of a cul-de-sac bulb is necessary in order to terminate the street with a turn around as required by ADC 12.190. One of the two driveways will provide the public with an option to use Willetta Street and 29th Avenue to access the site via the traffic signal at the Pacific Boulevard/29th Avenue intersection. Having that option will be particularly valuable for members of the public exiting the site to the south. The second driveway will provide secondary access to the secure parking and storage lot at the rear of the new facility and will generally be gated.
- 3.6 The design and location of proposed site driveways comply with the standards contained in ADC 12.100.
- 3.7 Albany's Traffic Impact Study Guidelines require submittal of a trip generation report for developments that will generate 50 or more new peak hour trips, and a full Traffic Impact Analysis for developments that will generate 100 or more new peak hour trips.
- 3.8 A trip generation study was submitted with the application. The study was performed by Mackenzie and is dated September 9, 2015. The study estimated that the new Police Station would generate 492 vehicle trips per day, of which 41 are expected to occur during the peak PM traffic hour.
- 3.9 Most trips to and from the site are expected to use the site's new driveway connection to Pacific Boulevard. APD staff will use the Pacific Boulevard driveway as a matter of routine, and that driveway will provide a more direct route for virtually all trips made by the public than would the site's public driveway at the end of Willetta Street.

- 3.10 Some visitors to the APD site that will be exiting via a left turn to the north will choose to go out of direction and use the Willetta Driveway in order to utilize the traffic signal at Pacific Boulevard/29th Avenue. Those trips are expected to constitute a minority of exiting movements from the site.
- 3.11 Albany's Transportation System Plan does not identify any capacity issues occurring along the frontage of the site.

CONCLUSIONS

- 3.1 The public streets that adjoin the development are currently constructed to city standards.
- 3.2 The development will add 492 week day vehicle trips to the street system. Of those, about 41 are expected to occur during the peak PM traffic hour.
- 3.3 The development will construct one new driveway to Pacific Boulevard along the frontage of the site. That driveway will be the primary access point for the site. Because Pacific Boulevard is part of the state highway system, ODOT will need to approve the construction of the new driveway.
- 3.4 The development will dedicate public right of way and construct a cul-de-sac bulb and two driveways on the south boundary of the site at the end of Willetta Street. Construction of the cul-de-sac bulb is necessary in order to terminate the street with a turn around as required by ADC 12.190.
- 3.5 The design and location of proposed site driveways comply with the standards contained in ADC 12.100.
- 3.6 The public street system can accommodate the proposed development.

CONDITIONS

- 3.1 The applicant shall obtain the approval and all necessary permits from ODOT prior to performing any work within Pacific Boulevard right of way.
- 3.2 Prior to issuance of an occupancy permit, the applicant shall dedicate right of way for and construct a cul-de-sac bulb and two driveways to city standards at the north end of Willetta Street as shown on the site plan.

Criterion (4) Public services for water, sanitary and storm sewer, water management and for fire and police protection are capable of servicing the proposed use.

FINDINGS OF FACT

Sanitary Sewer.

- 4.1 City utility maps show an 8-inch public sanitary sewer main in Willetta Street that terminates near the south boundary of the subject properties.
- 4.2 ADC 12.470 requires all new development to extend and/or connect to the public sanitary sewer system if the property is within 300 feet of a public sewer line.
- 4.3 The proposed new building will be served by a sewer service lateral that will be extended into the site from the public main near the south boundary of the site.
- 4.4 No adjacent properties exist that will require service access to this public main; therefore, no public sewer main extension will be required.

Water.

- 4.5 City utility maps show a 24-inch public water main in Willetta Street that runs through the site in a north-south direction, and a 12-inch main on the east side of Pacific Boulevard.
- 4.6 ADC 12.410 requires all new development to extend and/or connect to the public water system if the property is within 150 feet of an adequate public main.
- 4.7 The proposed new building will be served by a water service that will be connected to the public main running through the site.
- 4.8 Public utility easements of at least 15 feet in width, centered over the main, are required for all public water lines and appurtenances (ADC 12.370). Permanent structures are not allowed to encroach on a public utility easement, or be placed over a public water line (AMC 11.01.290).
- 4.9 The applicant is proposing to provide a public utility easement over the public main after the Willetta Street right-of-way is vacated.

Storm Drainage.

- 4.10 City utility maps show a 24-inch public storm drainage main in Pacific Boulevard, and Cathey Creek along the north boundary of the site. The facilities within the Pacific Boulevard right-of-way are under the jurisdiction of ODOT.
- 4.11 ADC 12.580 states that all new development within the City must, where appropriate, provide for the extension of existing storm sewer lines or drainageways serving surrounding areas. Extensions may be required along all frontages and/or through the interior of a property to be developed where the City Engineer determines that the extension is needed to provide service to upstream properties.
- 4.12 The applicant is required to submit a drainage plan, including support calculations, as defined in the City's Engineering Standards. The applicant is responsible for making provisions to control and/or convey storm drainage runoff originating from, and/or draining to, any proposed development in accordance with all City standards and policies as described in the City's Engineering Standards. In most circumstances, detention will be required unless it can be satisfactorily demonstrated by the applicant that there is no adverse impact.
- 4.13 The applicant's preliminary storm drainage plan shows the construction of an 18-inch public storm drainage main that will parallel the existing public water main that runs through the site. On-site detention will be provided as indicated in the applicant's storm drainage plan and as approved by the City's Public Works Department.
- 4.14 ADC 12.370 requires the dedication of public utility easements of at least 15 feet in width, centered over the main, for all public storm drain lines and appurtenances. Permanent structures are not allowed to encroach on a public utility easement, or be placed over a public storm drain line.
- 4.15 The applicant is proposing to provide a public utility easement over the new public storm drainage main that will run through the site.
- 4.16 ADC 12.540 states that open drainage ways, when approved by the City, must be provided with a public utility easement of sufficient width to allow for conveyance and maintenance.
- 4.17 The applicant is proposing to provide the required on-site stormwater quality facilities by constructing a number of stormwater quality ponds. A Storm Water Quality permit must be obtained by the applicant before this work is begun.

- 4.18 The City's Engineering staff has reviewed the applicant's preliminary storm water and storm water quality plans and has determined that they are generally acceptable.

Police Protection.

- 4.19 The proposed facility is the City's new police headquarters building. The building will be staffed 24 hours a day. The applicant notes that the site is within a well-connected location that allows for acceptable response time for fire and police protection in the event outside assistance is needed.

Fire Services.

- 4.20 According to the Albany Fire Marshal's Office, approved fire apparatus roadways must extend to within 150 feet of all exterior portions of the structure as measured by an approved route of travel around the exterior of the structure (OFC 503.1.1). Civil site plan submittals for all future building and planning permit approvals shall show fire protection details as required on the "Fire Site Plan Submittal Checklist" located on the City of Albany's web site.

- 4.21 Fire hydrants for this proposed project will be based on the following requirements:

- a. Fire hydrant location: All portions of buildings constructed or moved into the City shall be located within 400 feet (600 feet for fire sprinkler-protected buildings) of a fire hydrant located on a fire apparatus access road using an approved route of travel. (OFC 508.5.1)
- b. Required hydrants based on the required fire flow as calculated in accordance with OFC 503.7 and OFC Appendix B. The minimum number of fire hydrants is determined by OFC Table C105.1.
- c. Required fire hydrant spacing will be based upon required fire flows as determined by OFC Appendix C105.1 and Table C105.1. Please note that dead end roads require a reduced spacing.
- d. Fire hydrant spacing along new/required fire apparatus access roads. OFC Section C103.1 requires the placement of additional hydrants along all required fire access roads that are adjacent to any proposed building (and any future additions) and circulating through the property with spacing requirements per Appendix C 105.1. (See 2009 ICC Commentary, Appendix C-1, Section C103.1)

CONCLUSIONS

- 4.1 The proposed stormwater quality facilities can accommodate the proposed development.
- 4.2 With the construction of the proposed public storm drainage main through the site, the public utilities will be able to accommodate the proposed development.
- 4.3 While the City's Public Works Department has reviewed the applicant's preliminary utility plans, the final design details will be reviewed as part of the Permit for Private Construction of Public Utilities.
- 4.4 Public utilities, police and fire services can be provided to the proposed development when the following conditions of approval have been met.

CONDITIONS

- 4.1 **Prior to issuance of building permits** for the proposed project, the applicant shall construct an 18-inch public storm drainage main through the site, generally as shown on the applicant's preliminary utility plan. Public utility easements shall be provided over these facilities as required in the City's Standard Construction Specifications and Design Standards documents.
- 4.2 **Prior to issuance of building permits** for the proposed project, the applicant must obtain a Storm Water Quality permit from the City's Engineering Department, and the permit must be approved. Alternatively, the applicant may provide financial assurances for the required public infrastructure in order to obtain building permits for the project.
- 4.3 **Prior to issuance of an occupancy permit** for the proposed development, the applicant shall construct the required storm water quality facilities, generally as shown on the applicant's preliminary utility plan.
- 4.4 Civil site plan submittals for all future building and planning permit approvals shall show fire protection details as required on the "Fire Site Plan Submittal Checklist" located on the City of Albany's web site.
- 4.5 **Prior to issuance of building permits**, plans showing the location and placement of fire access routes and hydrants serving the development shall be reviewed and approved by the City of Albany Fire Marshal. An adequate water supply must be available on site prior to construction, or as otherwise approved by the Fire Marshal.

Criterion (5) The proposal will not have significant adverse impacts on the livability of nearby residentially zoned lands due to: (a) Noise, glare, odor, litter, and hours of operation; (b) Privacy and safety issues.

FINDINGS OF FACT

- 5.1 Noise, Odor, and Litter. The proposed use is for an office and institutional use for Albany Police, which will be a 24-hour operation. Due to the nature of the law enforcement use, no objectionable odors or litter is expected. Sirens may present an occasional noise impact. According to Police Chief Mario Lattanzio at the August 19, 2015 neighborhood meeting, police sirens will mainly be used when exiting onto Pacific Boulevard. Additionally, officers typically respond to calls while out on patrol, not from the station.
- 5.2 Glare. The applicant submitted a schematic lighting plan showing the types, numbers, and location of exterior lighting fixtures proposed with the project. Glare will be minimized through the use of appropriate house-side shielding on parking and building lighting fixtures, as shown in the photometric site plan, to ensure that exterior lighting will not result in glare upon the public right-of-way or beyond the facility's property line (Attachment D.13).
- 5.3 Hours of Operation. Police department staff confirmed that the front reception area is open Monday through Friday, from 7:30 a.m. to 6:30 p.m. Shift change for patrol officers is at 7:00 a.m. and 7:00 p.m. The Albany Police Department is a 24-hour operation.
- 5.4 Privacy and Safety Issues. The building will not create privacy or safety issues. The proposed site plan includes fencing and landscaping to protect the neighboring properties on all sides, including the residentially zoned neighbors to the south and west of the site.

CONCLUSIONS

- 5.1 No objectionable odors or litter is expected.
- 5.2 Sirens may cause an occasional noise impact; however, officers typically respond to calls while out on patrol, not from the station.

- 5.3 Glare will be controlled through shielding to ensure that exterior lighting will not result in glare upon the public right-of-way or beyond the facility's property line.
- 5.4 Privacy and safety issues for adjacent properties will be minimized through the use of landscaping and fencing.
- 5.5 This criterion is satisfied without conditions.

Criterion (6) Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.

FINDINGS OF FACT

- 6.1 Article 4: Airport Approach. The subject property is not located within the Airport Approach District.
- 6.2 Article 6: Floodplains. *Comprehensive Plan Plate 5: Floodplains*, does not show a 100-year floodplain on this property. FEMA/FIRM Community Panel No. 41043C-0526G, dated September 29, 2010, shows that this property is in Zone X, an area determined to be outside the 500-year floodplain.
- 6.3 Article 6: Natural Resource Districts. The site abuts a portion of Albany Canal/Cathey Creek, which is regulated by the Riparian Corridor (RC) overlay to the west of the site but not on the portion adjacent to the site. The RC buffer extends from the protected Oak Creek up Cathey Creek (southwest of the site) to the northeast diagonally across residential areas until it terminates just after crossing Liberty Street and 28th Avenue. The remaining segment serves as a storm drainage channel. The property also contains a number of wetlands, but they do not appear on the City's Significant Wetlands (SW) overlay. The subject property is in a part of town where wetland inventories were not studied. However, the City's wetlands consultant delineated wetlands on large swaths of the site (see Attachment D.8), requiring the City to mitigate the development of this area by contributing funds to a mitigation bank. Development can proceed in accordance with Oregon Department of State Lands/U.S. Army Corps of Engineers permitting and mitigation requirements.
- 6.4 Historic and Archaeological Resources. *Comprehensive Plan, Plate 9: Historic Districts*, shows the property is not located in a Historic District. There are no known archaeological sites on the property.

CONCLUSIONS

- 6.1 The subject property is not located within the Airport Approach district, 100-year floodplain, Natural Resource Overlay district or Historic district.
- 6.2 Wetlands have been identified on the site that will require mitigation in accordance with Oregon Department of State Lands/U.S. Army Corps of Engineers permitting and mitigation requirements.
- 6.3 There are no other special purpose districts to consider on this site. This criterion is met without conditions.

STAFF ANALYSIS

Design Standards

Commercial and Institutional Site Design (ADC 8.330-8.390)

In addition to the review criteria above, the following Design Standards must be met. **Note:** If there is a checked box symbol (☑) preceding a standard, it means that staff has compared the applicant's findings and plans to the standard(s) and find the standard(s) is met without comment. If the box is unchecked (☐), staff has provided

findings and conclusions as to the reason(s) why the standard is not met, and has added a condition. "NA" preceding the standard means it is not applicable to this particular development

Building Orientation (ADC 8.330). *Building orientation and maximum setback standards are established to help create an attractive streetscape and pleasant pedestrian environment.*

- (1) *New buildings shall be oriented to existing or new public streets. Building orientation is demonstrated by placing buildings and their public entrances close to streets so pedestrians have a direct and convenient route from the street sidewalk to building entrances.*
 - (a) *On sites smaller than three acres, new buildings shall be oriented to the public street/sidewalk and off-street parking shall be located to the side or rear of the building(s), except where it is not feasible due to limited or no street frontage, the site is an infill site less than one acre, conservation of natural resources, or where there are access restrictions.*
 - (b) *Buildings on sites larger than three acres may be setback from the public street and oriented to traffic aisles on private property, if the on-site circulation system is developed like a public street with pedestrian access, landscape strips and street trees.*
- (2) *Customer entrances should be clearly defined, highly visible, using features such as canopies, porticos, arcades, arches, wing walls, and planters.*

General Building Design (ADC 8.340). *New commercial buildings shall provide architectural relief and interest, with emphasis at building entrances and along sidewalks, to promote and enhance a comfortable pedestrian scale and orientation. Blank walls shall be avoided, except when not feasible.*

- (1) *Ground floor windows shall be provided along frontages adjacent to sidewalks. The main front elevation(s) of buildings shall provide windows or transparency at the pedestrian level in the following minimum proportions: OP District—25 percent transparency. The minimum window and door requirements are measured between 2 and 8 feet from the ground. Only the glass portion of doors may be used in the calculation.*
- (2) *Walls that are visible from a public street shall include a combination of architectural elements and features such as offsets, windows, entry treatments, wood siding, brick stucco, synthetic stucco, textured concrete block, textured concrete, and landscaping.*

Street Connectivity and Internal Circulation (ADC 8.350). *The standards apply to both public and private streets.*

- (1) *New commercial buildings may be required to provide street or driveway stubs and reciprocal access easements to promote efficient circulation between uses and properties, and to promote connectivity and dispersal of traffic.*
- (2) *The internal vehicle circulation system of a commercial development shall continue the adjacent public street pattern wherever possible and promote street connectivity. The vehicle circulation system shall mimic a traditional local street network and break the development into numerous smaller blocks.*
- (3) *Traffic aisles shall not be located between the building(s) and the sidewalk(s), except as provided in (4) below, or where drive-through windows are permitted, sites are constrained by natural resources, or are infill sites less than one acre.* [Ord. 5832, 4/9/14]

- (4) *Where drop off facilities are provided, they shall be designed to meet the requirements of the American with Disabilities Act but still provide for direct pedestrian circulation.*
- (5) *Internal roadways shall be designed to slow traffic speeds. This can be achieved by keeping road widths to a minimum, allowing parallel parking, and planting street trees to visually narrow the road. [Ord. 5445, 4/12/00]*

FINDINGS OF FACT: A traffic aisle will be located between the building and the public sidewalk along Pacific Boulevard. The subject property is larger than three acres. ADC 8.330(1)(b) states that buildings on sites larger than three acres may be setback from the public street and oriented to traffic aisles on private property, if the on-site circulation system is developed like a public street with pedestrian access, landscape strips and street trees. The applicant notes that the traffic aisle and the parking lot have been designed in the manner of a street with pedestrian zones and landscaping. Provided the traffic aisle, pedestrian walkways and parking lot are developed in accordance with submitted site and landscaping plans, street connectivity and internal circulation can be achieved.



Pedestrian Amenities (ADC 8.360).

- (1) *All new commercial structures and improvements to existing sites shall provide pedestrian amenities. The number of pedestrian amenities shall comply with the following sliding scale.*

<u>Size of Structure or Improvement</u>	<u>Number of Amenities</u>
<i>Less than 5,000 square feet</i>	<i>1</i>
<i>5,000 – 10,000 square feet</i>	<i>2</i>
<i>10,001 – 50,000 square feet</i>	<i>3</i>

- (2) *Acceptable pedestrian amenities include the following improvements. No more than two of any item may be used to fulfill the requirement:*
 - (a) *Sidewalks at least ten feet wide with ornamental treatments (e.g., brick pavers), or sidewalks which are 50 percent wider than required by the Code.*
 - (b) *Benches and public outdoors seating for at least four people.*
 - (c) *Sidewalk planter(s) enclosing a total of eight square feet.*
 - (d) *Pocket parks or decorative gardens (minimum usable area of 300 square feet).*
 - (e) *Plazas (minimum usable area of 300 square feet).*
 - (f) *Street trees that are 50 percent larger than required by the Code.*
 - (g) *Weather protection (awnings, etc.).*
 - (h) *Other pedestrian amenities that are not listed but are similar in scale and benefit.*
- (3) *Pedestrian amenities shall comply with the following standards:*
 - (a) *Amenities shall be located outside the building main entrance, along pedestrian corridors, or near transit stops. Amenities shall be visible and accessible to the general public from an improved public or private street. Access to pocket parks, plazas, and sidewalks must be provided via a public right-of-way or a public access easement.*
 - (b) *Amenities are not subject to setback requirements.*
 - (c) *Amenities are consistent with the character and scale of surrounding developments. For example, similarity in awning height, bench style, planter materials, street trees, and pavers is recommended to foster continuity in the design of pedestrian areas. Materials should be suitable for outdoor use, easily maintained, and have a reasonably long life cycle (e.g., 10 years before replacement).*

FINDINGS OF FACT: The size of the proposed police station will be 40,367 square feet; therefore, three pedestrian amenities are required (see ADC 8.360(1) above). The applicant's site plans show a public plaza, outdoor public seating (seat walls), a landscaped stormwater swale, and an entry canopy adjacent to the plaza (see Attachments D.15 and D.16). This standard is met.

- (4) *On sites at major transit stops provide the following:*
 - (a) *Either locate buildings within 20 feet of the transit stop, a transit street or an intersecting street or provide a pedestrian plaza at the transit stop or a street intersection.*
 - (b) *A reasonable direct pedestrian connection between the transit stop and building entrances on the site.*
 - (c) *A transit passenger landing pad accessible to the disabled.*
 - (d) *An easement or dedication for a passenger shelter if requested by the transit provider.*
 - (e) *Lighting at the transit stop.*



Pedestrian Connections (ADC 8.370).

- (1) *New retail, office and institutional buildings at or near existing or planned transit stops shall provide for convenient pedestrian access to transit.*
- (2) *Walkways shall be provided connecting building entrances and streets adjoining the site.*
- (3) *Pedestrian connections to adjoining properties shall be provided except where such a connection is impractical. Pedestrian connections shall connect the on-site circulation system to existing or proposed streets, walkways, and driveways that abut the property. Where adjacent properties are undeveloped or have potential of redevelopment, streets, access ways and walkways on site shall be laid out or stubbed to allow for extension the adjoining property.*

FINDINGS OF FACT: The nearest transit stops are located on the northwest corner of Pacific Boulevard (Highway 99E) and 29th Avenue and the northeast corner of Pacific Boulevard and 24th Avenue. These are located about 700 feet north and 720 feet south of the subject property, respectively. A direct pedestrian connection will be provided to the sidewalk along the west side of Pacific Boulevard, providing convenient access to transit. This standard is met.



Large Parking Areas (ADC 8.380). *The amount of parking needed for larger commercial development can result in a large expanse of pavement. Landscaping in a parking area shall be incorporated in a manner that is both attractive and easy to maintain, minimizes the visual impact of surface parking, and improves environmental and climatic impacts (Figure 8-6). In addition to the provisions of Article 9, the following standards apply to commercial development when more than 75 parking spaces are proposed.*

- (1) *Walkways are necessary for persons who will access the site by walking, biking or transit. A continuous pedestrian walkway at least 7 feet wide shall be provided from the primary frontage sidewalk to the customer entrance for each building. This internal walkway shall incorporate a mix of landscaping, benches, drop-off bays and bicycle facilities for at least 50 percent of the length of the walkway. The walkways must be designed for access by disabled persons. If the walkway crosses a parking area or vehicle aisle, the standards in Subsection (2) below apply.*
- (2) *For the safety of pedestrians, parking lots shall be designed to separate pedestrians from vehicles and include protected pedestrian walkways from parking areas to building entrances. Walkways shall be protected by landscaping or parking bumpers. Walkways shall be at least 7 feet wide with no car overhang or other obstruction; 9' 6" for car overhang on one side; 12 feet for car overhang on both sides. Walkways may cross a vehicle aisle if distinguished by a color, texture or elevation different from the parking and driving areas. Walkways shall not share a vehicle aisle.*
- (3) *The parking area shall be divided into pods of no more than 50 spaces each with landscape strips, peninsulas, or grade separations to reduce the visual impact of large expanses of paving,*

to direct vehicular traffic through the parking lot, and to provide a location for pedestrian walkways.

- (4) *Pods may have access at one or both ends. A pod may be U-shaped with double access at one end.*
- (5) *Pods shall be separated with physical breaks by providing one or more of the following:*
 - (a) *Landscape strips between parallel parking rows that are at least 5 feet wide with no car overhang and 10 feet wide with a car overhang. When incorporating pedestrian walkways, the strips shall be at least 20 feet wide to accommodate vehicle overhangs, walkways, lights, posts and other appurtenances.*
 - (b) *Building pads, landscaped pedestrian walkways, interior streets or other site features.*
- (6) *Landscaping for large parking areas shall consist of at least seven (7) percent of the total parking area plus one tree per eight parking spaces to create a canopy effect. The total parking area includes parking spaces, travel aisles, sidewalks and abutting landscaped areas.*

FINDING OF FACT: On-site parking for the development will be separated into two lots. The first lot will provide 32 spaces of public parking. The second parking lot will provide a secure parking area for police department employees and law enforcement vehicles. Based on a parking requirement of 101 spaces, the secured parking area will provide 69 spaces. The balance of the lot will be used for impound and storage; therefore, ADC 8.380 is not applicable.



Compatibility Details (ADC 8.390). *Commercial development shall be designed to comply with the following applicable details and any other details warranted by the local conditions:*

- (1) *On-site lighting is arranged so that light is reflected away from adjoining properties and/or streets.*
- (2) *Any undesirable impacts produced on the site, such as noise, glare, odors, dust or vibrations have been adequately screened from adjacent properties.*
- (3) *The site is protected from any undesirable impacts that are generated on abutting properties.*
- (4) *Unightly exterior improvements and items such as trash receptacles, exterior vents and mechanical devices have been adequately screened.*
- (5) *Storage areas, trash collection facilities and noise generating equipment are located away from public streets, abutting residential districts or development, or sight obscuring fencing has been provided.*
- (6) *Where needed, loading facilities are provided on-site and are of sufficient size and number to adequately handle the delivery or shipping of goods or people. Where possible, loading areas should be designed so that vehicles enter and exit the site in a forward motion.*

FINDINGS OF FACT: On-site lighting will be controlled through shielding to ensure that exterior lighting will not result in glare upon the public right-of-way or beyond the facility's property line. All overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors (Attachment D.13). Landscape plans include a variety of parking lot trees and shrubs lining the exterior of the parking lot/site with the intention of buffering the neighbors from any noise, glare, odors, and dust. The secure parking area will be surrounded by fencing. The subject property is bordered by single-family residences, professional offices, Pacific Boulevard and Cathey Creek drainage channel. None of these abutting properties impose undesirable impacts on the proposed police station. Trash receptacles will be stored in a concrete masonry unit (CMU) block trash enclosure south of the western secured parking area (Attachment D.14). Mechanical equipment will be located on the roof and will be blocked by parapets and mechanical screens, as shown in the building elevation drawings (Attachments D.3 – D.6). As law enforcement needs

arise, loading and unloading of people can be provided within the secure sally port area located on the north side of the building. No other loading area is proposed. This standard is met.

CONCLUSIONS

- DS 1. The applicant may change or add to the preliminary choice of pedestrian amenities. Three pedestrian amenities are required for this development.
- DS 2. Building transparency and architectural relief features associated with this development meet design standards as submitted.
- DS 3. Applicable design standards will be satisfied when the following conditions of approval are met.

CONDITIONS

- DS 1. **Before the City approves occupancy of the building**, a minimum of three pedestrian amenities shall be installed in accordance with ADC 8.360(2).

STAFF ANALYSIS

Tentative Replat File RL-10-15

Section 11.180 of the Albany Development Code (ADC) includes the following review criteria, which must be met for this application to be approved. Code criteria are written in *bold italics* and are followed by findings, conclusions, and conditions of approval where conditions are necessary to meet the review criteria.

Criterion (1) Development of any remainder of property under the same ownership can be accomplished in accordance with this Code.

FINDINGS OF FACT

- 1.1 The proposed replat will combine two parcels, and the vacated portions of Willetta Street and Cathey Creek, into one parcel under the same ownership (see Attachment D.18). The proposed development would utilize the entire site and would also dedicate the Willetta Street cul-de-sac bulb.
- 1.2 All of the property owned by the applicant is included in the area to be replatted. There is no remainder of property to be considered.

CONCLUSION

- 1.1 The above criterion is satisfied because there will be no remainder of property to consider.

Criterion (2) Adjoining land can be developed or is provided access that will allow its development in accordance with this Code.

FINDINGS OF FACT

- 2.1 This review criterion has been interpreted by the City Council to require only that adjoining land either have access, or be provided access, to public streets.
- 2.2 ADC 12.060 requires that development must have frontage on or approved access to a public street currently open to traffic.

- 2.3 The adjoining land is currently developed (except Cathey Creek to the north) and has access to existing right-of-ways. The areas proposed to be vacated are not necessary for adjoining lots to achieve access. Combining multiple parcels into one lot allows for construction of the proposed police station.

CONCLUSIONS

- 2.1 All of the adjoining land has access to public streets and/or private alleys, and the proposed replat would not affect the access of adjoining land.

- 2.2 This criterion is met.

Criterion (3) The proposed street plan affords the best economic, safe, and efficient circulation of traffic possible under the circumstances.

FINDINGS OF FACT

- 3.1 The subject lot is presently served by public streets.

- 3.2 Albany's Transportation System Plan (TSP) does not identify any capacity problems adjacent to the development.

- 3.3 No new streets are proposed. The cul-de-sac will serve the subject site and other properties on Willetta Street, and will provide the best possible circulation given the dead-end setting. The site will also continue to be served by Pacific Boulevard.

CONCLUSIONS

- 3.1 The proposed replat does not create additional lots nor leave any remaining amount of land.

- 3.2 No new street plan is being proposed.

- 3.3 This review criterion has been met.

Criterion (4) The location and design allows development to be conveniently served by various public utilities.

FINDINGS OF FACT

- 4.1 The subject site will be served by public utilities.

- 4.2 The replat would accommodate and provide easements for the existing public water and stormwater facilities on site.

CONCLUSION

- 4.1 The proposed lot consolidation will not adversely impact public utilities or services to the subject property.

- 4.2 This criterion is met without conditions.

Criterion (5) Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.

FINDINGS OF FACT

5.1 Findings 6.1 through 6.4 of Conditional Use CU-07-15 above, are hereby incorporated by reference.

CONCLUSIONS

- 5.1 The subject property is not located within the Airport Approach district, 100-year floodplain, Natural Resource Overlay district or Historic district.
- 5.2 Wetlands have been identified on the site that will require mitigation in accordance with Oregon Department of State Lands/U.S. Army Corps of Engineers permitting and mitigation requirements.
- 5.3 There are no other special purpose districts to consider with this replat application. This criterion is met without conditions.

OVERALL CONCLUSION

This report reviews and evaluates a consolidated Street Vacation, Conditional Use and Replat application submitted by Mackenzie on behalf of the City of Albany Police Department for conformance with the Albany Development Code (ADC). As proposed and conditioned, the application for a street vacation, conditional use review and replat to develop a new police station satisfies all of the applicable review criteria as outlined in this report.

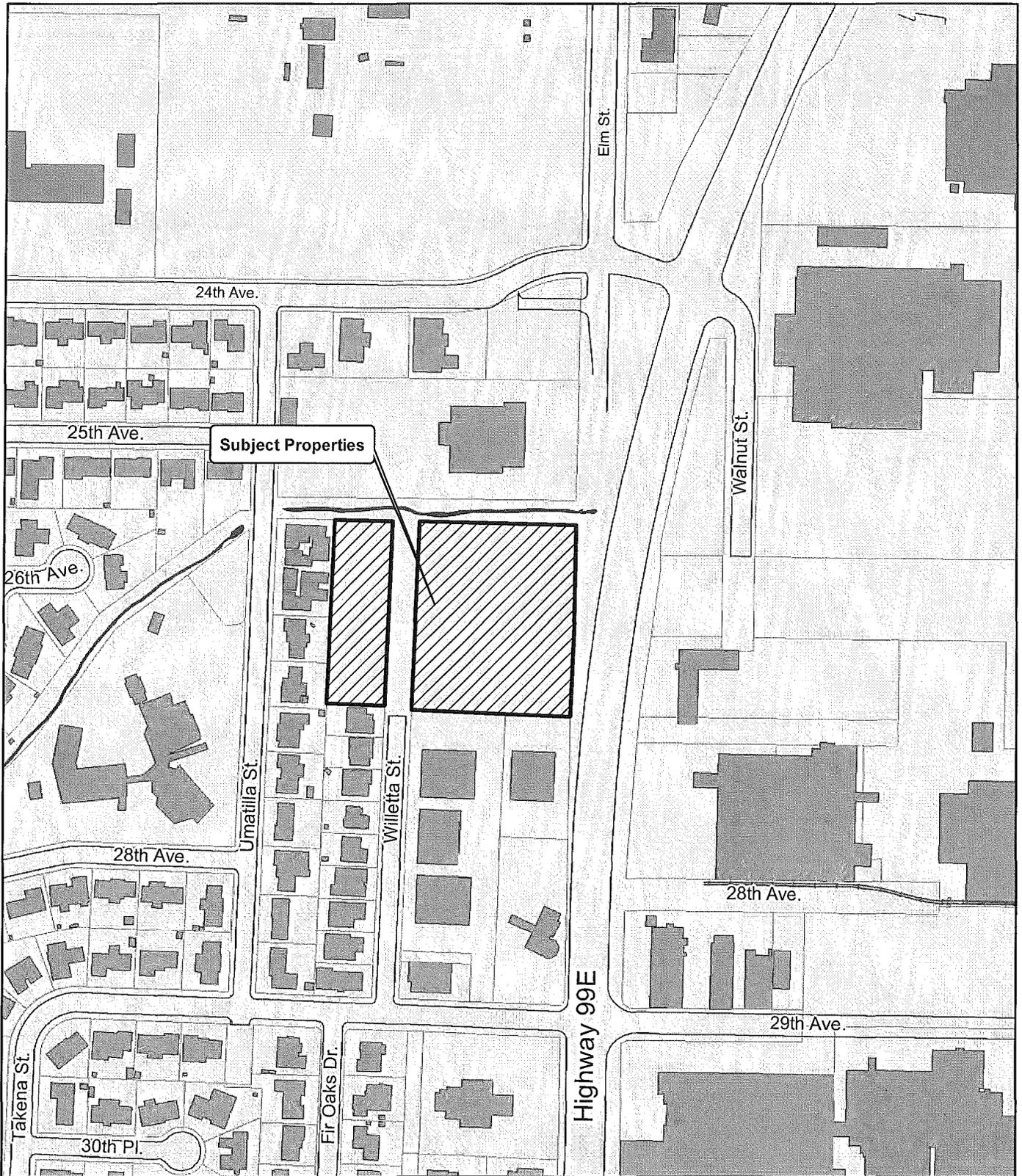
ATTACHMENTS

- A. Location Map
- B. Vacation Documents
- B.1 – B.3 Map Exhibits
 - B.4 Applicant Findings
 - B.5 Legal Descriptions
 - B.6 Property Reversion
- C. Conditional Use and Tentative Replat Findings Documents
- C.1 Mackenzie Response to Incomplete Letter, 10/14/2015
 - C.2 – C.48 Conditional Use and Tentative Replat Findings
 - C.49 – C.57 Neighborhood Meeting Documents
- D. Conditional Use and Tentative Replat Site Plans
- D.1 – D.2 Concept Massing – Prospective Views
 - D.3 – D.6 Building Elevation Drawings
 - D.7 Property Zoning Map
 - D.8 Existing Conditions Plan, Wetlands (Sheet C1.1)
 - D.9 Site Plan, 10/16/2015 (Sheet C2.1)
 - D.10 Grading Plan (Sheet C2.2)
 - D.11 Utility Plan (Sheet C2.3)
 - D.12 Site Details (Sheet C8.0)
 - D.13 Photometric Site Plan, Lighting Details (Sheet E1.1PH)
 - D.14 Revised Refuse Disposal Area Drawing
 - D.15 Plaza Layout and Materials Plan (Sheet L2.1)
 - D.16 Overall Landscape Plan (Sheet L4.0)
 - D.17 Irrigation Plan (Sheet L3.1)
 - D.18 Tentative Replat (Sheet C2.4)

ACRONYMS

ACOE	(U.S.) Army Corps of Engineers
ADC	Albany Development Code
AMC	Albany Municipal Code
APD	Albany Police Department
DSL	(Oregon) Department of State Lands
FEMA	Federal Emergency Management Agency
FIRM	Flood Insurance Rate Map
ICC	International Construction Code
ITE	Institute of Transportation Engineers
NAVD 1988	North American Vertical Datum of 1988*
NGVD 1929	National Geodetic Vertical Datum of 1929*
ODOT	Oregon Department of Transportation
OFC	Oregon Fire Code
OP	Office Professional Zoning District
/RC	Riparian Corridor Overlay District
RS-5	Residential Single-family Zoning District (minimum lot size 5,000 sq. ft.)
RS-6.5	Residential Single-family Zoning District (minimum lot size 6,500 sq. ft.)
SD	Subdivision
/SW	Significant Wetland Overlay District
TSP	City of Albany's Transportation System Plan

*The conversion factor from NGVD 1929 to NAVD 1988 in Albany is +3.38 feet.



Location Map: West side of Pacific Blvd., midway between 24th and 29th Avenues



The City of Albany's Infrastructure records, drawings, and other documents have been gathered over many decades, using differing standards for quality control, documentation, and verification. All of the data provided represents current information in a readily available format. While the data provided is generally believed to be accurate, occasionally it proves to be incorrect; thus its accuracy is not warranted. Prior to making any property purchases or other investments based in full or in part upon the material provided, it is specifically advised that you independently field verify the



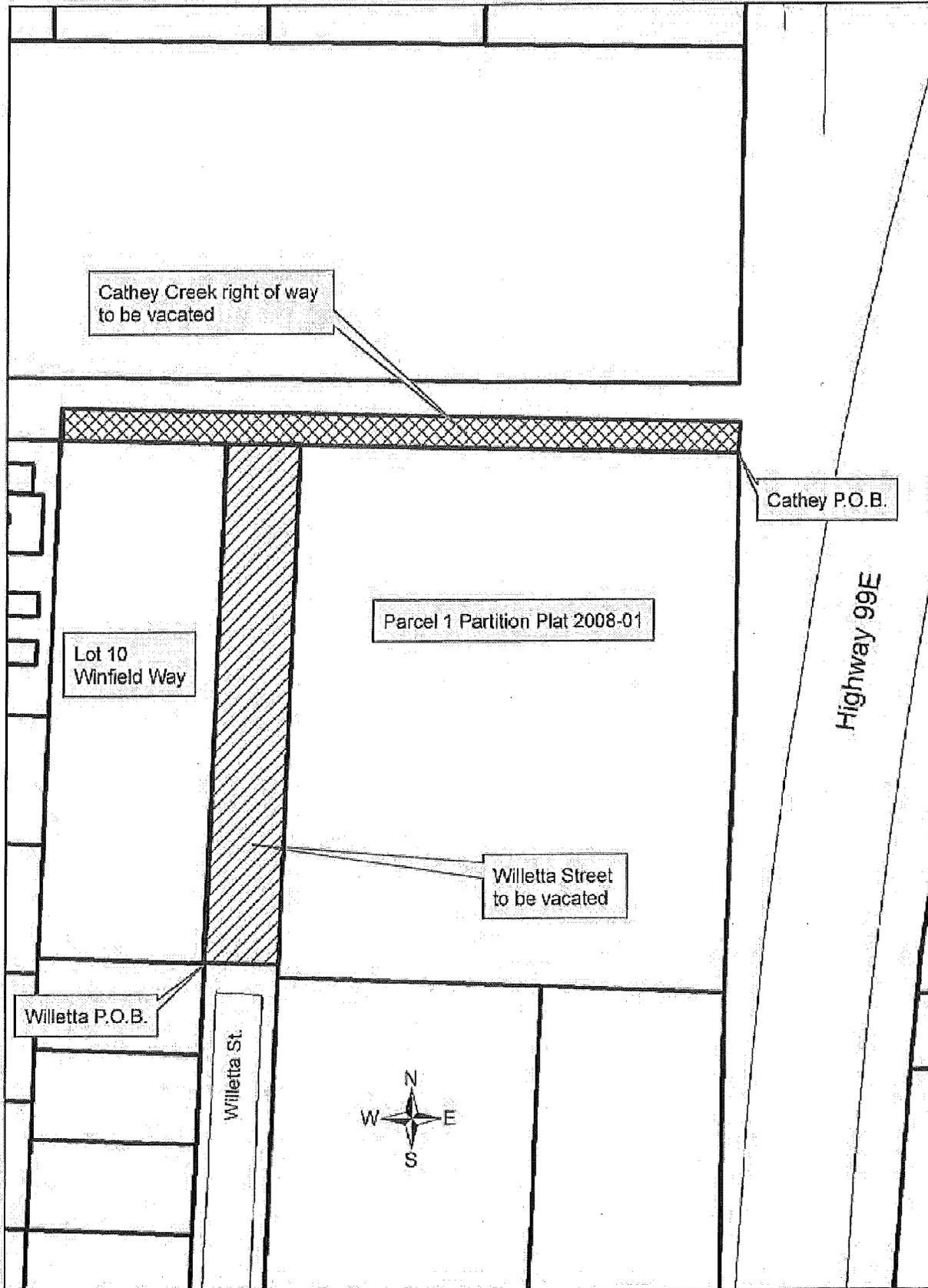
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June 2, 2015

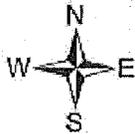
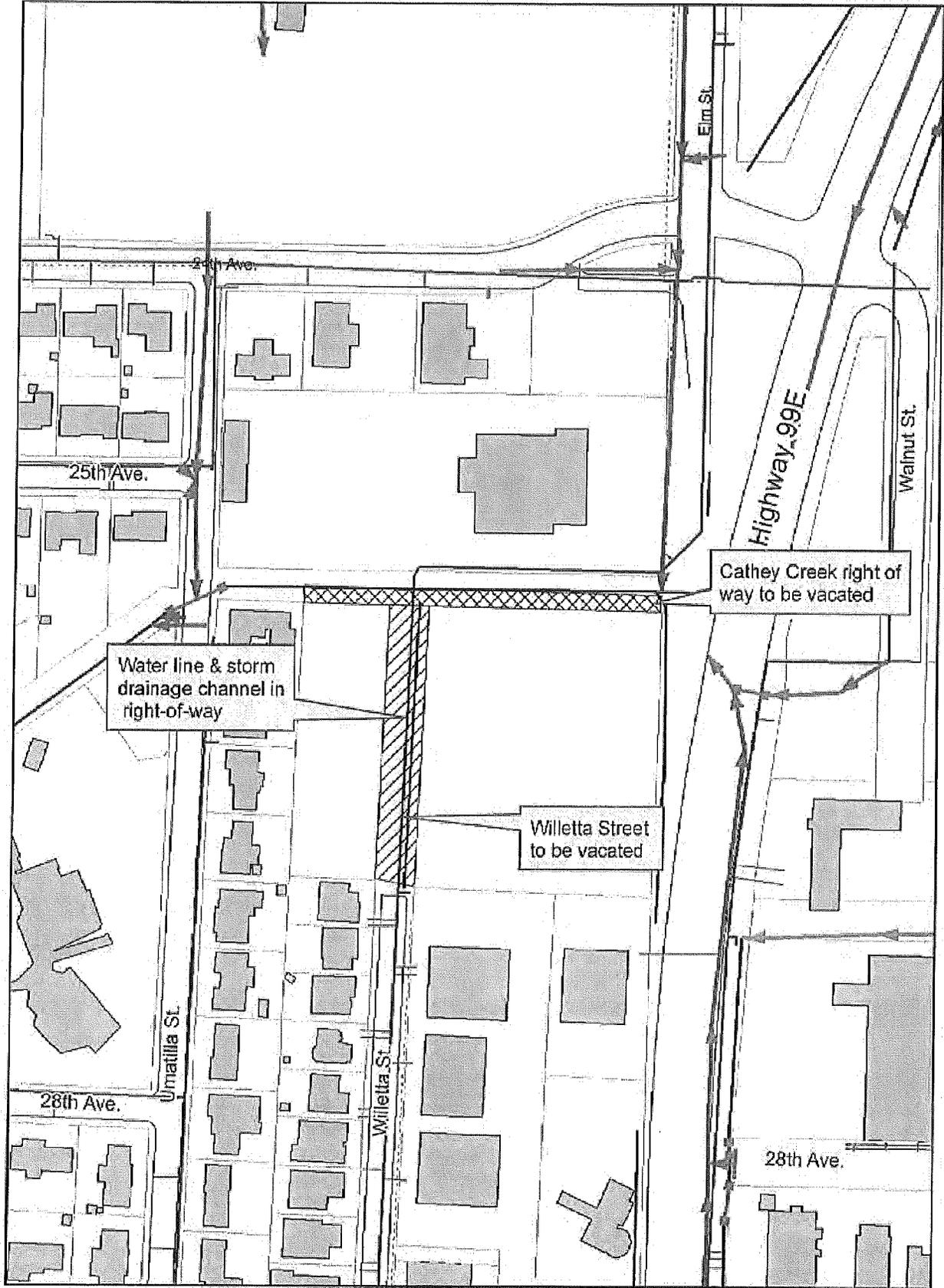
Planning Division

City of Albany - 333 Broadalbin St. SW, Albany, Oregon 97321 (541) 917- 7550

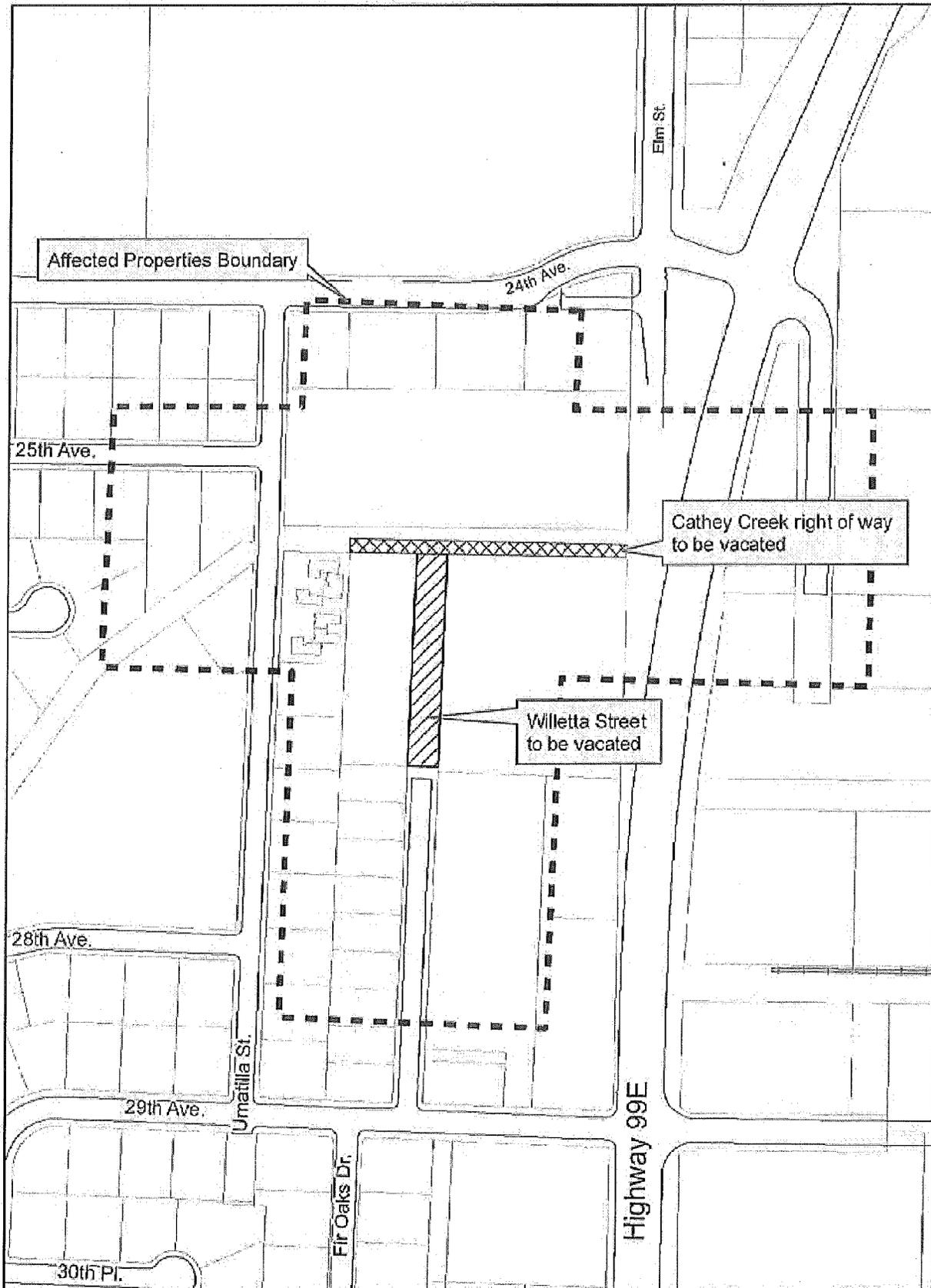
Exhibit A - Willetta Street & Cathey Creek Vacations



Site Plan for Right-of-way vacation



Attachment A - Affected Properties



Willetta Street and Cathey Creek - Vacation Review Criteria

- (1) The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan city transportation or public facility plan.**

Fact: Neither of these right-of-ways appear in the Albany Transportation System Plan.

Conclusion: Vacating these right-of-ways is not inconsistent with the Albany Transportation System Plan.

- (2) The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.**

Fact: This section of Willetta Street has never been constructed and never been open to traffic. All existing properties in the area have access to existing improved street. This section of Cathey Creek is a drainage facility, not a public street, and it does not provide access to any properties.

Conclusion: All adjacent properties currently have access to public streets. Vacating these right-of-ways will not have a negative impact on any property's access to public streets.

- (3) The requested vacation will not have a negative effect on traffic circulation or emergency service protection.**

Fact: The right-of-ways proposed to be vacated do not contain any constructed roadways.

Conclusion: There will be no negative effect on traffic circulation or emergency service protection due to vacating these rights-of-way.

- (4) The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.**

Fact: The right-of-ways will be converted into a portion of the new City of Albany Police Station. The Police Station will be required to meet Albany Development Code standards.

Conclusion: The vacated right-of-way will be brought into Development Code compliance.

- (5) The public interest, present and future, will be best served by approval of the proposed vacation.**

Fact: The vacated right-of-ways will be converted into a portion of the new City of Albany Police Station.

Conclusion: It is in the public interest to have a new Police Station. Approving this right-of-way vacation will allow the Police Station to be completed.

Legal Description of proposed Vacation areas of Willetta Street and Cathey Creek.

Willetta Street –

The northern portion of the Willetta Street right-of-way that was created by deeds recorded in Linn County, Oregon Deed Records Book 339, Page 355 and Book 340, Page 617, more particularly described as follows:

Beginning at the Southeast corner of Lot 10 of Winfield Way subdivision, a subdivision of record located in the NE ¼ of Section 13, T11S, R4W, W.M., said Southeast Corner of Lot 10 also lying on the west right-of-way line of Willetta Street; thence North 0°26'00" East 343.76 feet along the west right-of-way line of Willetta Street to the northeast corner of Lot 10, said northeast corner also lying on the south right-of-way line of Cathey Creek, a right-of-way recorded in Linn County, Oregon deed records Book 159, Page 174; thence east 50.01 feet along the south right-of-way line of the Cathey Creek right-of-way to the northwest corner of Parcel 1 of Linn County, Oregon, Partition Plat No. 2008-01, said corner also lying on the east right-of-way line Willetta Street; thence South 0°26'00" West 343.7651.79 feet along the east right-of-way line of Willetta Street to a point 8.03 feet north of the southwest corner of said Parcel 1; thence West 50.00 feet to the point of beginning, as shown on attached Exhibit A.

Cathey Creek –

The southern 20 feet of the eastern 453.11 feet of the drainage right-of-way created by a deed recorded in Linn County, Oregon deed records Book 159, Page 174, more particularly described as follows:

Beginning at the Northeast corner of Parcel 1 of Linn County, Oregon, Partition Plat No. 2008-01; thence south 89°23' west 293.19 along the north property line of said Parcel 1 to the northwest corner of said Parcel 1; thence continuing south 89°23' west 50.01 feet to the northeast corner of Lot 10 of Winfield Way subdivision; thence continuing south 89°23' west 109.91 feet along the north property line of said Lot 10 to the northwest corner of Lot 10; thence north 0°37' west 20 feet to the centerline of the Cathey Creek right-of-way; thence north 89°23' east 453.11 feet parallel with and 20 feet north of the south right-of-way line of Cathey Creek right-of-way to a point on the west right-of-way line of Pacific Highway, said point lying North 0°37' west 20 feet from the northeast corner of Parcel 1; thence south 0°37' east 20 feet along the west right-of-way line of Pacific Highway to the point of beginning, as shown on attached Exhibit A.

Properties to which the vacated area will be transferred

The property that the vacated area will revert to is Lot 10 of Winfield Way subdivision and Parcel 1 of partition Plat 2008-01. Both right-of-way dedications originally were attached to these properties. Since only the southern half of the Cathey Creek right-of-way is being vacated, it will all revert to the properties to the south. The northern half of Cathey Creek would revert to the property to the north, however, that half of the right-of-way is not being vacated.

MACKENZIE.

DESIGN DRIVEN | CLIENT FOCUSED

October 14, 2015

City of Albany Community Development Department
Attention: David Martineau, AICP, CFM
PO Box 490
Albany, OR 97321

Re: **Albany Police Station (VC-02-15, CU-07-15, & RL-10-15)**
Response to Incompleteness Determination
Project Number 2140284.02

Dear Mr. Martineau:

Thank you for your letter dated October 13, 2015 to Police Chief Mario Lattanzio regarding items necessary to deem the land use application complete for the proposed Albany Police Station on Pacific Boulevard. In accordance with your request we have enclosed a stormwater report to document compliance with City requirements for stormwater detention and stormwater quality.

Furthermore, we have amended the site plan to provide 133 parking spaces in the secure parking lot; to provide an additional drive aisle allowing direct access to the vehicle evidence bay; and to reflect a recent building size reduction from 41,749 square feet to 40,367 square feet as requested by the Police Department. An updated site plan is attached for use during the City's review of the land use applications.

If you have any questions regarding this information, please feel free to contact me at your earliest convenience.

Sincerely,



Brian Varricchione, P.E.
Land Use Planner

Enclosure(s): Stormwater Report
Revised Site Plan

c: Chief Mario Lattanzio – Albany Police Department



**CONDITIONAL USE
PERMIT, STREET
VACATION, &
TENTATIVE REPLAT**

To
City of Albany

For
Police Station
Pacific Highway between 24th
Avenue and 29th Avenue SW

Submitted
September 14, 2015

Project Number
2140284.02

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EXHIBITS

1. Zoning Map
2. Building Perspectives, Elevations, and Materials
3. Plans
4. Neighborhood Meeting Summary
5. Trip Generation Calculations
6. Street Vacation Findings
7. Wetland Delineation

I. PROJECT SUMMARY

Applicant: Albany Police Department
Att: Mario Lattanzio, Chief of Police
1117 Jackson Street SE
Albany, OR 97321
mario.lattanzio@cityofalbany.net
(541) 917-7680

Owner: City of Albany
PO Box 490
Albany, OR 97321
(541) 917-7500

Contact: Mackenzie
Att: Brian Varricchione
1515 SE Water Avenue, Suite 100
Portland, OR 97214
bvarricchione@mcknze.com
(503) 224-9560

Location: Pacific Boulevard SW (Highway 99E) between 29th Avenue and 24th Avenue
Tax Map 11S04W13AB, Tax Lots 6900 & 7003

Site Address: None

Site Size: 3.89 Acres

Zoning: RS-5 – Residential Single Family District (western portion of site)
OP – Office Professional District (eastern portion of site)

Comprehensive Plan: Residential – Low Density (western portion of site)
Commercial Light (eastern portion of site)

Adjacent Zoning: RS-5 and RM to the west, OP to the north and south, and LI to the east.

Existing Structures: There are no existing structures on the site.

Request: Conditional Use Permit, Tentative Replat, and Street Vacation for Albany Police Headquarters

II. INTRODUCTION

Description of Request

On May 19, 2015, the citizens of Albany passed a bond measure which allowed for the replacement and reconstruction of existing police and fire facilities which have become inadequate to serve the needs of the Police Department and Fire Department. Therefore, the Albany Police Department is now requesting the review and approval of land use applications to allow for the construction of a new headquarters facility on Pacific Boulevard SW (US Highway 99E). Required applications include a conditional use permit, a tentative replat to combine two parcels into one, and vacation of a portion of Willetta Street and the south 20 feet of the Albany Canal/Cathey Creek right-of-way.

Existing Site & Surrounding Land Use

The proposed site for the new Police Headquarters is comprised of two tax lots adjacent to Willetta Street SW (see Figure 1). The site is bounded to the west by residences on Umatilla Street SW, to the north by the Albany Canal/Cathey Creek drainage ditch right-of-way, to the east by Highway 99E, and to the south by residences on Willetta Street and commercial development on Highway 99E.

The site is currently undeveloped and is largely cleared of trees. The eastern portion of the site is in the Office Professional (OP) zone and the western part of the site is Residential Single Family (RS-5). The site is bounded by single family and medium density residential districts to the west, Office Professional districts to the north and south, and light industrial zoning to the east.



Figure 1: Site Boundaries

Description of Proposed Development

The applicant is proposing the development of a new 41,749 square foot police headquarters to serve the City of Albany. The total site area is 163,272 SF. Of that, 15% will be building area, 52% will be paved, and 33% will be landscaped.

Development will include a total of 167 on-site parking spaces, 135 of which will be dedicated to police station use in a secure area. Of the 32 parking spaces open to the public, 4 will be accessible spaces. The applicant proposes extensive landscaping within the parking lots and fronting Pacific Boulevard and a vegetated buffer in the western portion of the site to separate the development from adjoining residences. The building itself is proposed to have a brick and metal façade with a generous amount of windows to be an inviting and attractive addition to the Pacific Boulevard streetscape.

III. NARRATIVE & COMPLIANCE

Oregon Revised Statutes

Chapter 271 - Use and Disposition of Public Lands Generally; Easements

Response: The development proposal is contingent upon the vacation of the abutting portion of Willetta Street SW where it terminates at the site, plus the south half of the Cathey Creek right-of-way abutting the site. ORS Chapter 271 (271.080-271.230) governs the process for vacating streets within incorporated cities. Findings in support of the vacation request are included in Exhibit 6. This standard is met.

City of Albany Development Code

Article 1 – Administration and Procedures

Application Procedures

1.200 Land Use Application Procedures.

- (3) *When a proposal involves more than one application for the same property, the applicant(s) may submit concurrent applications that shall be processed simultaneously in accordance with the highest numbered procedure specified. When concurrent applications are received and accepted as complete, the 120-day requirement of Section 1.220(2) shall apply as if a single application had been made.*

Response: The proposed development will require numerous land use permits and processes. It will involve a street vacation to create a cul-de-sac at Willetta Street SW; a re-plat to remove property lines and combine parcels; a conditional use review for a Community Services land use designation in the RS-5 and OP zones; and site plan review for tree felling. All of these applications will be submitted simultaneously. Since the street vacation requires hearings in front of the Planning Commission and the City Council, the City Council will be the decision-making body for all associated applications. This standard is met.

1.204 Neighborhood Meeting Standards.

- (1) *The applicant shall consult with City staff to determine an appropriate meeting date, time, and place given the location of the proposed development and availability of staff to attend.*
- (2) *The applicant shall send mailed notice of the public meeting to the Community Development Department Director and all property owners within a minimum distance of 300 feet of the boundaries of the subject property with the specific area to be determined by the Director based on the project scale, land use and transportation patterns or anticipated public interest in the project. If any part of the subject property is within the boundaries of a neighborhood association recognized by the City of Albany or within 300 feet of any other neighborhood association recognized by the City, notice shall be sent to the designated representative(s) of such neighborhood association(s). The property owner list shall be compiled from county tax assessor's property owner list from the most recent property tax assessment roll. The address for the designated representative(s) of the affected neighborhood association(s) shall be obtained from the City. The notice shall be sent a minimum of 10 days and no more than 30 days before the meeting, and shall include:*
- (a) *Date, time and location of the public meeting.*

- (b) *A brief written description of the development proposal and proposed use(s) with enough specificity so that the project is easily discernable.*
- (c) *The location of the subject property(ies), including address (if applicable), nearest cross streets and any other easily understood geographical reference, and a map (such as a tax assessors map) that depicts the subject property.*
- (3) *The applicant's presentation at the neighborhood meeting shall include:*
 - (a) *A map depicting the location of the subject property(ies) proposed for development.*
 - (b) *A visual description of the project including a site plan, tentative subdivision plan and elevation drawings of any proposed structures, when applicable.*
 - (c) *A description of the nature of the proposed use(s) including but not limited to, sizes and heights of structures, proposed lot sizes, density, etc.*
 - (d) *The expected or anticipated impacts from the proposed development (e.g. traffic, storm drainage, tree removal, etc.).*
 - (e) *Mitigation proposed by the applicant to alleviate the expected/anticipated impacts.*
 - (f) *An opportunity for the public to provide comments.*

Response: The applicant coordinated with Planning Division staff to arrange for and publicize the required neighborhood meeting, which was held on August 13, 2015. The notice and materials followed the above procedures. A summary of the meeting is attached as Exhibit 4. This standard is met.

Article 2 – Review Criteria

Conditional Uses

2.250 Review Criteria.

Requests for conditional uses will be approved if the review body finds that the application meets all of the following criteria, either outright or with conditions that bring the proposal into compliance:

- (1) *The proposed use is consistent with the intended character of the base zone and the operating characteristics of the neighborhood.*

Response: The proposed use is a conditional use in both base zones, RS-5 and OP. The intended character of the RS-5 district is that of low- to moderate-density single-family development. The proposed use will complement the existing single-family residential uses and will not have a negative effect on their operations. Per Albany Development Code 4.020,

The OP district is intended to provide a vertical or horizontal mix of professional offices, personal services, live-work, residential and limited related commercial uses in close proximity to residential and commercial districts...OP is typically appropriate along arterial or collector streets as a transitional or buffer zone between residential and more intense commercial or industrial districts.

The proposed building will include office and institutional uses, and is along an arterial street. The use will serve to buffer the residential and industrial districts on either side. In addition, the site is currently vacant so the development provides an opportunity to establish a civic structure with a strong design. In addition, the new station will act as a public safety service and will protect and enable healthy growth of the surrounding community. This standard is met.

- (2) *The proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, setbacks, and landscaping or the proposal mitigates difference in appearance or scale through such means as setbacks, screening, landscaping or other design features.*

Response: Because the site is in a transition area between land use zones, there is no established scale or standards to mimic. The proposal is for a two-story structure designed to a human scale with reliefs and landscaping. A large portion of the lot will be used as parking for police vehicles and official use, all screened by landscaping. This standard is met.

(3) *The transportation system can support the proposed use in addition to the existing uses in the area. Evaluation factors include street capacity and level of service, on-street parking impacts, access requirements, neighborhood impacts and pedestrian safety.*

Response: A transportation impact study was not required for this development. Based on the size and scope of the proposed police station, Mackenzie transportation engineers estimate that the project would generate 492 average daily trips, of which 41 would be during the evening peak hour. To minimize impacts on Highway 99E, only one driveway is proposed directly onto Pacific Boulevard. This driveway will serve the majority of traffic to and from the site, while the connections to Willetta Street will be for secondary access only. Pedestrian connections are proposed to Pacific Boulevard and Willetta Street, and bicycle parking will be located on site for use by employees and visitors. It is not anticipated that the vehicle traffic associated with the station would cause any of the existing transportation network to fall below mobility standards for either the City of Albany or the Oregon Department of Transportation (ODOT), which has jurisdictional control over Pacific Boulevard (Highway 99E). This standard is met.

(4) *Public services for water, sanitary and storm sewer, water management, and for fire and police protection, can serve the proposed use.*

Response: As shown in the attached plans, public services such as water, sanitary, storm sewer, and water management will fully serve the proposed police facility without requiring infrastructure upgrades (water and storm drainage facilities traverse the site from south to north). The site is within a well-connected location that allows for acceptable response time for fire and police protection in the event outside assistance is needed. This standard is met.

(5) *The proposal will not have significant adverse impacts on the livability of nearby residentially zoned lands due to:*

- (a) *Noise, glare, odor, litter, or hours of operation.*
- (b) *Privacy and safety issues.*

Response: The proposed use is for an office and institutional use for Albany Police, which will be a 24-hour operation. Due to the nature of the law enforcement use, no objectionable noises, odor, or litter are expected. Glare will be minimized through the use of appropriate shielding on parking and building lighting fixtures, as shown in Exhibit 3 Sheet E1.1PH. The building will not create privacy or safety issues; the proposed site plan includes fencing and landscaping to protect the neighboring properties on all sides, including the residentially zoned neighbors to the south and west of the site. This standard is met.

(6) *Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.*

Response: The proposal is not sited within any special purpose districts. This standard does not apply.

Vacations

2.630 Review Criteria.

A vacation request may be approved if the review body finds that the applicant has shown that all of the following review criteria are met:

- (1) *The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan, city transportation or public facility plan.*
- (2) *The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.*
- (3) *The requested vacation will not have a negative effect on traffic circulation or emergency service protection.*
- (4) *The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.*
- (5) *The public interest, present and future, will be best served by approval of the proposed vacation.*

Response: Findings in support of the proposed street vacation request are attached as Exhibit 6. This standard is met.

Article 3 – Residential Zoning Districts

Zoning Districts

3.020 Establishment of Residential Zoning Districts.

In order to implement the mixed-use and livability concepts in the Town Center and Albany Comprehensive Plans, the following zoning districts are created:

- (4) *RS-5—RESIDENTIAL SINGLE FAMILY DISTRICT. The RS-5 District is intended primarily for low- to moderate-density single-family development. The average minimum detached single-family lot size is 5,000 square feet.*

Response: The eastern portion of the proposed site, as shown in Exhibit 1, is within the RS-5 zone and is subject to its development standards. The lot in the RS-5 district will be combined with the Willetta Street right-of-way and lot in the OP district. No single-family residences are proposed as part of this application. This standard is met.

Schedule of Permitted Uses

3.050 Schedule of Permitted Uses.

[...]

The abbreviations used in the schedule have the following meanings:

[...]

CU Use permitted conditionally under the provisions of Sections 2.230-2.260 through a Type III procedure.

USES ALLOWED IN RESIDENTIAL ZONING DISTRICTS	
Use Categories	RS-5
Institutional	
Community Services	CU

Response: Community services are permitted conditionally under the provisions of Sections 2.230-2.260 through a Type III procedure. No building is proposed in the RS-5 zone, but the proposed parking, landscape, and pedestrian circulation system will serve the proposed Community Services use, allowed through a Type III Conditional Use permit in the RS-5 residential zone. This application includes a request for a Type III Conditional Use approval. This standard is met.

Development Standards*3.190 Purpose.*

Development standards are intended to promote site planning and design that consider the natural environment, site intensity, building mass, and open space. The standards also promote energy conservation, needed privacy, safe and efficient parking areas for new development, and improve the general living environment and economic life of a development. Table [3-1], on the following page, summarizes the basic development standards. It should be used in conjunction with the sections immediately succeeding the table, which address special circumstances and exceptions. See Article 8 for design standards for single-family and multiple-family developments.

TABLE 3-1 EXCERPT: RS-5 - RESIDENTIAL DISTRICT DEVELOPMENT STANDARDS (EXCERPTS FOR RS-5 ZONE AS APPLICABLE TO NON-RESIDENTIAL PROJECTS)		
Standard	Allowance	Proposed
Minimum Lot Depth	70'	160' (in RS-5 zone, proposed through replat)
Setbacks:		
Minimum Front ¹	15'	N/A (no building)
Maximum Front	None	N/A (no building)
Minimum Interior: Two or More Stories ¹	5'	N/A (no building)
Maximum Height ²	30' (or through CU, per 3.340)	N/A (no building)
Maximum Lot Coverage	60%	59.8% (portion within RS-5 zone)
Minimum Landscaped Area	All yards adjacent to streets	40.2%

(1) Additional setbacks may be required, see Sections 3.230-3.330 and the buffer matrix at 9.210; exceptions to Setbacks for Accessibility Retrofits are in Section 3.263; Zero-Lot Line standards are in Sections 2.365 and 2.370.

(2) See exceptions to height restrictions, Section 3.340.

Response: No building will be located in the RS-5 zone. As noted in the above table, the project meets all applicable criteria that apply to non-residential projects. This standard is met.

Setbacks*3.330 Special Setbacks for Education and Religious Institutions, Public and Semi-Public Buildings.*

Any new construction of an educational institution, religious institution, or public or semi-public building shall be set back at least 25 feet from any property line adjoining or directly across public right-of-way from any residential district. No required front or interior yard of the lot on which such building or use is located shall be used for stockpiling or storage of materials or equipment. All other setbacks of the district where the property is located continue to apply.

Response: Through the Tentative Replat proposed with this application package, the lot will contain some RS-5 zoning. As shown in the attached plans, the building will be set back 85.3 feet from the RS-5 zoning boundary. No materials or equipment will be stockpiled or stored in the front or interior yard of the lot. This standard is met.

Height*3.340 Height Exceptions.*

Height limitations are shown in Table 1, Development Standards. See also Table 2, Accessory Structure Standards.

- (1) *Roof Structures and Architectural Features.* Roof structures for the housing of elevators, stairways, tanks, ventilating fans and similar equipment required to operate and maintain the building, fire walls, skylights, towers, flagpoles, chimneys, smokestacks, wireless masts, antennas, steeples, and similar structures may be erected above the height limits prescribed in this Article provided that no roof structure, feature, or any other device above the prescribed height limit shall be allowed or used for the purpose of providing additional floor space.
- (2) *Religious Institutions and Public and Semi-Public Buildings.* In zoning districts where religious institutions and certain public and semi-public buildings require Conditional Use approval, the height restrictions may be waived as a part of the Conditional Use proceedings, provided that a request for such has been noted in the public hearing notice.

Response: No building will be located in the RS-5 zone. This standard does not apply.

Article 4 – Commercial and Industrial Zoning Districts

Zoning Districts

4.020 Establishment of Commercial and Industrial Zoning Districts.

In order to regulate and segregate the uses of lands and buildings and to regulate the density of development, the following commercial and industrial zoning districts are created:

- (1) *OP – OFFICE PROFESSIONAL DISTRICT.* The OP district is intended to provide a vertical or horizontal mix of professional offices, personal services, live-work, residential and limited related commercial uses in close proximity to residential and commercial districts. The limited uses allowed in this district are selected for their compatibility with residential uses and the desired character of the neighborhood. OP is typically appropriate along arterial or collector streets as a transitional or buffer zone between residential and more intense commercial or industrial districts.

Response: As shown in Exhibit 1, the western portion of the site is within the OP zone and is subject to its development standards. The proposed use is allowed conditionally in the zone and its development standards will be met. This standard is met.

Schedule of Permitted Uses

4.050 Schedule of Permitted Uses.

[...]

The abbreviations used in the schedule have the following meanings:

[...]

S Use permitted that requires a site plan approval prior to the development or occupancy of the site or building.

CU Use considered conditionally through the Type III procedure under the provisions of Sections 2.230-2.260.

TABLE 4-1 SCHEDULE OF PERMITTED USES	
Use Categories	OP
Institutional	
Community Services ¹	S/CU

(1) Community Service Uses. Community Service uses that may have significant off-site impacts, such as public swimming pools, public safety facilities and homeless shelters, may be considered through the conditional use process.

Response: A Community Services use is permitted through a Type III Conditional Use permit in the OP zone. The zone also requires the approval of a site plan prior to development or occupancy of the site or building. This application includes a request for a Type III Conditional Use approval and includes a site plan. This standard is met.

Development Standards

4.090 Purpose.

Development standards are intended to promote site planning and design that consider the natural environment, site intensity, building mass, and open space. The standards also promote energy conservation, needed privacy, safe and efficient parking areas for new development, and improve the general living environment and economic life of a development. Table 4-2, on the following page, summarizes the basic development standards. It should be used in conjunction with the sections immediately succeeding the table, which address special circumstances and exceptions. See Article 8 for design standards for single-family and multiple-family developments.

Response:

TABLE 4-2 EXCERPT: OP- COMMERCIAL AND INDUSTRIAL DEVELOPMENT STANDARDS		
Standard	Allowance	Proposed
Minimums:		
Lot Size	None	109,908 SF (within OP zone, proposed through replat)
Lot Width	None	375'
Lot Depth	None	293' in OP zone
Front Setback	10'	92.9'
Interior Setback – abutting non res'l	5'	44.4'
Interior Setbacks – abutting res'l district ¹	10'	85.3'
Maximums:		
Building Size ²	None	41,749 SF
Lot Size	None	109,908 SF (within OP zone, proposed through replat)
Height	30'	39'
Lot Coverage	70%	70.0%
Landscaped Area ³	100%	100% adjacent to streets; 30% overall in OP zone

(1) Structures on property abutting residential districts and/or uses require 1' of setback for each foot of finished wall height, with a minimum setback of 10'.

(2) The maximum business footprint for supporting commercial uses allowed in IP is 5,000 SF. The maximum business footprint for convenience-oriented and personal service-oriented retail uses in NC and OP is 5,000 SF.

(3) All yards adjacent to streets. Approved vegetated post-construction stormwater quality facilities are allowed in landscaped areas.

Response: As shown in the above table, the project meets all applicable criteria except height, which will be addressed through the Conditional Use process as specified in Section 4.240. All other standards are met.

Setbacks*4.210 Special Setbacks for Educational and Religious Institutions, Public and Semi-Public Buildings.*

Any new construction of a school, church, or public or semi-public building must be set back at least 25 feet from any property line adjoining or directly across public right-of-way from any residential district. Stockpiling or storing materials or equipment is not permitted in the required front or interior setbacks. All other setbacks of the district where the property is located apply.

Response: As illustrated on Exhibit 3 Sheet C2.1, the proposed building would be located upwards of 80 feet from any property line within the RS-5 zone (on the rear of the site). This standard is met.

4.220 Parking Restrictions in Setback Areas.

Parking and loading spaces may not be located in a required front or side setback, except:

- (1) *Driveways meeting dimensional standards may be used to fulfill parking requirements for single-family and two-family residences. Each space must be a paved area at least 10 feet wide and 20 feet long.*

Response: As shown in the attached plans, no parking or loading will be located in the required 10' front setback (at least 20 feet is provided) or side setback. No residences are proposed. This standard is met.

Height*4.240 Height Exceptions.*

- (1) *Roof structures for the housing of elevators, stairways, tanks, ventilating fans and similar equipment required to operate and maintain the building, fire walls, skylights, towers, flagpoles, chimneys, smokestacks, wireless masts, antennas, steeples, and similar structures may be erected above the height limits prescribed in this article, provided that no roof structure, feature, or any other device above the prescribed height limit may be allowed or used for the purpose of providing additional floor space.*
- (2) *Religious Institutions and Public and Semi-Public Buildings. In zoning districts where churches and certain public and semi-public buildings require Conditional Use approval, the height restrictions may be waived as a part of the Conditional Use proceedings, provided that a request for such has been noted in the public hearing notice.*

Response: As illustrated on Exhibit 3 Sheet A3.1, the proposed building would have a height of 39 feet, which exceeds the 30-foot standard for the OP zone. The applicant requests that the City authorize this additional height as allowed for public buildings. This standard is met.

Outside Storage*4.290 General.*

- (1) *In the NC, OP, TD and IP zoning districts, outside storage or display of materials, junk, parts, or merchandise is not permitted, except for automobile sales (where allowed).*

4.300 Screening of Refuse Containers.

The following standards apply to all development, except for one- and two-family dwellings. Any refuse container or disposal area that would otherwise be visible from a public street, customer or resident parking area, any public facility, or any residential area, must be screened from view by placement of a sight-obscuring fence, wall, or hedge at least 6 feet tall. All refuse materials must be contained within

the screened area. Refuse disposal areas may not be located in required setbacks or buffer yards and must be placed at least 15 feet from any dwelling window.

Response: As shown in the attached plans (Exhibit 3 Sheet C2.1), a trash enclosure will be located on-site, northwest of the cul-de-sac. The enclosure will provide a six-foot sight-obscuring masonry wall to screen the trash containers. The enclosure will be located 5 feet from the property line. This standard is met.

Article 6 – Natural Resource District

Significant Natural Resource Overlay Districts

6.260 Overview.

The Significant Natural Resource overlay districts include Riparian Corridors (/RC) and Significant Wetlands (/SW) and fish-bearing waterways throughout the city, and a Habitat Assessment area (/HA) designated specifically for turtle habitat in and around Thornton Lakes in North Albany.

Response: The site abuts a portion of Albany Canal/Cathey Creek, which is regulated by the Riparian Corridor overlay to the west of the site but not on the portion next to the site. The property also contains a number of wetlands, but they have not been identified as being within the Significant Wetlands overlay, so development can proceed in accordance with Oregon Department of State Lands/U.S. Army Corps of Engineers permitting and mitigation requirements. A copy of the wetland delineation report is attached as Exhibit 7. This standard does not apply.

6.280 Lands to Which These Regulations Apply.

The procedures and requirements of this section apply only to property that is within a Significant Natural Resource overlay district.

(A) *Riparian Corridor overlay district (/RC): The Riparian Corridor overlay district extends 50 feet upland from the Ordinary High Water mark, measured horizontally.*

If the Riparian Corridor overlay district area includes all or portions of a significant wetland, the district extends upland 50 feet, measured horizontally from the edge of the significant wetland. Fish-bearing waterways, and the significant wetlands associated with such waterways, are included in the Significant Wetland overlay district.

(1) *The Riparian Corridor boundary applies to the following Albany water resources (and in-stream lakes): [...]*

(B) *Significant Wetland overlay district (/SW): The Significant Wetland overlay district is comprised of fish-bearing waterways below the Ordinary High Water Mark, the wetlands associated with riparian corridors, and higher quality isolated significant wetlands, in the City's Local Wetland Inventory (LWI), and as amended through wetland delineations approved by the DSL and the ACE, if applicable. Notice to, and potentially permits from, DSL and ACE are still required for potential impact to all wetlands regulated by DSL or ACE.*

Response: The site shares its northern border with a segment of Cathey Creek. The creek is a tributary of the Calapooia River and Oak Creek. The riparian corridor (/RC) buffer extends from the protected Oak Creek up Cathey Creek (SW of site) to the northeast diagonally across residential areas until it terminates just after crossing Liberty Street and 28th Avenue. The City maps do not show any Local Wetlands Inventory, Riparian Overlay, or Habitat Overlay on the site.

However, the City's wetlands consultant delineated wetlands on large swaths of the site (see Exhibit 7), requiring the City to mitigate the development of this area by contributing funds to a mitigation bank.

This process is regulated by the Oregon Department of State Lands/U.S. Army Corps of Engineers rather than by the Albany Development Code. This standard does not apply.

Article 8 – Design Standards

Commercial and Institutional Site Design

8.310 Purpose.

These sections are intended to set threshold standards for quality design in new commercial, mixed-use, and institutional development. Good design results in buildings that are visually compatible with one another and adjacent neighborhoods and contribute to a commercial district that is attractive, active and safe. These qualities in turn contribute to the creation of commercial districts that facilitate easy pedestrian movement and a rich mixture of land uses.

Response: Care has been taken to design a community service building that contributes aesthetically to the urban fabric of the neighborhood in which it is placed. The design focuses on maximizing efficacy as a police station while also creating a landmark that will invigorate the community. This standard is met.

8.315 Applicability.

These standards apply to the design of new commercial, mixed-use, or institutional development and to the expansion of existing developments in any district.

Response: The development of the new police station is classified as a Community Service, thus falling within the “institutional development” designation that triggers compliance with the commercial and institutional site design standards. The following section of the narrative will discuss how the proposal satisfies the required design standards. This standard is met.

8.330 Building Orientation.

Building orientation and maximum setback standards are established to help create an attractive streetscape and pleasant pedestrian environment.

- (1) *New buildings shall be oriented to existing or new public streets. Building orientation is demonstrated by placing buildings and their public entrances close to streets so pedestrians have a direct and convenient route from the street sidewalk to building entrances.*
 - (a) *On sites smaller than three acres, new buildings shall be oriented to the public street/sidewalk and off-street parking shall be located to the side or rear of the building(s), except where it is not feasible due to limited or no street frontage, the site is an infill site less than one acre, conservation of natural resources, or where there are access restrictions.*
 - (b) *Buildings on sites larger than three acres may be setback from the public street and oriented to traffic aisles on private property, if the on-site circulation system is developed like a public street with pedestrian access, landscape strips and street trees.*
- (2) *Customer entrances should be clearly defined, highly visible, using features such as canopies, porticos, arcades, arches, wing walls, and planters.*

Response: As shown in the attached plans, the building will face Pacific Boulevard (Highway 99E). The front façade and main public entry will be enhanced with a pedestrian-scale plaza, bicycle parking, and decorative landscaping. As the subject site is larger than three acres, the building is set back from the street and faces the on-site drive aisle and parking lot, which is designed with pedestrian walkways and landscaping including numerous trees. As shown in the attached elevations (Exhibit 3 Sheet A3.1), the main entry will be defined with a vestibule, canopy, and glazing. This standard is met.

8.340 General Building Design.

New commercial buildings shall provide architectural relief and interest, with emphasis at building entrances and along sidewalks, to promote and enhance a comfortable pedestrian scale and orientation. Blank walls shall be avoided except when not feasible.

- (1) Ground floor windows shall be provided along frontages adjacent to sidewalks. The main front elevation(s) of buildings shall provide windows or transparency at the pedestrian level in the following minimum proportions:

District	% Transparency
RC, CC, NC, OP, MUC	25%
MS, LE, PB, ES, MUR, WF	50%
HB, CB	75%

The minimum window and door requirements are measured between 2 and 8 feet from the ground. Only the glass portion of doors may be used in the calculation. If there are upper floor windows, they shall continue the vertical and horizontal character of the ground level windows.

Response: Due to the site's location within the OP zone, twenty-five percent of the street elevation is required to be transparent between a height of 2 and 8 feet above ground. As shown in the attached elevation drawings, the front of the building will provide pedestrian-level transparency with an aluminum-framed storefront system, with transparency of approximately 546 SF or 35.5% of the 1,537 SF pedestrian-level area, exceeding the 25% minimum. The upper floor windows mirror the style and locations of those on the first floor to the extent possible. This standard is met.

- (2) Walls that are visible from a public street shall include a combination of architectural elements and features such as offsets, windows, entry treatments, wood siding, brick, stucco, synthetic stucco, textured concrete block, textured concrete), and landscaping.

Response: As shown in the attached elevation drawings, all walls of the building will include architecturally interesting features such as windows and varying building materials. The east and south elevations, most visible from public streets, will feature masonry veneer, structural masonry, and metal panel walls as well as window and storefront systems. Exhibit 3 Sheets L4.1- L4.4 illustrate the proposed landscaping that is intended to complement the building design. This standard is met.

8.350 Street Connectivity and Internal Circulation.

The following standards emphasize the importance of connections and circulation between uses and properties. The standards apply to both public and private streets.

- (1) New commercial buildings may be required to provide street or driveway stubs and reciprocal access easements to promote efficient circulation between uses and properties, and to promote connectivity and dispersal of traffic.

Response: As shown in the attached plans, access to the site will be available from the Willetta Street cul-de-sac to the south and to Pacific Boulevard (Highway 99E) to the east. The primary entrance will be on Pacific Boulevard. This standard is met.

- (2) The internal vehicle circulation system of a commercial development shall continue the adjacent public street pattern wherever possible and promote street connectivity. The vehicle circulation system shall mimic a traditional local street network and break the development into numerous smaller blocks.

Response: The site is located near the end of the Willetta Street right-of-way, bordered by Cathey Creek to the north, bordered by developed lots to the west, and bordered by Pacific Boulevard (Highway 99E) to the east. The street network is already set at this location. No additional connectivity is possible, although the internal vehicle circulation system is designed to connect to both the existing rights-of-way. This standard does not apply.

- (3) *Traffic aisles shall not be located between the building(s) and the sidewalk(s), except as provided in (4) below, or where drive-through windows are permitted, sites are constrained by natural resources, or are infill sites less than one acre.*

Response: In accordance with Section 8.330(1)(b), the building is oriented toward the traffic aisle and parking lot which have been designed in the manner of a street with pedestrian zones and landscaping. This standard does not apply.

- (4) *Where drop off facilities are provided, they shall be designed to meet the requirements of the American with Disabilities Act but still provide for direct pedestrian circulation.*

Response: No drop-off facilities are proposed. This standard does not apply.

- (5) *Internal roadways shall be designed to slow traffic speeds. This can be achieved by keeping road widths to a minimum, allowing parallel parking, and planting street trees to visually narrow the road.*

Response: Internal circulation within parking areas is designed to discourage aggressive driving and ensure a safe environment for pedestrian and motor vehicle operators. The use of landscaping and islands will create a perception of a pedestrian-focused environment while also creating a more aesthetically pleasing and environmentally friendly design. This standard is met.

8.360 Pedestrian Amenities.

Amenities such as awnings, seating, special paving and planters can have a dramatic [effect] on the pedestrian environment. Commercial developers should give as much thought to the pedestrian environment as they give to vehicle access, circulation and parking. The standards for pedestrian amenities are related to the scale of the development and also provide the flexibility for the developer to select the most appropriate amenities for the particular site and use.

- (1) *All new commercial structures and improvements to existing sites shall provide pedestrian amenities. The number of pedestrian amenities shall comply with the following sliding scale.*

REQUIRED PEDESTRIAN AMENITIES	
Size of Structure or Improvement	Number of Amenities
Less than 5,000 sf	1
5,000 – 10,000 sf	2
10,001 – 50,000 sf	3
More than 50,000 sf	4

- (2) *Acceptable pedestrian amenities include the following improvements. No more than two of any item may be used to fulfill the requirement:*

- (a) *Sidewalks at least 10 feet wide with ornamental treatments (e.g., brick pavers), or sidewalks that are 50 percent wider than required by the Code.*
- (b) *Benches or outdoor public seating for at least four people.*
- (c) *Sidewalk planter(s) enclosing a total of 8 square feet.*
- (d) *Pocket parks or decorative gardens (minimum usable area of 300 square feet).*
- (e) *Plazas (minimum usable area of 300 square feet).*

- (f) *Street trees 50 percent larger than required by the Code.*
- (g) *Weather protection (awnings, etc.).*
- (h) *Other pedestrian amenities that are not listed but are similar in scale and benefit.*
- (3) *Pervious pedestrian amenities can include approved vegetated post-construction stormwater quality improvements.*
- (4) *Pedestrian amenities shall meet the following standards:*
 - (a) *Amenities shall be located outside the building main entrance, along pedestrian corridors, or near transit stops. Amenities shall be visible and accessible to the general public from an improved public or private street. Access to pocket parks, plazas, and sidewalks must be provided by a public right-of-way or a public access easement.*
 - (b) *Amenities are not subject to setback requirements.*
 - (c) *Amenities are consistent with the character and scale of surrounding developments. For example, similarity in awning height, bench style, planter materials, street trees, and pavers is recommended to foster continuity in the design of pedestrian areas. Materials should be suitable for outdoor use, easily maintained, and have a reasonably long life cycle (e.g., 10 years before replacement).*

Response: The proposed building size of 41,749 SF requires the inclusion of three pedestrian amenities. As shown in the attached plans, the amenities provided are a public plaza, outdoor public seating (seat walls), a landscaped stormwater swale, and an entry canopy adjacent to the plaza. This standard is met.

8.370 Pedestrian Connections.

- (1) *New retail, office and institutional buildings at or near existing or planned transit stops shall provide convenient pedestrian access to transit.*

Response: The subject site is 0.1 miles from the transit stop at 24th Avenue SW and Highway 99E. As shown in the attached plans, the building will provide a direct pedestrian connection to the sidewalk on Highway 99E, providing convenient access to transit. This standard is met.

- (2) *Walkways shall be provided that connect building entrances and streets adjoining the site.*

Response: As shown in the attached plans (Exhibit 3 Sheet C2.1), walkways will be provided from the main building entrance to the adjacent streets. This standard is met.

- (3) *Pedestrian connections to adjoining properties shall be provided except where impractical. Pedestrian connections shall connect the on site circulation system to existing or proposed streets, walkways, and driveways that abut the property. When adjacent properties are undeveloped or have the potential to be redeveloped, streets, accessways and walkways on site shall be laid out or stubbed to allow for extension to the adjoining property.*

For the purposes of this section, "impractical" means where one or more of the following conditions exists:

- (a) *Physical or topographic conditions make a connection impracticable. Such conditions include but are not limited to freeways, railroads, steep slopes, wetlands or other bodies of water where a connection could not reasonably be provided;*
- (b) *Buildings or other existing development on adjacent land physically preclude a connection now or in the future considering the potential for redevelopment; or*
- (c) *Where streets or accessways would violate provisions of leases, easement, covenants, restrictions or other agreements existing as of May 1, 1995, which preclude a required street or accessway connection.*

Response: The subject site is surrounded by developed properties and rights-of-way. No connections to adjoining properties are practical. This standard does not apply.

- (4) *On sites at major transit stops provide the following:*
- (a) *Either locate buildings within 20 feet of the transit stop, a transit street or an intersecting street, or provide a pedestrian plaza at the transit stop or a street intersection;*
 - (b) *A reasonably direct pedestrian connection between the transit stop and building entrances on the site;*
 - (c) *A transit passenger landing pad accessible to disabled persons;*
 - (d) *An easement or dedication for a passenger shelter, if requested by the transit provider; and*
 - (e) *Lighting at the transit stop.*

Response: The subject site is not at a transit stop. This standard does not apply.

8.390 Compatibility Details.

Attention to detail can significantly increase the compatibility of commercial development with adjacent uses. Commercial development shall be designed to comply with the following applicable details and any other details warranted by the local conditions:

- (1) *On-site lighting is arranged so that light is reflected away from adjoining properties and/or streets.*

Response: As depicted on Exhibit 3 Sheet E1.1PH, all overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors. Light pollution will be mitigated by utilizing appropriate shielding on light fixtures and planting trees along the borders of the parking lots. This standard is met.

- (2) *Any undesirable impacts produced on the site, such as noise, glare, odors, dust, or vibrations have been adequately screened from adjacent properties.*

Response: The landscape plan (Exhibit 3 Sheets L4.1-L4.2) includes a variety of parking lot trees and shrubs lining the exterior of the parking lot/site with the intention of segregating the neighbors from any noise, glare, odors, and dust. Furthermore, the secure parking area will be surrounded by fencing. This standard is met.

- (3) *The site is protected from any undesirable impacts that are generated on abutting properties.*

Response: The site is bordered by residential and medical uses, Highway 99E, and Cathey Creek. These properties do not create undesirable impacts and no protections are needed. This standard does not apply.

- (4) *Unsightly exterior improvements and items such as trash receptacles, exterior vents and mechanical devices have been adequately screened.*

Response: As shown in the attached plans, trash receptacles will be stored in a concrete masonry unit (CMU) block trash enclosure south of the western parking area. Mechanical equipment will be located on the roof and will be blocked by parapets and mechanical screens, as shown in the attached elevations. This standard is met.

- (5) *Storage areas, trash collection facilities and noise generating equipment are located away from public streets, abutting residential districts or development, or sight obscuring fencing has been provided.*

Response: As shown in the attached plans, the trash area will be screened by a sight-obscuring CMU block wall. This standard is met.

- (6) *Where needed, loading facilities are provided on-site and are of sufficient size and number to adequately handle the delivery or shipping of goods or people. Where possible, loading areas should be designed so that vehicles enter and exit the site in a forward motion.*

Response: The proposed development is for a Community Services use. No loading area is needed or proposed. This standard does not apply.

Article 9 – On-Site Development and Environmental Standards

Off-Street Parking

9.020 Space Requirements.

Off-street parking and loading must be provided for all development in the amounts indicated in the table below subject to any applicable reductions permitted in this Article. All required parking must be developed in accordance with the standards in this Article.

- (1) *Calculating Floor Area for Parking. The area measured is the combined floor area of each level of a building exclusive of vent shafts, court yards, stairwells, elevator shafts, restrooms, storage rooms and rooms designed and used for the purpose of storage and operation of maintenance equipment, and covered or enclosed parking areas.*
- (2) *Employees. The number of employees shall include those working on the premises, plus proprietors, during the largest shift at peak season.*
- (3) *Fractional Space Requirements shall be counted to the nearest whole space; half spaces will be rounded up.*
- (4) *Unspecified Uses and Alternative Standards. When a use is not specifically listed in the Table 9-1: Parking Requirements, the Director will determine if the use is similar to a listed use in terms of parking needs. When a use is not similar to a use listed in Table 9-1 or the applicant has documentation that demonstrates a different parking demand, the Director may approve alternative parking standards. Acceptable documentation may include parking standards from other cities of similar size, company data on parking demand, parking demand studies, or the ITE Parking Generation Manual.*

Response: Table 9-1 (Parking Requirements) does not identify the minimum number of spaces required for police stations. No similar uses are listed. Therefore, the Planning Director may approve alternative parking standards. Parking standards from other cities of similar size are provided in Table 1 below.

TABLE 1: PARKING REQUIREMENTS IN CITIES OF SIMILAR SIZE		
City	Use Category	Ratio (parking spaces per unit)
St. Louis Park, MN	Police Station	1 per 1,000 SF
Bloomington, IN	Police Station	3 per 1,000 SF (1 per 333 SF) plus 1 per company vehicle
Racine, WI	Police Station	4 per 1,000 SF (1 per 250 SF)
Bend, OR	Government – point of service intended to serve the City	1 per 350 SF
Corvallis, OR	Public Safety Services	1 per 400 SF or 2 per bed
Keizer, OR	Government offices open to the public	1 per 500 SF
Tigard, OR	Emergency Services	3 per 1,000 SF (1 per 333 SF)

The first three cities presented above were used based on population size as listed in *Parking Standards*, a publication of the American Planning Association Planning Advisory Service in 2002. St. Louis Park, Bloomington, and Racine all have parking standards specifically for police stations. As shown, their ratios vary widely from 1 per 1,000 SF to 4 per 1,000 SF floor area (1 per 250 SF).

Also shown are standards for similar uses from four Oregon cities with similar populations per the 2014 city population estimates produced by the Population Research Center at Portland State University. Many other cities of similar size to Albany do not specify parking for similar civic uses. However, as shown above, in Oregon, parking for government/public safety services uses ranges from 1 space per 333 to 1 per 500 square feet.

The proposed building is 41,749 SF. Based on the figures in Table 1 above, the average ratio is approximately 1 space per 452 SF of gross floor area. To be conservative, the applicant proposes a standard of one parking space per 400 SF of gross floor area, which equates to a minimum of 105 spaces. As illustrated on Exhibit 3 Sheet C2.1, the proposed site plan would provide 32 parking spaces for public/visitor use and 135 secure spaces for police parking, totaling 167 spaces, which exceeds the proposed requirement. This standard is met.

9.025 Parking in the Public Right-of-Way.

Parking spaces in a public right-of-way may not be counted as fulfilling any part of the parking requirements except when permitted below. Any parallel parking spaces in the right-of-way that are counted toward fulfilling the parking requirements must be at least 25 feet long.

- (5) *Non-Residential Development. The amount of off-street parking required may be reduced by one space for every on-street space abutting the development for up to 25 percent of the minimum parking requirement, except when the development is proposed in a residential zoning district in Article 3 or in the ES (Elm Street) zone.*
- (a) *The on-street parking spaces must be at least 100 feet from a residential zoning district or the ES zoning district.*
- (b) *On-street parking credits can only be granted for developments with frontage on streets that allow parking on both sides and with approval from the Director of Public Works.*

Response: No parking in the public right-of-way is proposed. This standard does not apply.

9.120 Parking Area Improvement Standards.

All public or private parking areas, loading areas and outdoor vehicle sales areas must be improved based on the following standards:

- (1) *General. All parking spaces must be improved in accordance with these standards and available for use at the time of project completion.*
- (2) *Other Requirements. All parking areas shall conform to the setback, clear vision, landscaping, and buffering/screening provisions of this Code.*

Response: The proposed parking lot has been designed in compliance with applicable provisions of the Albany Development Code and shall be completed prior to the opening of the new police station. This standard is met.

- (3) *Surfacing. All required parking, including travel aisles and access, shall have a durable, dust-free surface of asphalt, cement concrete, or other materials approved by the Director. Parking lot surfacing shall not encroach upon the public right-of-way except when it abuts a concrete public*

sidewalk, or has been otherwise approved by the Director of Public Works. Pervious pavements, such as pervious asphalt or pervious concrete, may be allowed by the Director of Public Works.

Response: As described in the response to Section 9.020(4) above, based on the alternative parking demand data presented, 105 stalls are required for the proposed 41,749 SF building. As shown in the attached plans, all required and non-required striped parking spaces will be hard-surfaced with asphalt concrete over aggregate. This standard is met.

- (4) *Drainage. All parking lots must provide a drainage system to dispose of the runoff generated by the impervious surface. Post-construction stormwater quality facilities are required per Title 12 of the Albany Municipal Code when applicable. Provisions shall be made for the on-site collection of drainage water to eliminate sheet flow of such water onto sidewalks, public rights-of-way, and abutting private property. All drainage systems must be approved by the Director of Public Works.*

Response: As shown in the attached plans (Exhibit 3 Sheets C2.2 and C2.3), the paved areas will drain to stormwater facilities on the east and west sides of the site, with a portion also draining to the proposed public storm line running in the Willetta Street corridor (discharging to Cathey Creek). These facilities will meet City standards. This standard is met.

- (5) *Perimeter Curb. Perimeter curbing is required for protection of landscaped areas and pedestrian walkways, and to prevent runoff onto adjacent properties. All parking areas except those required in conjunction with a single- or two-family dwelling or approved overflow parking areas shall provide a curb at least 6 inches high along the perimeter of all parking areas. Exceptions may be allowed for connections to approved vegetated post-construction stormwater quality facilities.*

Response: As shown in the attached plans, on the northern-facing and internal parking areas, a continuous 6" curb will surround the parking stalls. Abutting the stormwater facilities, no curb will be provided per the exception above. This standard is met.

- (6) *Wheel Bumper. All parking stalls fronting a sidewalk, alleyway, street or property line, except for those required in conjunction with a single- or two-family dwelling, shall provide a secured wheel bumper at least 6 inches high and at least 6 feet long, set back from the front of the stall at least 2-1/2 feet, but no more than 3 feet. If the sidewalk is widened to 7 feet 6 inches to allow for vehicle encroachment, no wheel bumpers are required.*

Response: As shown in the attached plans, parking stalls fronting walkways narrower than 7'-6" will include 6" high, 6' long wheel stops. This standard is met.

- (7) *Turnaround. Except for single-family and duplex dwellings, groups of more than two parking spaces must be located and served by an aisle or turnaround so that their use will require no backing movements or other maneuvering in a street right-of-way other than an alley.*

Response: As shown in the attached plans, all stalls will be served by aisles and no backing into a street right-of-way will be required. This standard is met.

- (8) *Striping. Lots containing more than two parking spaces must have all required spaces permanently and clearly striped. Stripes must be at least four inches wide. When motorcycle parking, compact, or handicapped parking spaces are provided, they shall be designated within the stall.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the proposed parking spaces in the parking lots are separated by a 4-inch-wide stripe. Handicapped spaces are appropriately denoted with painted and signed designations. This standard is met.

- (9) *Connecting to Adjacent Parking Areas. Where an existing or proposed parking area is adjacent to a developed or undeveloped site within the same zoning district, any modifications to the parking areas must be designed to connect to the existing or future adjacent parking area. This requirement may be waived by the Director when it is deemed impractical or inappropriate due to the nature of the adjoining uses.*

Response: The proposed parking area abuts the Cathey Creek right-of-way to the north, Highway 99E to the east, and developed sites to the south and west. No future connections are expected. This standard does not apply.

- (10) *Parking Lot Landscaping. Parking lots shall be landscaped according to the standards in Section 9.150.*

Response: Compliance with this requirement is outlined in the response to Section 9.150. This standard is met.

- (11) *Compact Car Parking. No more than 40% of parking spaces provided may be designated for compact cars. Compact spaces must be signed and/or the space painted with the words "Compact Car Only."*

Response: No compact spaces are proposed as part of this development. This standard does not apply.

- (12) *Parking Accessible to the Disabled. All parking areas must provide accessible parking spaces in conformance with the Oregon Structural Specialty Code.*

Response: For parking areas with 151 to 200 spaces, the Oregon Structural Specialty Code requires six accessible spaces, one of which must be van accessible. As illustrated on Exhibit 3 Sheet C2.1, five standard accessible space and one van-accessible space is proposed for this facility. This standard is met.

- (13) *Bicycle Parking. Bicycle parking space requirements are as follows:*

- (a) *For multiple-family dwellings (three or more units) – one space per four units.*
- (b) *For industrial development – one space for every 10 automobile spaces required.*
- (c) *For commercial or office development - at least two spaces, plus one space for every 10 automobile spaces required.*
- (d) *Exemptions -- the Director may allow exemptions to or reductions in required bicycle spaces in connection with temporary uses or uses that are not likely to need bicycle parking.*

Bicycle parking spaces shall meet the following standards:

- (e) *Required spaces should be visible and not hidden, and must be located as near as possible to building entrances used by automobile occupants.*
- (f) *Each required bicycle parking space must have a parking rack securely fastened to the ground. Parking racks must support each bicycle at a minimum of two points, including at least one point on the frame, and must allow the frame and at least one wheel to be locked with a U-type lock.*
- (g) *Bicycle parking areas must provide at least 3 feet of clearance around all 3 sides of a fully-loaded bicycle rack and have an overhead clearance of at least 7 feet.*
- (h) *At least one-half of required bicycle parking spaces must be sheltered. Spaces must be protected from precipitation by a roof overhang or a separate roof at least 7 feet tall. Bicycle parking spaces within roofed buildings and bike lockers are considered sheltered spaces.*

Response: The closest category for this use is commercial or office development, so this project would need to provide at least two bicycle parking spaces, plus one space for every 10 required vehicle spaces. Based on the required 105 parking spaces, a total of 13 bicycle spaces are required. As illustrated on Exhibit 3 Sheets C2.1 and L2.1, parking for 8 bicycles has been provided east and west of the building,

with spaces for another 14 bicycles in the sally port. The public racks are proposed to be located so that the canopy provides weather protection. This standard is met.

(14) *Lighting. Any lights provided to illuminate any public or private parking area or vehicle sales area must be arranged to reflect the light away from any abutting or adjacent properties.*

Response: As shown in Exhibit 3 Sheet E1.1PH, parking lot lighting fixtures will be located to concentrate lighting around the parking areas and shielding will be provided as appropriate. This standard is met.

(15) *Pedestrian Access. Walkways and accessways shall be provided in all new off-street parking lots and additions to connect sidewalks adjacent to new development to the entrances of new buildings. All new public walkways and handicapped accessible parking spaces must meet the minimum requirements of the Oregon Structural Specialty Code.*

Response: As shown in the attached plans, walkways will be provided from the existing sidewalks on Willetta Street and Highway 99E to the building, as well as from the building to the parking areas. This standard is met.

(16) *When employee parking is designated in new developments, parking for carpools and vanpools shall be provided and located near the employee entrances to buildings.*

Response: As shown in the attached plans, carpool/vanpool parking will be located near the rear entry of the building. This standard is met.

9.130 Off-Street Parking Lot Design.

All off-street parking lots must be designed in accordance with City standards for stalls and aisles as set forth in Table 9-2: Parking Lot Design and supplemental drawings. Stall dimensions are measured from inside the stripes.

- (1) *Compact spaces shall be at least 8 feet wide by 16 feet long.*
- (2) *Accessible spaces shall be a minimum of 9 feet wide and 17 feet long and designed in accordance with the Oregon Structural Specialty Code (OSSC). An adjacent access aisle must be provided that is at least 8 feet wide and 17 feet long for a van-accessible space, and 6 feet wide for a standard accessible space.*
- (3) *Stall Width. Long-term parking spaces must be at least 8.5 feet wide. Parking stalls for grocery stores or adjacent to planter islands must be at least 9.5 feet wide.*
- (4) *Minimum Aisle Widths. Aisles for two-way traffic and emergency vehicle operations must be at least 24 feet wide. One-way aisles and one-way emergency vehicle access must be at least 20 feet wide.*
- (5) *The design of driveways and on-site maneuvering and loading areas for commercial and industrial developments shall include 20 feet of storage length for entering and exiting vehicles, in order to prevent vehicles from backing into the flow of traffic on the public street or causing unsafe conflicts with on-site circulation.*

A Parking Angle	B Stall Width	C Curb Width	D Aisle Width	E Stall Depth	F Bumper Overhang	G Dead-end Back-up
(Parallel)	8.0 feet	8.0 feet	N/A	25.0 feet	N/A	N/a
45	8.5	12.0	13.0	17.5	2.0	5.0
	9.0	12.7	12.0	17.5	2.0	5.0
	9.5	13.4	11.0	17.5	2.0	5.0
	10.0	14.1	11.0	17.5	2.0	5.0

60	8.5	9.8	18.0	19.0	2.5	5.0
	9.0	10.4	16.0	19.0	2.5	5.0
	9.5	11.0	15.0	19.0	2.5	5.0
	10.0	11.6	14.0	19.0	2.5	5.0
Compact	8.0 C	8.0 C	26.0 C	16.0 C	3.0	5.0
90	8.5	8.5	26.0	18.5	3.0	5.0
	9.0	9.0	26.0	18.5	3.0	5.0
	9.5	9.5	26.0	18.5	3.0	5.0
	10.0	10.0	24.0	18.5	3.0	5.0

Response: As shown in the attached plans, all parking spaces will be 90 degree and will meet the dimensional standards above. Stalls will be 9' or 10' wide, with 15.5' to 16.5' paved areas and 3' landscaped overhangs, and two-way parking lot aisles will be 26 feet wide. This standard is met.

Landscaping

9.140 General Requirements.

Landscaping requirements by type of use are listed below:

- (6) *Landscaping Required – Residential. All front setbacks (exclusive of accessways and other permitted intrusions) are required to be landscaped before an occupancy permit will be issued or final building permit approved. In all residential districts except Rural Residential (RR), the minimum landscaping acceptable for every 50 lineal feet of street frontage (or portion thereof, deducting the width of the driveway) is:*
- One tree at least 6 feet tall.*
 - Four 1-gallon shrubs or accent plants.*
 - The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, and evergreen shrubs).*

Response: No residential uses are proposed. This standard does not apply.

- (7) *Landscaping Required – Non-Residential. All required front and interior setbacks, exclusive of accessways and other permitted intrusions, must be landscaped before an occupancy permit will be issued. Minimum landscaping acceptable for every 1,000 square feet of required setbacks in all commercial industrial districts is as follows:*
- One tree at least 6 feet tall for every 30 feet of street frontage.*
 - Five 5-gallon or eight 1-gallon shrubs, trees or accent plants.*
 - The remaining area treated with suitable living ground cover, lawn, or decorative treatment of bark, rock, or other attractive ground cover.*
 - When the yard adjacent to a street of an industrially zoned property is across a right-of-way from other industrially or commercially zoned property, only 30 percent of such setback area must be landscaped.*

Response: The subject site has 377' of frontage in the OP zone. Thus, 13 trees are required. As shown in the attached plans (see Exhibit 3 Sheets L4.0-L4.4), over 25 trees will be provided in the front setback area. The required setback area, including the 5' interior setback and 10' front setback areas, is 6,725 SF. Thus, the minimum 5-gallon shrubs required are 5 per 1,000 SF, or 34. As shown in the attached plans, upwards of 100 5-gallon shrubs will be planted in the required setback area, and the remaining areas of the proposed improvements will be treated with ground cover. This standard is met.

- (3) *Alternate Plan – Non-Residential - The Director may approve placement of the required setback landscaping in public right-of-way when the following conditions are met:*

- (a) *The site contains existing development that includes substantial building(s), and is subject to ADC improvement requirements due to a change of use or vacancy; and*
- (b) *The appropriate government agency grants written permission for use of the right-of-way; and*
- (c) *The applicant provides written assurance that on-site setback landscaping will be installed within 90 days in the event permission to use the right-of-way is revoked; and*
- (d) *The Director finds that the required setback landscaping can feasibly be installed on the property without creating other violations of this Code; and*
- (e) *The Director finds that providing the landscaping in the public right-of-way in the interim fulfills the intent this Code for high quality development (9.010) and the minimum landscaping requirements in Section 9.140(2)(a) through (c).*

Response: The applicant is not requesting that landscaping in the right-of-way satisfy the Development Code requirements for setback plantings. This standard does not apply.

9.150 Parking Lot Landscaping.

The purpose of landscaping in parking lots is to provide shade, reduce stormwater runoff, and direct traffic. Incorporation of approved vegetated post-construction stormwater quality facilities in landscaped areas is encouraged. Parking lots must be landscaped in accordance with the following minimum standards:

- (1) *Planter Bays. Parking areas shall be divided into bays of not more than 12 parking spaces. At both ends of each parking bay there shall be curbed planters at least 5 feet wide, excluding the curb. Gaps in the curb may be allowed for connections to approved post-construction stormwater quality facilities. Each planter shall contain one canopy tree at least 10 feet high and decorative ground cover containing at least two shrubs for every 100 square feet of landscape area. Neither planter bays nor their contents may impede access on required public sidewalks or paths, or handicapped-accessible parking spaces.*

Response: As shown in the attached plans, the parking area is divided into bays of no more than nine spaces, with curbed planters at every end. Each island will contain a canopy tree and numerous shrubs. No bays or contents impede access to required public sidewalks or ADA parking spaces. This standard is met.

- (2) *Entryway Landscaping. Both sides of a parking lot entrance shall be bordered by a minimum 5-foot-wide landscape planter strip meeting the same landscaping provisions as planter bays, except that no sight-obscuring trees or shrubs are permitted.*

Response: As shown in the attached plans, parking lot entrances will be bordered with wide planter strips with compliant plantings and ground cover. This standard is met.

- (3) *Parking Space Buffers. Parking areas shall be separated from the exterior wall of a structure by pedestrian walkways or loading areas or by a 5-foot strip of landscaping materials.*

Response: As shown in the attached plans, all parking spaces will be separated from the building by at least 7' of walkway or other buffers. This standard is met.

- (4) *Alternate Plan. An alternate plan may be submitted that provides landscaping of at least five percent of the total parking area exclusive of required landscaped yard areas and that separates parking areas of more than 100 spaces into clusters divided by landscape strips. Each planter area shall contain 1 tree at least 10 feet tall and decorative ground cover containing at least 2 shrubs for every 100 square feet of landscape area. Landscaping may not impede access on required public sidewalks or paths, or handicapped-accessible parking spaces.*

Response: No alternate plan is proposed. This standard does not apply.

- (5) *Landscape Protection. Required landscaped areas adjacent to graveled areas must be protected, either by railroad ties secured by rebar driven 18 inches into the ground, by large boulders, or by another acceptable means of protection.*

Response: No graveled areas are proposed. This standard does not apply.

9.160 Irrigation of Required Landscaping.

All required landscaped areas must be provided with an irrigation system unless a licensed landscape architect, landscape construction professional or certified nurseryman submits written verification that the proposed plants do not require irrigation. Irrigation systems installed in the public right-of-way require an encroachment permit.

Response: Exhibit 3 Sheet L3.1 illustrates the irrigation requirements for all of the landscaped areas. The landscaped area and the stormwater treatment areas will be irrigated using rotary spray nozzles. Temporary irrigation will be provided to help establish plantings in the Highway 99E right-of-way abutting. Final design of the irrigation system would be performed at the time of permitting. This standard is met.

9.170 Identification of Existing Trees.

In all proposed developments, existing trees over 25 inches in circumference (8 inches in diameter) as measured 4.5 feet above mean ground level from the base of the trunk shall be noted on all development plans, with notations indicating whether they are to be removed or utilized in the development. To obtain the circumference of a tree with multiple trunks, add the individual trunk circumferences, which are greater than 6 inches in circumference. Clusters of trees in open space and floodplain areas may be noted in approximate locations.

Response: The required information is contained with Exhibit 3 Sheet C2.1. This standard is met.

Tree Protection

9.207 Applicability.

Site Plan Review approval is required for the felling of 5 or more trees larger than 25 inches in circumference (approximately 8 inches in diameter) on a lot or property in contiguous single ownership in excess of 20,000 square feet in any zone.

The following activities are exempt from site plan review if they meet the applicable requirements of the Significant Natural Resource overlay districts in Article 6:

- (1) *The action of any City official or of any public utility necessary to remove or alleviate an immediate danger to life or property; to restore utility service or to reopen a public street to traffic.*
- (2) *Felling of any tree that is defined as a nuisance under the Albany Municipal Code.*
- (3) *Any felling necessary to maintain streets or public or private utilities within a public right-of-way or utility easement provided the Tree Commission or City Forester approved the proposed tree felling.*
- (4) *Felling of trees planted as Christmas trees.*
- (5) *Felling of trees on property under a Forest Stewardship Plan approved by the Oregon Department of Forestry.*

Response: As shown in the attached plans, the proposed development will result in the removal of four trees on site in excess of 8 inches in diameter. Since this is fewer than five trees, the proposed tree felling is not subject to review under Section 9.208. This standard does not apply.

9.208 Tree Felling Criteria.

The following review criteria replace the Site Plan Review criteria found elsewhere in this code for the purpose of reviewing tree felling. A Site Plan Review for tree felling will be processed as a Type I-L land use decision.

- (1) *The Community Development Director or his/her designee shall approve a Site Plan Review for tree felling when the applicant demonstrates that the felling of the tree(s) is warranted because of the condition of the tree(s) with respect to disease, hazardous or unsafe conditions, danger of falling, proximity to existing structures or proposed construction, or interference with utility services or pedestrian or vehicular safety. The Director, in consultation with the City Arborist, may also grant an exception to any of the tree cutting standards for industrial development on industrially zoned land. The Director may require the applicant to provide a Certified Arborist's report.*
- (2) *For property where a site plan review, conditional use or land division application has been approved or is currently under review, the Community Development Director, City Forester, or his/her designee shall approve site plan review when the applicant demonstrates that all of the following review criteria are met:*
 - (a) *It is necessary to fell tree(s) in order to construct proposed improvements in accordance with an approved site plan review or conditional use review, or to otherwise utilize the applicant's property in a manner consistent with its zoning, this code, applicable plans adopted by the City Council, or a logging permit issued by the Oregon Department of Forestry.*
 - (b) *The proposed felling is consistent with State standards, City ordinances, and the proposed felling does not negatively impact the environmental quality of the area, including but not limited to: the protection of nearby trees and windbreaks; wildlife; erosion; soil retention and stability; volume of surface runoff and water quality of streams; scenic quality, and geological sites.*
 - (c) *The uniqueness, size, maturity, structure, and historic value of the trees have been considered and all other options for tree preservation have been exhausted. The Director may require that trees determined to be unique in species, size, maturity, structure, or historic values are preserved.*
 - (d) *Tree felling in Significant Natural Resource Overlay Districts meets the applicable requirements in Article 6.*

Response: As illustrated on Exhibit 3 Sheet C2.1, removal of multiple trees is required to construct the proposed police station, parking areas, stormwater management facilities, and required landscaping. Due to the site's location within the heart of the City, it is not anticipated that tree removal would negatively impact environmental quality. The site is not within a Significant Natural Resource Overlay District. This standard is met.

- (4) *The Director may attach conditions to the approval of the tree felling permit to ensure the replacement of trees and landscape or otherwise reduce the effects of the felling, and may require an improvement assurance to ensure all conditions are met.*
- (5) *Precautions shall be made to protect residual trees and tree roots from damaging agents during and after the removal process. The following tree protection specifications should be followed to the maximum extent feasible for all projects with protected existing trees.*

- (a) *Within the drip line of any protected existing tree, there shall be no cut or fill over a four-inch depth unless a qualified arborist or forester has evaluated and approved the disturbance.*
- (b) *Prior to and during construction, an orange fence shall be erected around all protected existing trees that is a minimum of 4 feet tall, secured with metal T-posts, no closer than 6 feet from the trunk or within the drip line, whichever is greater. There shall be no storage or movement of equipment, material, debris or fill within the fenced tree protection zone.*
- (c) *During the construction stage of development, the applicant shall prevent the cleaning of equipment or material or the storage and disposal of waste material such as paints, oils, solvents, asphalt, concrete, motor oil or any other material harmful to the life of a tree within the drip line of any protected tree or group of trees.*
- (d) *No damaging attachment, wires, signs or permits may be fastened to any protected tree.*
- (e) *Large property areas containing protected trees and separated from construction or land clearing areas, road rights-of-way and utility easements may be "ribboned off," rather than erecting protective fencing around each tree as required in subsection (5)(b) above. This may be accomplished by placing metal t-post stakes a maximum of 50 feet apart and tying ribbon or rope from stake-to-stake along the outside perimeters of such areas being cleared.*
- (f) *The installation of utilities, irrigation lines or any underground fixture requiring excavation deeper than 6 inches shall be accomplished by boring under the root system of protected existing trees at a minimum depth of 24 inches. The auger distance is established from the face of the tree (outer bark) and is scaled from tree diameter at breast height as described in Table 9-3 below.*

Tree Diameter at Breast Height (inches)	Auger Distance from Face of Tree (feet)
8-9	5
10-14	10
15-19	12
Over 19	15

Response: The proposed plantings illustrated on Exhibit 3 Sheets L4.1 and L4.2 more than compensate for the trees proposed for removal. As all trees are proposed to be removed, there is no need for protection measures such as those outlined above. This standard is met.

Buffering and Screening

9.210 General Requirements/Matrix.

In order to reduce the impacts on adjacent uses of a different type, buffering and screening is required in accordance with the matrix that follows Section 9.300. The property owner of each proposed development is responsible for the installation and maintenance of such buffers and screens. The Director may waive the buffering/screening requirements of this section where such has been provided on the adjoining property in conformance with this Code. Where a use would be abutting another use except for separation by right-of-way, buffering (but not screening) shall be required as specified in the matrix. Where a proposed use abuts undeveloped property, only one half of the buffer width shall be required.

TABLE 9-4: BUFFER AND SCREENING MATRIX	
Abutting Use or District	Proposed Use
	Commercial/professional or mixed-use
Dwellings in RM and RS-6.5	10' S
Any arterial street	10'
Commercial or professional uses, or commercial and mixed use districts	0'
Any parking lot with at least 5 spaces	0'

"S" indicates screening required.

The buffer/screening standard does not apply along arterial streets where it conflicts with other provisions of this code.

Response: Albany Development Code Table 9-4 (Buffer and Screening Matrix) dictates that a 10' buffer with screening is required when abutting dwellings. This is applicable on the western and southern portions of the site that abut houses in the RM and RS-6.5 zones. A 10' buffer (but no screening) is required on Pacific Boulevard since it is an arterial street. No other buffering or screening is required. The standards of this section are addressed below.

9.240 Buffering.

The minimum improvements within a buffer area consist of the following:

- (1) *At least one row of trees. These trees will be not less than 10 feet high at time of planting for deciduous trees and spaced not more than 30 feet apart and 5 feet high at time of planting for evergreen trees and spaced not more than 15 feet apart. This requirement may be waived by the Director when it can be demonstrated that such trees would conflict with other purposes of this Code (e.g. solar access).*
- (2) *At least five 5-gallon shrubs or ten 1-gallon shrubs for each 1,000 square feet of required buffer area.*
- (3) *The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, evergreen shrubs).*

Response: As shown in the attached plans (see Exhibit 3 Sheets L4.0 through L4.4), a perimeter of one row of parking lot trees will be on the north and south lot lines. Perimeters of two rows of trees will be provided on the eastern lot line and of three rows of trees will be provided on the western lot line. In addition, an 8' high fence will be provided around the site boundary on the north, west, and southwestern lot lines. More than five 5-gallon shrubs will be provided for each 1,000 SF of buffer area. All other landscape area will be covered in ground cover. This standard is met.

9.250 Screening.

Where screening is required or provided, the following standards apply in addition to conditions (1) and (3) above:

- (1) *One row of evergreen shrubs that will grow to form a continuous hedge at least 4 feet tall within two years of planting, or*
- (2) *A fence or masonry wall at least 5 feet tall constructed to provide a uniform sight-obscuring screen, or*
- (3) *An earth berm combined with evergreen plantings or a fence that forms a sight and noise buffer at least 6 feet tall within two years of installation.*

Response: As shown in the attached landscape plans, a continuous evergreen hedge will surround the parking area. In addition, an 8' high wall will be provided around the site boundary on the north, west,

and southwestern lot lines, satisfying the requirement to screen adjoining residences. This standard is met.

9.260 Clear Vision.

Buffering and screening provisions are superseded by the clear vision requirements of Section 12.180 and by the fence and wall height restrictions of the zone when applicable.

Response: Exhibit 3 Sheet L4.0 illustrates the required clear vision areas at the driveway entrances and the landscaping has been selected to comply with the applicable standards. This standard is met.

Fences

9.380 Standards.

Fences and walls shall meet the following standards. If a fence or wall is used to meet required screening, it shall meet the provisions in Section 9.385.

Standards in Commercial, Industrial, HD, CB, ES, LE, MS, PB, WF, and TD zones:

- (4) *Fences in front setbacks. Fences shall be no taller than 6 feet in required front setbacks. 6-foot fences containing barbed wire on top or fences taller than 6 feet are not permitted in the front setback.*

Response: As illustrated in Exhibit 3, no fencing is proposed in the front setback. This standard does not apply.

Article 11 – Land Divisions and Planned Developments

Subdivisions and Partitions

11.180 Tentative Plat Review Criteria.

Approval of a tentative subdivision or partition plat will be granted if the review body finds that the applicant has met all of the following criteria which apply to the development:

- (1) *Development of any remainder of property under the same ownership can be accomplished in accordance with this Code.*

Response: As illustrated on Exhibit 3 Sheet C2.4, the replat would combine two parcels, plus the vacated portions of Willetta Street and Cathey Creek, into one parcel under the same ownership. The proposed development would utilize the entire site and would also dedicate the Willetta Street cul-de-sac bulb. This standard is met.

- (2) *Adjoining land can be developed or is provided access that will allow its development in accordance with this Code.*

Response: The adjoining land is currently developed (except Cathey Creek to the north) and has access to existing rights-of-way so the proposed vacation area is unnecessary for development of adjoining lots. Combining multiple parcels into one lot allows for construction of the proposed police station. This standard is met.

- (3) *The proposed street plan affords the best economic, safe, and efficient circulation of traffic possible under the circumstances.*

Response: No new streets are proposed. The cul-de-sac will serve the subject site and other properties on Willetta Street, and will provide the best possible circulation given the dead-end setting. Combining multiple parcels into one lot allows for construction of the proposed police station. This standard is met.

(4) *The location and design allows development to be conveniently served by various public utilities.*

Response: As shown in the attached plans, the subject site will be served by public utilities. The replat would accommodate and provide easements for the existing public water and stormwater facilities on site. This standard is met.

(5) *Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.*

Response: The subject site is not in a special purpose district. This standard does not apply.

Article 12 – Public Improvements

Streets

12.060 General Provisions.

No development may occur unless it has frontage on or approved access to a public street currently open to traffic. A currently non-open public right-of-way may be opened by improving it to City standards.

Response: The subject site has frontage on Highway 99E and Willetta Street. This standard is met.

Streets shall be connected to reduce travel distance, provide multiple travel routes, and promote the use of alternative modes. Street patterns have a greater long-range effect on land use patterns, than do parcel patterns or building location.

Response: The site is located near the end of the Willetta Street right-of-way, bordered by Cathey Creek to the north, developed lots to the west, and Pacific Boulevard (Highway 99E) to the east. The street network is already set at this location. This standard does not apply.

Streets (including alleys) within and adjacent to a development shall be improved in accordance with the standards in this Article. In addition, any new street or additional street width planned as a portion of an approved street plan shall be dedicated and improved in accordance with this Article.

Response: This proposal will include improvement of the cul-de-sac at Willetta Street. This standard is met.

When the City Engineer determines that a required street improvement would not be timely, the City Engineer may accept a Petition for Improvement/Waiver of Remonstrance for a future assessment district.

Response: No remonstrance is requested. This standard does not apply.

The City Engineer may approve adjustments to the required street right-of-way and planter widths when necessary to accommodate approved street-side post-construction stormwater quality facilities.

Response: No adjustment is requested. This standard does not apply.

12.070 Creation of Streets.

Streets are usually created by approval of a subdivision or partition plat. However, the City Council may also approve creation of a street by acceptance of a deed. If creating a street unintentionally results in a land partition, the owner is not required to apply for partition approval as long as the resulting parcels comply with Code standards.

Response: The site is located near the end of the Willetta Street right-of-way and is bordered by Pacific Boulevard (Highway 99E) to the east. The street network is already set at this location. The proposed vacation would eliminate the existing Willetta Street right-of-way and the proposed replat would dedicate the Willetta Street cul-de-sac bulb. This standard is met.

12.100 Access to Public Streets.

With the exceptions noted in Section 1.070, the location and improvement of an access point onto a public street shall be included in the review of a development proposal. In addition, the following specific requirements shall apply to all access points, curb cuts, and driveways:

- (1) *Approaches and driveways to City streets and alleys must be paved and constructed in accordance with the Standard Construction Specifications. Driveways serving more than one property shall be paved the full length of the shared portion.*

Response: As shown in the attached plans, paved driveway access points are proposed on the Willetta Street cul-de-sac and onto Highway 99E. This standard is met.

- (2) *Driveways for single- and two-family dwellings must have a minimum width of 10 feet and a maximum width of 24 feet (not to exceed the width of the driveway curb cut) and minimum separation of 5 feet.*

Up to four multiple-family units that front on a public street may have separate driveways. The driveways shall meet the same standards as for single- and two-family dwellings.

Driveways for all other uses must have widths of 12-16 feet for one-lane (one-way) driveways, 24-32 feet for two-lane driveways, and 36 feet for three-lane driveways. Three-lane driveways must have designated lanes and turning movements. Industrial driveways shall have a width of 24-48 feet. There must be a minimum separation of 22 feet between all driveways except for single- and two-family dwellings. The width of a driveway will be determined by measuring at the curb line and will exclude the transitions which must conform to standards fixed by the City Engineer.

Response: This proposal is for a public building and use. As shown in the attached plans, the driveways will be two-way and will be a minimum of 24' wide. This standard is met.

- (3) *All driveways must be located as far as practical from a street intersection, and in no instance shall the distance from an intersection be less than the following, as measured from the nearest curb return radius:*

Arterial Street 40 feet

Collector Street 20 feet

Local Street 10 feet

At intersections with bulbouts or post-construction stormwater quality curb extensions incorporated into the curb return the measurement will be made from the nearest curb return radius. When different classes of streets intersect, the distance required is between an access point and the intersection of the street type that requires the greater distance.

Response: The subject site is located on Pacific Boulevard/Highway 99E, a Principal Arterial, and Willetta Street, a local street. The proposed driveway onto Highway 99E will be approximately 460 feet from the nearest arterial street intersection (at 24th Avenue SW) and approximately 540 feet from the nearest local street intersection (at 29th Avenue SW). This standard is met.

- (4) *The location, width, and number of accesses to a public street may be limited for developments that are subject to site plan review. All development that proposes access to an arterial street is subject to site plan review and the design requirements of 12.230.*

Response: This process is understood. Section 12.230 is addressed in this narrative. This standard is met.

(5) *Access points to a public street shall be the minimum necessary to provide reasonable access while not inhibiting the safe circulation and carrying capacity of the street.*

Response: Only one access point is proposed to each public street. This standard is met.

(6) *Properties with frontage on more than one street may be restricted to access on the street(s) of a lower classification through site plan, land division, or other review procedures.*

Response: This potential limitation is understood. Due to the need to provide access to Pacific Boulevard and limit site traffic on Willetta Street, the applicant requests a primary entrance on Pacific Boulevard. This standard is met.

(7) *A common access point at a property line is encouraged and may be required in order to reduce the number of access points to streets. Construction of common access points must be preceded by recording of joint access and maintenance easements.*

Response: This potential requirement is understood.

(8) *With the exception of single-family residential development, approach grades must not exceed 10 percent slope within 20 feet of a public street. Driveways for single-family residential development shall comply with applicable fire and building codes.*

Response: As shown in the attached grading plan (Exhibit 3 Sheet C2.2), the slopes within 20' of the public streets will be no more than 5.7%. This standard is met.

(9) *Access to designated state highways is subject to the provisions of this Article in addition to requirements of the State Highway Division and State Department of Transportation. When regulations of the City and State conflict, the more restrictive requirements apply.*

Response: This relationship is understood. The applicant will apply for ODOT permits for the proposed driveway on Highway 99E. This standard is met.

(10) *For developments on property larger than five acres in contiguous ownership fronting on an arterial street or limited access highway, a frontage road may be required in order to provide a single access determined by the review body to be the most appropriate location for safety and convenience.*

Response: The subject site is less than five acres. This standard does not apply.

(11) *When access is allowed on an arterial street, efforts shall be made to locate it adjacent to the interior property line where it could be shared by the adjacent property.*

Response: Access is proposed on Highway 99E, a Principal Arterial, and Willetta Street, a local street. The driveway onto Highway 99E cannot be shared with the northern property, as it is separated by Cathey Creek. This standard does not apply.

12.110 Street Location, Width and Grade.

The location, width, and grade of all streets must conform to any approved transportation master plan or recorded subdivision plat. When location of a street is not shown in an approved street plan, the arrangement of streets in a development shall either provide for the continuation or appropriate projection of existing principal streets in the surrounding areas or conform to a plan for the neighborhood approved or adopted by the City to meet a particular situation where topographical or other conditions made continuance of or conformance to existing streets impractical or where no plan has been previously adopted.

In addition, new streets may be required to be located where the City Engineer determines that additional access is needed to relieve or avoid access deficiencies on adjacent or nearby properties. In

determining the location of new streets in a development or street plan, consideration shall be given to maximizing available solar access for adjoining development sites.

Street grades must be approved by the City Engineer, who will consider drainage and traffic safety.

Response: No changes are proposed to the existing street locations or grades. This standard does not apply.

12.120 Rights-of-Way and Roadway Widths.

Unless otherwise indicated on an approved street plan or in Section 12.130, the street right-of-way and roadway widths shall not be less than the minimum shown below in Table 12-1. Where a range is indicated, the width shall be determined by the City Engineer. Reductions in roadway width may be allowed to facilitate curb extensions required for approved street side post-construction stormwater quality facilities in the landscape strip/planter area. Reductions require approval of the City Engineer.

TABLE 8-1: STREET RIGHT-OF-WAY AND ROADWAY WIDTH REQUIREMENTS		
Type of Street	Minimum Rights-of-Way Width	Minimum Roadway Width
Arterial	70-120 feet	40-70 feet
Collector	60-80 feet	36-48 feet
Local*	42-56 feet	22-32 feet
Radius for turnaround at end of cul-de-sac	43 feet	36 feet
Alley	14-20 feet	12-20 feet

*When street rights-of-way are less than 60 feet wide, a parallel public utility easement 7-foot-wide shall be dedicated on both sides of the right-of-way unless waived by the City Engineer.

Response: No changes are proposed to the existing right-of-way widths. This standard does not apply.

12.140 Additional Rights-of-Way.

A development project requiring land use approval is required to dedicate additional right-of-way if an existing street abutting or within the development does not meet the widths designated in Section 12.120. This provision does not apply to property line adjustments or historic review. While not required to dedicate additional right-of-way, single- and two-family dwellings (and related accessory buildings) are subject to setbacks from future street rights-of-way as provided in Section 3.190.

Response: The existing rights-of-way meet the designated standards. This standard does not apply.

12.150 Future Extensions of Streets.

When it is necessary to give access to or permit a future division of adjoining land, streets shall be extended to the adjoining tract. A barricade at the end of the street shall be installed and paid for by the property owners. It shall not be removed until authorized by the City Engineer.

Response: No access to or division of adjoining land is required. This standard does not apply.

12.160 Street Alignment.

As far as practical, streets shall be dedicated and constructed in alignment with existing streets. Arterial and collector streets shall have continuous alignments without offset or staggered intersections. In no case shall streets be designed so that jogs of less than 300 feet are created as measured from the centerline of any intersection involving an arterial or collector street.

Response: The site is located near the end of the Willetta Street right-of-way, bordered by Cathey Creek to the north, developed lots to the west, and Pacific Boulevard (Highway 99E) to the east. The street network is already set at this location. This standard does not apply.

12.170 Intersections.

Streets must intersect as nearly as possible at right angles. Proposed intersection of two streets at an acute angle of less than 75 degrees is not allowed. An oblique street should be curved approaching an intersection to provide at least 100 feet of street at right angles with the intersection. Not more than two streets shall intersect at any one point.

Response: The street network is already set at this location. This standard does not apply.

12.180 Clear Vision Area.

A clear vision area must be maintained at each access to a public street and on each corner of property at the intersection of two streets or a street and a railroad. No fence, wall, hedge, sign, or other planting or structure that would impede visibility between the heights of 2 and 8 feet shall be established in the clear vision area. Visibility is not considered impeded by a fence where materials are 35 percent or less of the surface area of that portion of the fence above 2 feet. Fence posts spaced at 8 feet or more apart are not counted as part of the fence surface area. Height measurements shall be made from the top of the curb or, when no curb exists, from the established street center line grade.

- (1) *The clear vision area provisions do not apply to the following:*
 - (a) *a public utility pole,*
 - (b) *a tree trimmed (to the trunk) to a line at least eight feet above the level of the intersection,*
 - (c) *another plant species of open growth habit that is not planted in the form of a hedge and that is planted and trimmed to leave at all seasons a clear and unobstructed cross-view,*
 - (d) *a supporting member or appurtenance to a permanent building lawfully existing on the date this standard becomes effective,*
 - (e) *an official warning sign or signal,*
 - (f) *the post section of a pole sign when there are no more than two posts and any post is less than eight inches in diameter, and*
 - (g) *existing or new buildings that meet the minimum setbacks.*
- (2) *A clear vision area consists of a triangular area, two sides of which are lot lines or a driveway and a lot line for a distance specified in this section, or, where the lot lines have rounded corners, the lot lines extended in a straight line to a point of intersection and so measured, and the third side of which is a line across the corner of the lot joining the non-intersecting ends of the other two sides (See illustration below, Figure 6). The measurements in Table 12-4 below shall establish the clear vision areas:*

TABLE 12-4: MEASUREMENTS FOR ESTABLISHING CLEAR VISION AREAS	
Type of Intersection	Measurement Along Each Lot Line or Drive Edge*
Controlled Intersection (stop sign or signal)	20 feet
Uncontrolled Intersection	30 feet
Commercial and Industrial District driveways	20 feet
Residential District driveways	15 feet
Alley (less than 25 feet)	20 feet

*When two or more streets of different right-of-way width intersect, the distance to be measured along the lot lines shall be the distance specified for each type street.

Response: Exhibit 3 Sheet L4.0 illustrates the required clear vision areas at the driveway entrances. No obstacles are proposed within the regulated heights (2-8' above grade) and the landscaping has been selected to comply with the applicable standards. This standard is met.

12.190 Cul-de-sacs.

The street pattern may include cul-de-sacs and bulbs only if connectivity and block length standards have been met. A cul-de-sac must not exceed 400 feet. A cul-de-sac must end in a circular turnaround, except as provided in 12.130 (4)(c). Dead-end streets longer than 400 feet may be approved by the City Engineer if no other means is available for development of the property.

A 10-foot-wide lighted concrete bikeway/pedestrian accessway shall be dedicated and constructed from the end of each cul-de-sac to the nearest street or property line of adjacent property, except where the cul-de-sac abuts developed property and/or the City Engineer determines there is no need for a connection.

Response: The proposed development includes the improvement of an existing dead end street with a new cul-de-sac. No accessway is necessary. The cul-de-sac will end in a circular turnaround. This standard is met.

12.200 Street Abutting New Development.

Sections of existing streets that directly abut a new development and do not meet City standards shall be constructed to City standards. The City Engineer may approve construction of a partial-width street, provided the design is determined to be adequate to accommodate needed public facilities, storm drainage runoff, traffic volumes, and traffic loadings. The design of the improvement shall consider the ultimate design of the fully widened street. For purposes of this section, "development" means a land division, new commercial or industrial development, construction of multi-family residential units, or a manufactured home or recreational vehicle park.

A future improvement assurance, as described in Section 12.600, may be accepted by the City when the City Engineer determines that the street improvement would not be timely.

Response: The existing street stub of Willetta Street will be improved to current standards with the cul-de-sac. This standard is met.

12.210 Slope and Curves.

Slope shall not exceed 6 percent on arterials, 10 percent on collector streets, or 12 percent on other streets. Center line radii or curves shall be not less than 600 feet on arterials, 400 feet on collectors, or 200 feet on other streets. When existing conditions, particularly the topography, make it otherwise impractical to provide buildable sites, steeper grades and sharper curves may be approved by the review body. In flat areas, allowance shall be made for finished street grades having a minimum slope of at least 0.5 percent, when possible.

Response: No changes to the existing street system are proposed. This standard does not apply.

12.220 Street Adjacent to Railroad.

Whenever a proposed development contains or is adjacent to a railroad right-of-way, provision may be required for a street approximately parallel to and on each side of the railroad right-of-way at a distance suitable for appropriate use of the land between each street and the railroad. The distance shall be determined with consideration at each cross street of the minimum distance required for approach

grades to a future grade separation and to provide sufficient depth to allow screen planting along the railroad right-of-way in non-industrial areas.

Response: The subject site is not adjacent to a railroad. This standard does not apply.

12.230 Access to Arterials.

When a development abuts or contains an existing or proposed arterial street, the development design shall provide adequate protection for residential properties and shall separate residential access and through traffic; or if separation is not feasible, the design shall minimize the traffic conflicts. The design requirements may include any of the following:

- (1) A parallel access street along the arterial;
- (2) Lots abutting the arterial of suitable depth to provide adequate buffering and having frontage along another street;
- (3) Screen planting at the rear or side property line to be contained in a non-access reservation along the arterial; or
- (4) Other treatment, as determined by the Director, suitable to meet the objectives of this subsection.

Response: This standard is primarily intended to protect new residences from the traffic effects from an arterial. Since no residences are proposed for this development, this standard does not apply.

12.260 Traffic Signals.

Where a single development or concurrent group of developments will create a need for a traffic signal at an intersection, such installation may be a condition of development approval.

Response: Feedback from the City Engineer and ODOT staff has indicated that no traffic signal is required. This standard does not apply.

Sidewalks

12.290 Requirement.

All development for which land use applications are required by Section 1.060 must include sidewalks adjacent to public streets. This requirement also applies to new single-family houses and duplexes if they are located on arterial or collector streets or on curbed local streets, if there is an existing sidewalk within 500 feet on the same side of the street.

Sidewalks shall be built when arterial and collector streets are constructed and at the discretion of the City Engineer during their reconstruction. This provision shall also apply to local streets that serve commercial and multi-family development. Sidewalks are required on both sides of all streets. If an interim street standard is being constructed which does not include bike lanes or sidewalks, interim bikeways or walkways for pedestrians shall be provided by paved roadway shoulders at least 8 feet wide on arterials and 6 feet on other streets. Provision of sidewalks may be waived when the street serves a use or combination of uses that generate fewer than 50 trips a day (based on ITE standards) and cannot be continued or extended to other properties.

Response: As shown in the attached plans, a sidewalk will be added on the southeastern side of the new cul-de-sac to connect the on-site walkway to the existing sidewalk. This standard is met.

12.300 Design, Width, and Location.

All sidewalks must be constructed, replaced or repaired in accordance with the Standard Construction Specifications. The required width and location of sidewalks is as follows:

- (1) *The required width for a sidewalk on an arterial or collector street is 7 feet. This may be reduced to 6 feet if the sidewalk is separated from the curb by a landscaped planter strip at least 5 feet wide. When there is inadequate right-of-way for additional width and no additional right-of-way can be obtained as a condition of development approval, the sidewalk width may be reduced to 5 feet. In all cases, any right-of-way remaining outside the sidewalk is to be landscaped and maintained by the adjoining property owner.*

Response: No new sidewalks on arterial or collector streets are proposed. This standard does not apply.

- (2) *Sidewalks along residential and other local streets must be at least 5 feet wide. A planter strip at least 6 feet wide shall separate the sidewalk from the street. Street trees shall be selected from the list of approved street trees established by the City. The planter strip shall be of permeable materials. Locating approved street-side post-construction stormwater quality facilities in the planter strip is encouraged.*

Response: The subject site does abut a local street, Willetta Street. The cul-de-sac area will be constructed to City of Albany standards. As shown in the attached plans, the sidewalk on the Willetta Street cul-de-sac will be 5 feet wide. This standard is met.

- (3) *In the Historic Downtown and Central Business districts, as defined on the zoning map, sidewalks must be at least 10 feet wide and be installed adjacent to the curb.*

Response: The subject site is not in the Historic Downtown or Central Business district. This standard does not apply.

- (4) *Regardless of other provisions contained in this article, any sidewalk project that is less than 200 feet in length and connects on either end to an existing sidewalk may be designed to match the existing pattern with the approval of the City Engineer.*

Response: This option is not proposed.

- (5) *When obstructions exist or are proposed (including, but not limited to, mailboxes, utility poles, trees, planters, fire hydrants, signs, benches, bus stops, post-construction stormwater quality facilities, etc.), provisions must be made to maintain a minimum of 4 feet of unobstructed sidewalk width on local streets, 5 feet on collector and arterial streets, and 6 feet in the Historic Downtown (HD) and Central Business (CB) districts.*

Response: No obstructions exist in the area of the new cul-de-sac and sidewalk. This standard does not apply.

- (6) *Clustered mailboxes shall be on an accessible path and shall include a 72" minimum accessible turning space. Adjacent sidewalks may be incorporated in the measurement of the turning space. Where constructed in the public right-of-way, construction shall be in accordance with the Standard Construction Specifications.*

Response: No clustered mailboxes are proposed. This standard does not apply.

- (7) *Maintenance of sidewalks and planter strips shall be the continuing obligation of the adjacent property owner except for approved post-construction stormwater quality facilities located in planter strips or when the lot backs onto an arterial. Other than approved post-construction stormwater quality facilities, planter strips shall be landscaped and maintained in like manner to the front yard setback requirements of Article 9.*

Response: Maintenance of the sidewalk and planting strip will be the obligation of the owner. This standard is met.

- (8) *Sidewalks shall be designed to parallel streets in line and grade and shall avoid unnecessary meandering from the curb line and elevation changes except as necessary to avoid significant trees or traverse topographic barriers.*

Response: As shown in the attached plans, the sidewalk southeast of the cul-de-sac was designed in line with the cul-de-sac. This standard is met.

- (9) *Public paths not adjacent to a public street shall be a minimum of 10 feet wide.*

Response: No public paths not adjacent to a public street are proposed. This standard does not apply.

Street Trees

12.321 General Requirements.

When a new public street is created in conjunction with development, street trees are required in accordance with the standards provided in the Standard Construction Specifications and the Urban Forestry Management Plan.

Response: No new public streets are proposed. This standard does not apply.

12.324 Street Tree Planting Options.

Options available to meet this requirement are identified below. Where street side post-construction stormwater quality facilities are proposed on a new public street, the option selected must match the planting option selected for the post-construction stormwater quality facilities as identified in Title 12 of the Albany Municipal Code:

- (1) *Submit a street tree plan to the City for planting and establishing trees within the public rights-of-way that meets the tree planting standards in the Urban Forestry Management Plan. The City Forester shall either approve or deny the plan based on the plan's compliance with these requirements.*
- (2) *Pay a fee to the City based upon a requirement for one tree per thirty linear feet of street frontage. This fee shall be deposited into the City's Urban Forestry Program Fees Fund. The City shall thereafter assume responsibility for the purchase, planting, and establishment of street trees within the public right-of-way or on public lands maintained by the City within or abutting the specified development.*

Response: No new public streets are proposed; nonetheless, street trees are proposed in accordance with City requirements (see Exhibit 3 Sheets L4.0-L4.4). This standard is met.

Bikeways

12.330 Master Bikeways Plan.

The City's adopted Master Bikeways Plan is in the Comprehensive Plan.

12.340 Provisions for Bikeways.

Developments adjoining or containing proposed bikeways identified on the adopted Master Bikeways Plan shall include provisions for the future extension of such bikeways. Land use approvals issued for planned developments, greenway conditional use permits, subdivisions and other developments that will principally benefit from such bikeways may be conditioned to include bikeway improvements.

In the case of arterial or collector streets, bike lanes shall be built during their construction, and considered during their reconstruction. This provision shall also apply to local streets in other than single-family residential developments.

Response: The subject site is not adjoining or containing a proposed bikeway. No construction of arterial or collector streets is proposed. This standard does not apply.

12.350 Bikeway Design.

Where possible, bikeways should be separated from other modes of travel, including pedestrian. Minimum width for bikeways shall be 6 feet per travel lane when adjacent to a curb (one-way) and 10 feet when not on a roadway (two-way). A reduction in standards may be allowed when the City Engineer finds that no safety hazard will be created and other special circumstances (such as physical constraints) exist.

Response: No new bikeways are required. This standard does not apply.

Utilities – General

12.360 Utility Easements.

The developer shall make arrangements with the City of Albany and each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development. All utility easements must be public easements.

Response: Easements will be provided for the existing water and stormwater facilities in the Willetta Street corridor. This standard is met.

12.370 Utility Easement Width.

The standard width for public utility easements adjacent to street rights-of-way is 7 feet. The minimum width for all other public utility easements shall be 15 feet for water, 20 feet for sewer, and 15 feet for piped storm drainage unless otherwise specified by the utility company or City Engineer. When feasible, utility easements shall be centered on a lot line.

Response: As shown in the attached plans, public water and storm drainage lines on the site will provide for the required easement widths noted above. This standard is met.

12.380 Information on Development Plans.

The developer must show easements for all utilities. Plans showing the location of all utilities shall be submitted to the City as part of the site plan review or land division process.

Response: All public utilities on the site show easements as required in section 12.370. This standard is met.

12.390 Requirement for Underground Utilities.

Except as exempted in Section 12.400, all utility lines, cables, or wires (including but not limited to those used for electricity, communication, street lighting, and cable television) constructed upon, adjacent to, or within land subdivided or prepared for development after the effective date of this Code, must be placed underground. The intent of the City is that no poles, towers, or other structures associated with utility facilities shall be permitted on any street or lot within such a subdivision or development.

Response: As described in the response to 12.400 below, the proposal is exempt from this standard.

12.400 Exceptions.

Overhead facilities are only permitted in the following instances:

- (1) *Emergency installations, electric transmission lines, or through feeders operating at distribution voltages which act as a main source of supply to primary laterals and to direct connected distribution transformers and primary loads.
Should it be necessary to increase the capacity of major power transmission facilities for service to the area, new or revised installations shall be made only on rights-of-way or easements on which overhead facilities exist at the time of the capacity increase.*
- (2) *Appurtenances and associated equipment such as surface-mounted transformers, pedestal-mounted terminal boxes, meter cabinets, telephone cable closures, connection boxes, and the like.*
- (3) *Structures without overhead wires, used exclusively for fire alarm boxes, street lights, or municipal equipment installed under the supervision and with the approval of the City Engineer.*
- (4) *Power substations, pumping plants, and similar facilities necessary for transmission or distribution of utility services.*
- (5) *Television antennas and satellite dishes [See Section 3.080 (12)].*
- (6) *Industrial developments, except for utility lines, cables, and/or wires providing service to an individual lot. Such lines must be placed underground from the nearest power pole to the facility ultimately being operated on the individual lot. Certain industries requiring exceptionally large power supplies may request direct overhead power as a condition of site plan approval. Underground utilities may be required in Industrial Park developments and planned developments in the Industrial Districts.*
- (7) *New development on existing individual lots of record in areas where service is currently by overhead utilities.*

Response: Overhead facilities exist on the eastern property line and will be allowed to remain per criterion (7) above. The proposal meets this exception.

12.405 Property Monuments.

Upon completion of a utility project and before acceptance by the City, all property corners and other monuments disturbed or removed by the project shall be reestablished and protected by an Oregon-licensed surveyor retained by the developer.

Response: This process is understood.

Water

12.410 When Public Water is Available.

All new development, including a single-family residence, must extend and connect to the public water system when service is available within 150 feet of the property. Fire hydrants, mains, and related appurtenances shall be installed as required by the City Fire Marshal.

Response: As shown in the attached plans, the proposed development will connect to the public water system in Willetta Street. This standard is met.

12.420 When Public Water is Not Available.

No new development is allowed on private well systems, except for construction of one single-family dwelling on an existing lot of record. Residential lots created by a land partition may be served by private wells if approved by the City, and provided the new lots are subject to a Petition for

Improvement/Waiver of Remonstrance for a future assessment district for public water. If a second partition plat is filed on the same parcel, the application will be subject to the subdivision requirement that the development be served by public water.

Response: Public water is available to the subject site. This standard does not apply.

12.430 Extension Along Property Frontage and Within Interior.

Water distribution mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion. Main extensions may be required through the interior of properties when necessary to provide service to other properties or to provide looping for fire flows.

Response: The public water system currently meets this standard. This standard does not apply.

12.440 Water Plan Approval.

Preliminary water plans and systems must be submitted to the City Engineer as part of the tentative plat or Site Plan Review application. These plans must provide enough information to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The utility plans provided with this submittal show the information required above. This standard is met.

12.450 Design Requirements for New Development.

All new development within the City must, when appropriate, make provisions for the continuation or appropriate projection of existing principal water lines serving surrounding areas.

Response: The existing water system provides the services required to surrounding areas. This standard does not apply.

12.460 Restriction of Development.

The review body may restrict development approvals when a deficiency exists in the water system or portion thereof which cannot be corrected as a part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

Sanitary Sewers

12.470 When Public Sewer is Available.

All new development must extend and connect to the public sewer system when service is available within 300 feet of the property.

Response: As shown in the attached plans, the proposed development will connect to the public sewer system in the new cul-de-sac on Willetta Street. This standard is met.

12.480 When Public Sewer is Not Available.

Where sewer is not available within 300 feet of the property, no development is allowed on private septic systems, except for construction of one single-family dwelling on an existing lot of record or on a parcel no smaller than five acres created through the land division process. Any private on-site system allowed by this section must be approved by the county.

Response: Public sewer is available to the subject site. This standard does not apply.

12.490 Extension Along Property Frontage and Within Interior.

Sewer collection mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion. When private sanitary sewer services will exceed 100 feet long, as measured from the public main to the structure, the City Engineer may require extension of public sewers into the interior of the property.

Response: The public sewer system currently meets this standard. This standard does not apply.

12.500 Sewer Plan Approval.

Preliminary sewer plans and systems must be submitted to the City Engineer as part of the tentative plat or Site Plan Review application. These plans must provide enough information to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The utility plans provided with this submittal show the information required above. This standard is met.

12.510 Design Requirements for New Developments.

All new development within the City must, where appropriate, make provisions for the continuation or appropriate projection of existing sewer lines serving surrounding areas. Line extensions may be required through the interior of a property to the developed when the City Engineer determines that the extension is needed to provide service to upstream properties.

Response: The existing sewer system provides the services required to surrounding areas. This standard does not apply.

12.520 Restriction of Development.

The review body may restrict development approvals where a deficiency exists in the sewer system or portion thereof that cannot be corrected as a part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

*Storm Drainage**12.530 General Provisions.*

The review body will approve a development request only when adequate provisions for storm and flood water run-off have been made as determined by the City Engineer. The stormwater drainage system must be separate from and independent of any sanitary sewer system. When possible, inlets should be provided so surface water is not carried across any intersection or allowed to flood any street. Surface water drainage patterns and proposed storm drainage must be shown on every development proposal plan. All proposed storm drainage management plans and systems must be approved by the City Engineer as part of the tentative plat or site plan review process.

Response: The proposed development will provide a storm drainage system meeting the requirements listed above. This standard is met.

12.535 Storm Drainage Plan Approval.

Preliminary storm drainage management plans and systems must be submitted to the City Engineer as part of the Tentative Plat or Site Plan Review application. These plans must provide enough information

to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The proposed development will provide a storm drainage plan meeting the requirements listed above. This standard is met.

12.540 Easements.

When a subdivision is traversed by a watercourse, drainageway, channel or stream, a public stormwater easement conforming substantially to the lines of the watercourse and further width as the City Engineer determines will be adequate for conveyance and maintenance shall be provided. Improvements to the drainage way, streets, or parkways parallel to watercourses may be required.

Response: The proposed public storm line crossing the property will have an easement dedicated as required above. This standard is met.

12.550 Accommodation of Upstream Drainage.

A culvert or other drainage facility shall be large enough to accommodate potential run-off from its entire upstream drainage area, whether inside or outside of the development. The City Engineer must review and approve the necessary size of the facility, based on the provisions of the Storm Drainage Master Plans, and sound engineering principles, and assuming conditions of maximum potential watershed development permitted by the Comprehensive Plan.

Response: The proposed storm line crossing the property has been sized and designed to accommodate the upstream drainage area. This standard is met.

12.560 Effect on Downstream Drainage Facilities.

When the City Engineer anticipates that the run-off resulting from the development will overload or cause damage to an existing drainage facility, the review body will withhold approval of the development until provisions have been made for improvement, or prevention, of said potential condition.

Response: This condition does not exist to the knowledge of the applicant. This policy is understood but does not apply.

12.570 Storm Drainage Management Practices.

Development must employ storm drainage management practices approved by the City Engineer that minimize the amount and rate of surface water run-off into receiving streams or drainage facilities or onto adjoining properties. As required by Title 12 of the Albany Municipal Code, the development must also employ post-construction stormwater quality management practices approved by the City Engineer that regulate the quality of the stormwater leaving the site. Drainage management practices must include, but are not limited to, one or more of the following practices:

- (1) Temporary and permanent ponding or detention of water;
- (2) Post-construction stormwater quality facilities;
- (3) Minimization of impervious surfaces;
- (4) Emphasis on natural drainageways;
- (5) Prevention of uncontrolled water flow from the development;
- (6) Stabilization of natural drainageways as necessary below drainage and culvert discharge points for a distance sufficient to convey the discharge without channel erosion;
- (7) Collection of runoff from impervious surfaces and transportation to a natural drainage facility with sufficient capacity to accept the discharge; and

(8) *Other practices and facilities designed to transport stormwater and improve water quality.*

Response: As shown in the attached plans, the proposed on-site storm drainage system meets the requirements listed above. This standard is met.

12.575 Extension Along Property Frontage and Within Interior.

Storm mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion.

Response: As shown in the attached plans, the public storm line crossing the site (replacing the existing ditch) is designed to meet this requirement. This standard is met.

12.580 Design Requirements for New Development.

All new development within the City must, when appropriate, provide for the continuation or appropriate projection of existing storm drain lines or drainageways serving surrounding areas. Extensions may be required through the interior of a property to be developed when the City Engineer determines that the extension is needed to provide service to upstream properties.

Response: As shown in the attached plans, the proposed public storm line is designed to provide drainage for the upstream area identified in the June 17, 2015, pre-application conference. No further extension of the public storm drainage system is required. This standard is met.

12.581 Restriction of Development.

The review body may restrict development approvals where a deficiency exists in the stormwater system or portion thereof that cannot be corrected as part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

12.585 NPDES Permit Required.

A National Pollutant Discharge Elimination System (NPDES) permit must be obtained from the Department of Environmental Quality (DEQ) for construction activities (including clearing, grading, and excavation) that disturb one or more acre of land, or whatever the current standard is at the time the application is submitted.

Response: An NPDES permit from DEQ will be acquired prior to any commencement of construction. This standard is met.

Article 13 – Signs

Review Procedures.

13.210 Permit Needed.

Except as specifically excluded herein, no property owner, lessee, contractor, or other person shall display or cause to be displayed any sign requiring a permit as set forth in this Code, except for maintenance of signs that conform with this ordinance, without first obtaining from the Building Official a written permit to do so, paying the fees prescribed therefore, and otherwise complying with all of the applicable provisions of this ordinance. If a governmental agency requires the relocation of a sign, the fee described above shall be waived.

Response: The applicant will obtain necessary permits prior to installation of any signage. This standard is met.

IV. CONCLUSION

Based on the information presented and discussed in this narrative and the attached supporting plans and documentation, this application meets all applicable standards necessary for land use approval. The proposed development complies with all applicable standards of the Albany Development Code and furthers the City's objectives of promoting downtown and providing a facility that meets the needs of the Police Department. The applicant respectfully requests approval by the City.



TO: City of Albany, Community Development Department
 FROM: Mario Lattanzio, Chief of Police
 DATE: August 17, 2015

The purpose of this report is to document the results of the neighborhood meeting. Following is pertinent information concerning this meeting:

A. Dates and Locations of all neighborhood meetings:

One meeting was held for the purpose of providing information to the neighborhood.

Date: Thursday, August 13, 2015, at 6:00 p.m.

Location: Albany First United Methodist Church
 1115 28th Avenue SW
 Albany, OR 97321

B. Method of publicizing meeting:

For the neighborhood meeting, 150 public notices were mailed through the United States Postal Service to all property owners and residents within 1,000 feet of the boundaries of the subject property. The public notice included the date, time, and location of the public meeting. It also contained a brief description of the development plans and included a map for geographical reference.

C. Attendees:

The neighborhood meeting was attended by 52 people. Two people were from Mackenzie, and nine people were from the City. The remaining 41 attendees were neighbors or other interested parties. See the attached attendance list.

D. A summary of the concerns, issues, and problems raised by neighbors:

Police Chief Mario Lattanzio opened the neighborhood meeting, introduced staff and presenters, and discussed the details of the property site and building. He specified that the building is not a jail. There are five temporary holding cells (three for adults and two for juveniles) which give staff an area to continue investigations, if needed. The majority of people that are arrested will be taken directly to the Linn County Jail.

Jeff Humphreys, Mackenzie, gave an overview of the design for the proposed police building. He said the state of Oregon requires the City to spend 1.5 percent of the total construction value on green technology. That is being done by putting in solar panels on the roof. He explained that Pacific Boulevard/Highway 99E is the primary access driveway and Willetta Street is the secondary access driveway.

Acting Engineer Staci Belcastro discussed the traffic flow. She said staff has received comments and concerns from citizens about increased traffic on Willetta Street and that the Oregon Department of Transportation (ODOT) has approved the driveway access point on Pacific Boulevard/Highway 99E. She explained that the City has received approval from ODOT, but not the permit, for an unrestricted driveway which means vehicles can make a right in/right out and left in/left out from the site. She said getting the unrestricted access was very important to this project in order to alleviate traffic volume on Willetta Street. She said the ODOT permit application cannot be submitted until the land-use application is submitted.

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There will be additional opportunities for residents to review the land-use application during the land-use process. She said a police station would create a fraction of the traffic compared to other potential buildings such as multifamily housing or offices.

The neighbors expressed the following concerns and questions:

- What street does the front of the building face?
- Has ODOT given the City a permit for the driveway entrance on Pacific Boulevard?
- Increased traffic on Willetta Street.
- Why was this specific location chosen?
- How will this impact property values?
- Can the City clean up the debris in Cathey Creek?
- Potential of flooding on properties.
- Are any changes to the traffic signal anticipated?
- Has a traffic study been completed?
- How will the construction traffic be handled?
- Why wasn't a bridge over Cathey Creek chosen for secondary access?
- Will police staff use Willetta Street?
- What will be the use of sirens in the neighborhood?
- How many shift changes are there?
- How many employees are typically at the station at one time?
- Why are there so many parking spaces?
- How many cars will leave the station at one time when responding to an incident?
- What are the impacts on crime rates in neighborhoods when a police department is located nearby?
- If you arrest someone at the station and let them go, do you let them go in our neighborhood?
- Do you release individuals at all hours?
- Will the jail stay where it is?
- How tall is the lighting in the parking lot?
- Will there be a firing range located within the police department?
- Will there have to be zoning changes?
- Will you assess how the building could impact homes in the area such as increasing flooding in yards or sewer backups?
- What kind of landscaping will go along the outside wall that is around the building?
- Will there be another neighborhood meeting to discuss updates?

E. A discussion of how the applicant has addressed or intends to address concerns, issues, and problems:

- What street does the front of the building face? **Pacific Boulevard**
- Has ODOT given the City a permit for the driveway entrance on Pacific Boulevard? **The City has received approval from ODOT, but doesn't have a permit yet. The permit application takes place during the land-use process.**
- Increased traffic on Willetta Street. **Willetta Street would be used for any development that could occur on the site. The Police Chief will be able to tell his employees which streets to be used to minimize the impact on Willetta Street.**
- Why was this specific location chosen? **The City Council and staff looked at every site available within the City. This property was the best deal we could find with good access points to the community. In addition to this, the south part of Albany is where we will likely see increased growth.**

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August 19, 2015

- How will this impact property values? **Chief Lattanzio said the police stations in Arizona were in the middle of neighborhoods. He doesn't believe the station will have a negative impact on property values.**
- Can the City clean up the debris in Cathey Creek? **If there are issues with Cathey Creek, staff will work to get the area cleaned up.**
- Are any changes to the traffic signal anticipated? **None are anticipated, but we won't know for sure until the traffic analysis is submitted to ODOT for review. Possibly a timing change could be required to the signals on 29th Street.**
- Has a traffic study been completed? **No, this will be submitted to ODOT after the design review.**
- How will the construction traffic be handled? **This will be addressed during permit and planning process. The goal will be to use Highway 99E as the main access point during construction.**
- Why wasn't a bridge over Cathey Creek chosen for secondary access? **There would be a bigger cost impact to build a bridge.**
- Will police staff use Willetta Street? **Minimally.**
- What will be the use of sirens in the neighborhood? **Not typically. Sirens will mainly be used when going out on Pacific Boulevard. Officers typically respond to calls while out on patrol, not from the station.**
- How many shift changes are there? **Two.**
- How many employees are typically at the station at one time? **40 people at any given time, but that number can fluctuate depending on the day and time of the week.**
- Why are there so many parking spaces? **This building is being built to accommodate 20 years' worth of growth.**
- How many cars will leave the station at one time when responding to an incident? **Multiple cars won't try to leave the station at one time. Most officers on shift are already out in the community working and they tend to stay out in the field during their working hours.**
- What are the impacts on crime rates in neighborhoods when a police department is located nearby? **There should not be an increase in crime rates as most individuals who are arrested are processed through the county jail.**
- If you arrest someone at the station and let them go, do you let them go in our neighborhood? **Individuals are usually released from the jail. If they happen to be released from the station, normally someone will come pick them up.**
- Do you release individuals at all hours? **It can happen, but it is not typical procedure.**
- Will the jail stay where it is? **Yes.**
- How tall is the lighting in the parking lot? **That hasn't been finalized yet, but should be around 16 feet tall and the lamps will be light-saving and there is a directional cutoff to minimize light spills offsite.**
- Will there be a firing range located at the police department? **No.**
- Will there have to be zoning changes? **There has not been a zone change petition submitted and one is not needed. 1/3 of the property is zoned residential and 2/3 of the property is zoned office professional, both of which permit police stations.**
- Will you assess how the building could impact homes in the area such as increasing flooding in yards or sewer backups? **There will not be an increase in flooding or sewage backups. Improvements to the Umatilla Lift Station and force main are being constructed this fiscal year and the force main is rerouted.**
- What kind of landscaping will go along the outside wall that is around the building? **Landscaping will be done on the police station side.**
- Will there be another neighborhood meeting to discuss updates? **An additional neighborhood meeting isn't planned, but this project will be moving forward to the**

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August 19, 2015

**Planning Commission for review and a public hearing on November 16, 2015.
Residents will be able to make additional comments at that time.**

F. A discussion of any concerns, issues, and problems the applicant is unable or unwilling to address and why:

- Building a bridge over Cathey Creek. **This is not a cost effective solution.**

Police Chief Lattanzio thanked everyone for attending and closed the meeting at 7:00 p.m.

ML:de
Attachment



**Neighborhood Meeting for City of Albany Police Station
August 13, 2015**

PLEASE PRINT YOUR NAME AND ADDRESS

Name (Please Print)	Address	Telephone
BILL KNAUSS	820 COX ST.	541-967-6588
Al Steward	1162 Morse Lane SW	541-926-8595
Alycia Loewen	2601 Umatilla SW #3	541-928-5429
Mica Egger	2601 Umatilla SW #2	541 791 3182
Phyllis Greig	1134 SW 29	541 928-3299
Leslie + John Jenks	3174 Fir Oaks Pl SW Albany	541-967-9731
A + L Steckley	2629 S.W Umatilla	
Jim + Pam Van Schoone	1126 28 th Ave SW	541-926-6558
Jim Delapoev	1114 Lakewood Dr. SW	541 9266 724
Sean Kide	3015 Liberty St. SW	(904) 315-8004
Frank Moore	2529 Hill St	541 9057937
MIKE MARTIN	1400 Cassin NE3 NW	541 907 6220
John Becker	1100 30 th pl SW	541 926 5612
Larry + Marge Tomlin	1139 - Morse Ln SW	541-928-4327



**Neighborhood Meeting for City of Albany Police Station
August 13, 2015**

PLEASE PRINT YOUR NAME AND ADDRESS

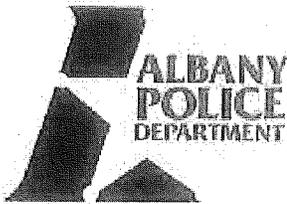
Name (Please Print)	Address	Telephone
Steve Mills	2023 Kodiak Ave. SW	541-928-0174
Floyd Collins	754 Quarry Rd	541-905-4516
NICHOLSON	1111 30th Pl, SW	541-928-4076
Jackie Ceguske	1112 30th Pl SW	541-926-3505
LARRY SCHOEN	2400 PACIFIC BLDG SW	541 926-0059
Forest + Mary Linn	1105 Lakewood DR SW	541 926 1116
Marilyn Smith	City of Albany	
Charles Alma Mea	2700 WILLETTSW	541-928-4219
Irene COBORN	3035 LIBERTY ST. SW	541-740-7518
DANI L. RADABOVETI	1110 S.W. 25TH ST,	541-926-6935
Cynthia J. Hummel	2601 Unatilla St #1	541-791-3156
Heggy, Susan Holman	3061 Fir Oaks Dr SW Albany	541 928 4324



Neighborhood Meeting for City of Albany Police Station
August 13, 2015

PLEASE PRINT YOUR NAME AND ADDRESS

Name (Please Print)	Address	Telephone
Bub + Denn Williams	1108- 30 th Pl SW	
Cheryl Kocwa	2810 W. 11 th St	
Helene Baker	1100 30 th Place SW	
Mary Ann Archer	1170- 25 th Ave SW	
Paul Rippner	3200 SE Davidson —	
Jeff Mueller	1232 Crescent Ave —	
David Martineau	City of Albany, 333 Grandview SW, Albany 97321	
CAROL CRANFORD	MACLEOD RD 1ST SW WATER PORTLAND 97214	
JEFF HUMPHREYS	LI LI	
Jeff Woodward - City of Albany		
Greg Roe		
Chief Mario Lattanzio - City of Albany		
Staci Belcastro - City of Albany		
Sharon Konopa - City of Albany Mayor		
Nes Hare - City of Albany		
Eric Carter - City of Albany		
Jeff Hinrichs - City of Albany		
Diana Eilers - City of Albany		



1117 Jackson Street SE
Albany, OR 97322-3245
www.cityofalbany.net

Records & Information
(541) 917-7680

Detectives
(541) 917-7686

Property & Evidence
(541) 917-7685

Community Resource Unit
(541) 917-7653

FAX
(541) 928-6992

NEIGHBORHOOD MEETING FOR CITY OF ALBANY POLICE STATION

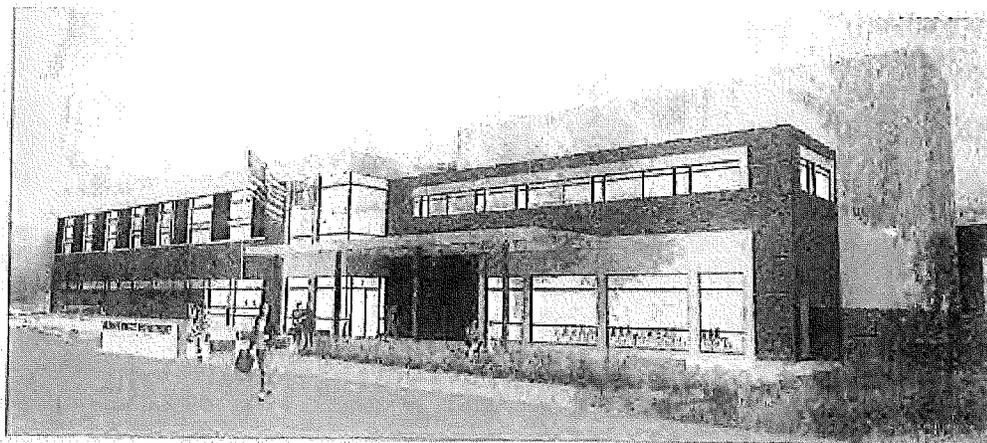
Dear Neighbor:

The City of Albany is holding an informational neighborhood meeting to present plans for a new Albany Police Station. The subject property is located in the 2400 block of Pacific Boulevard SW. The new building will be a two-story structure with approximately 41,000 square feet and will house all police department operations. Plans for the new facility incorporate modern technology, staff and community needs, and meet the latest earthquake and safety standards.

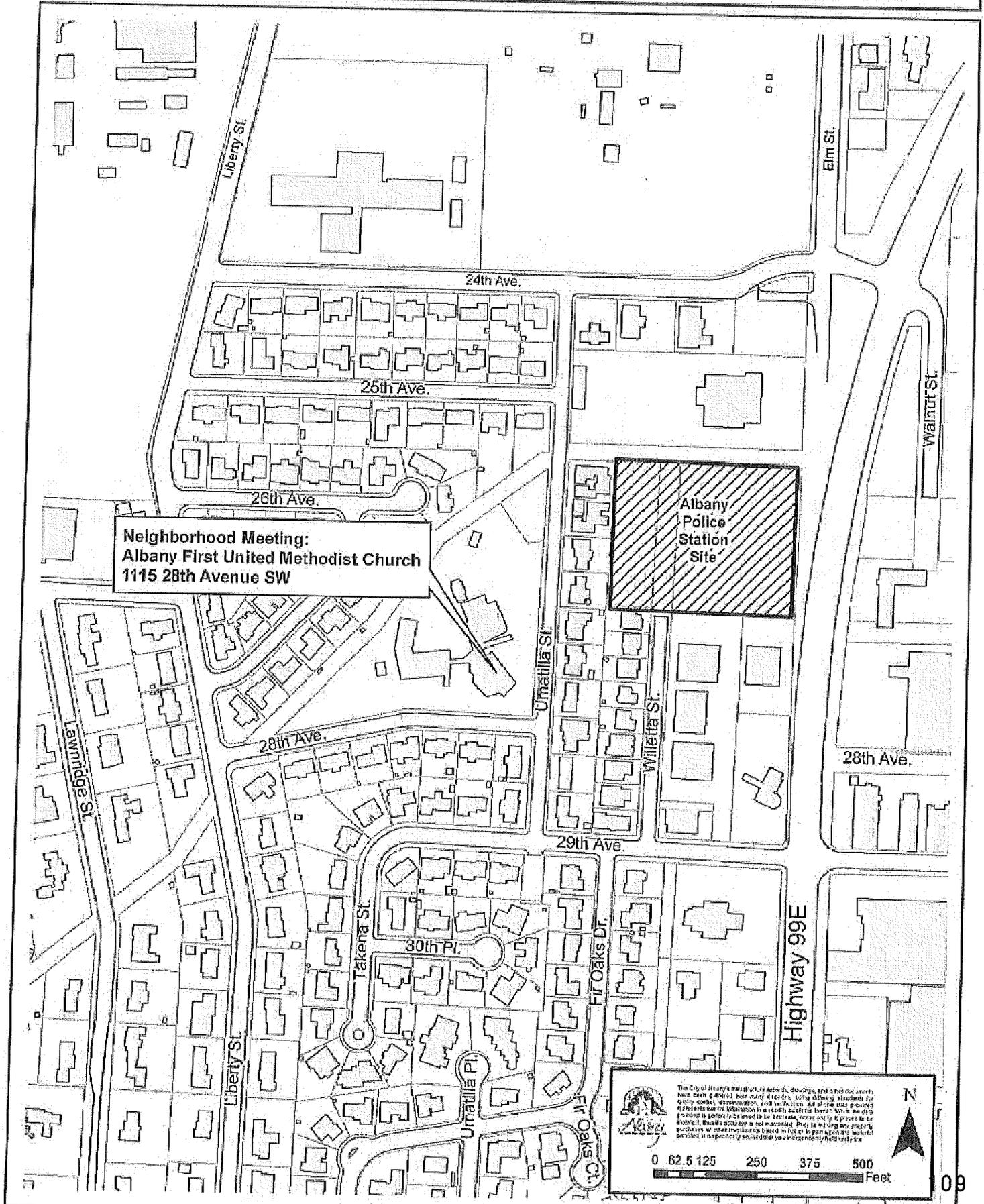
Meeting Date & Time: Thursday, August 13, 2015, at 6:00 p.m.

Meeting Location: Albany First United Methodist Church
1115 28th Avenue SW
Albany, OR 97321

If you have questions about the neighborhood meeting, please call Administrative Services Supervisor Diana Eilers at 541-917-3202.



**Neighborhood Meeting: Thursday, August 13, 2015, 6:00 p.m.
Albany First United Methodist Church, 1115 28th Avenue SW**



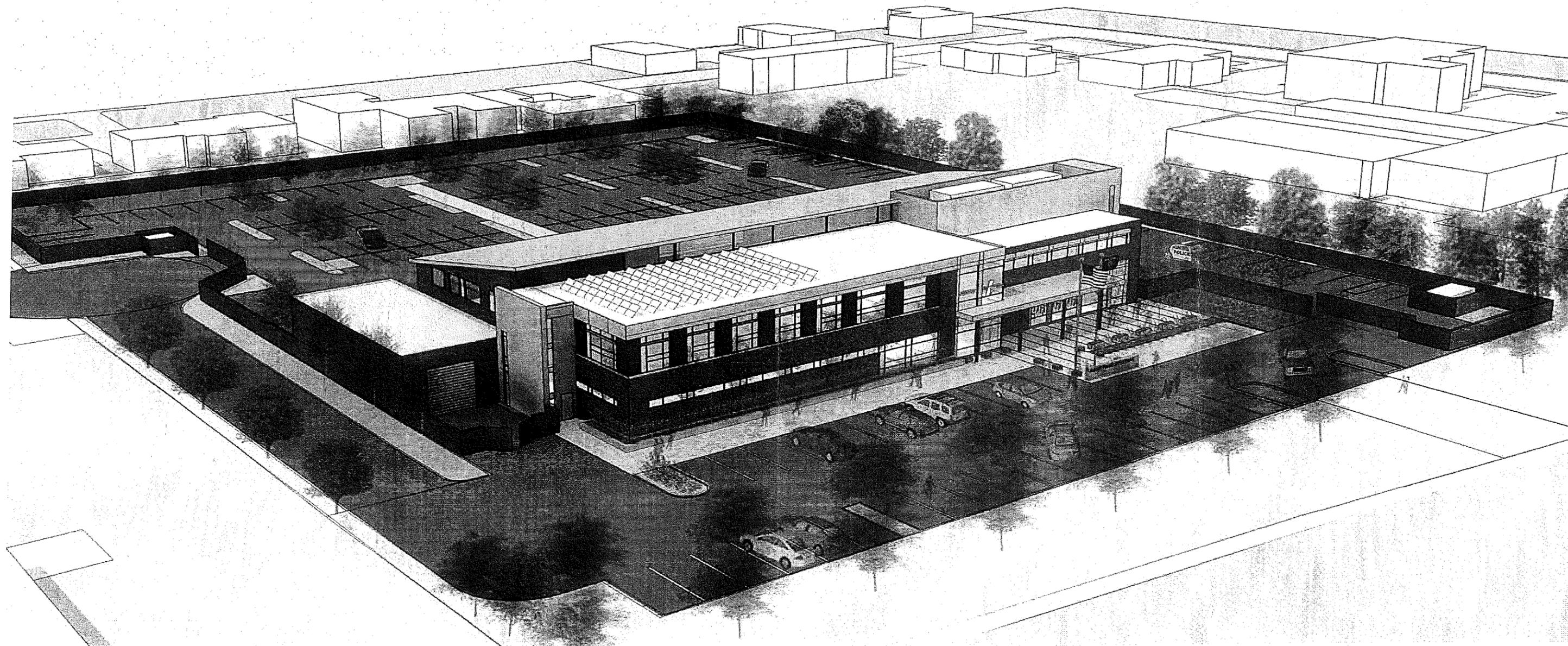
Neighborhood Meeting:
Albany First United Methodist Church
1115 28th Avenue SW

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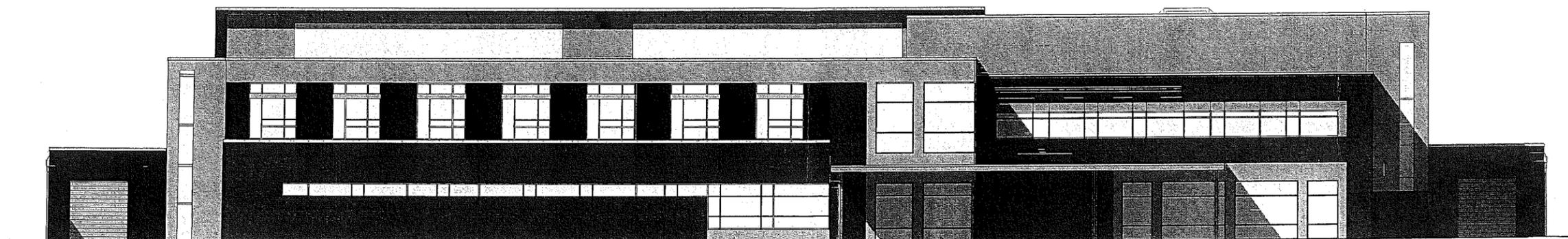
0 62.5 125 250 375 500 Feet



Concept Massing-Perspective View from the Northeast 1

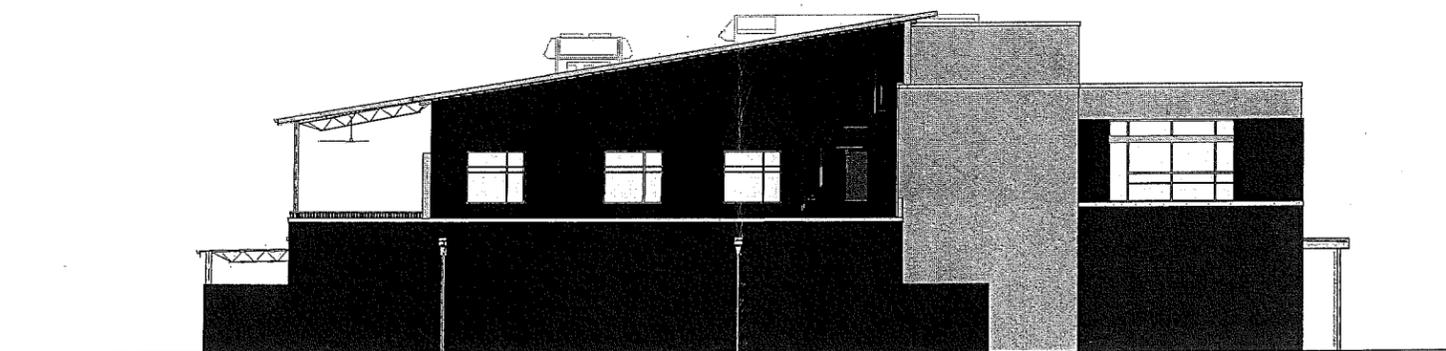


Concept Massing-Aerial View from the Southeast 2



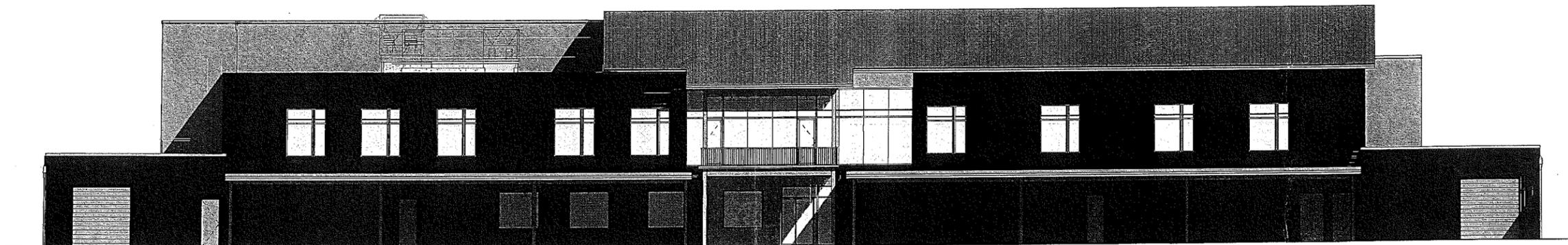
EAST ELEVATION

3/32" = 1'-0"



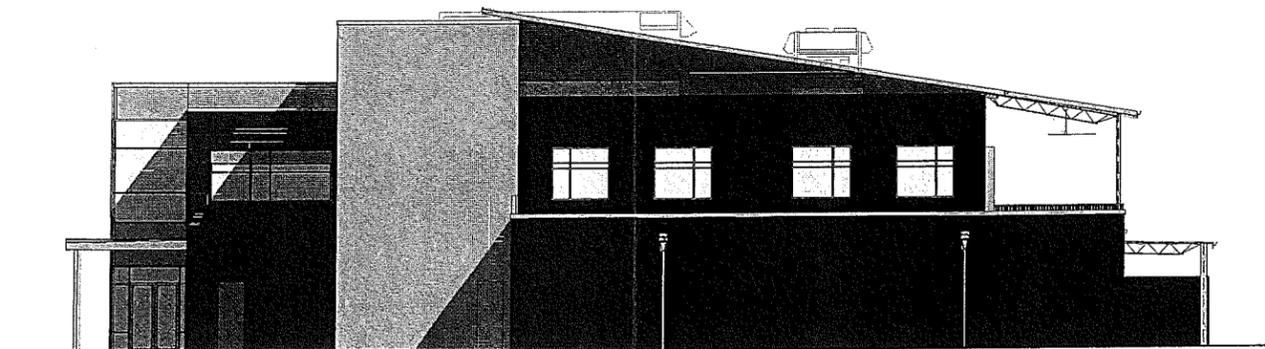
SOUTH ELEVATION

3/32" = 1'-0"



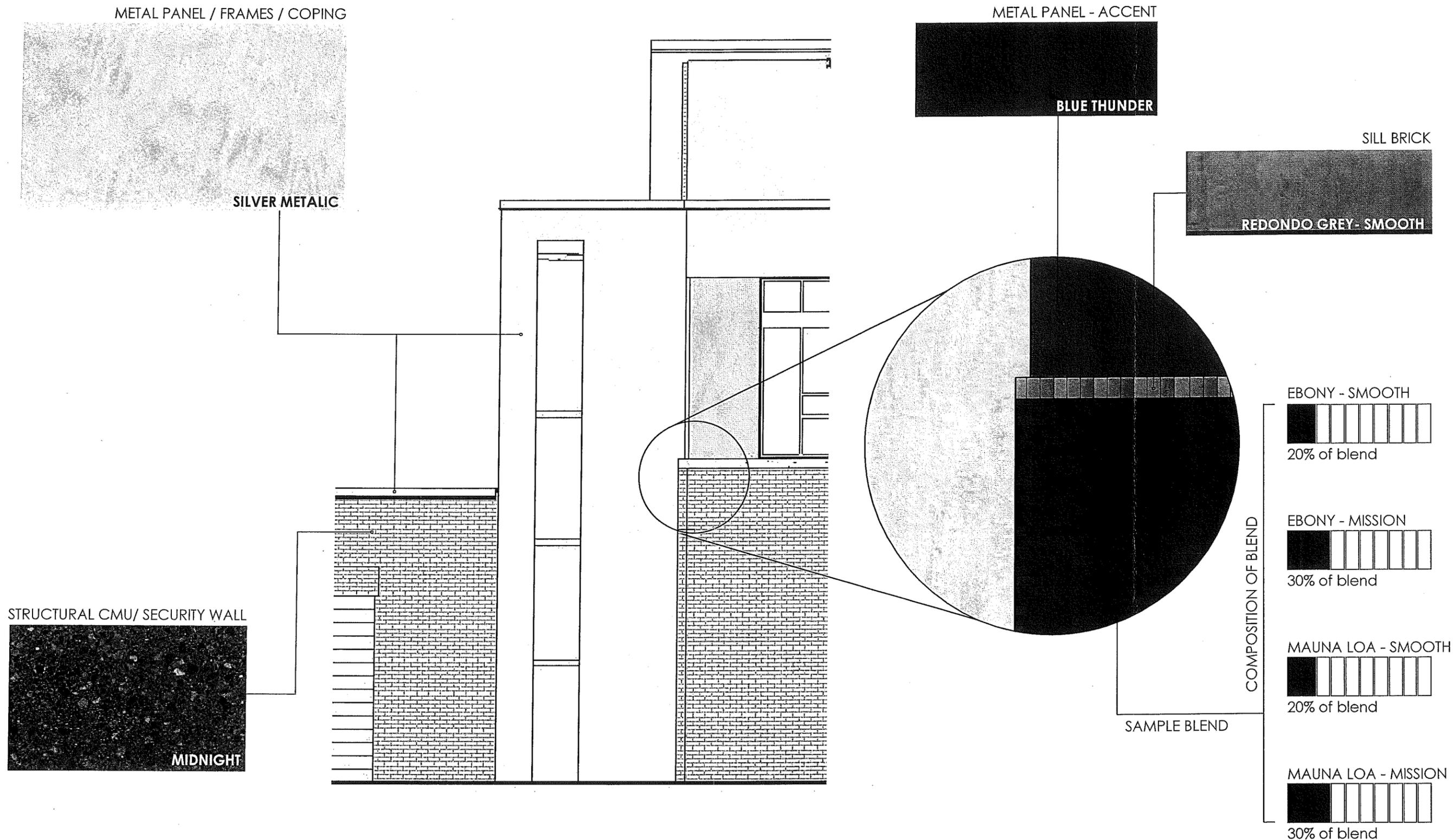
WEST ELEVATION

3/32" = 1'-0"



NORTH ELEVATION

3/32" = 1'-0"





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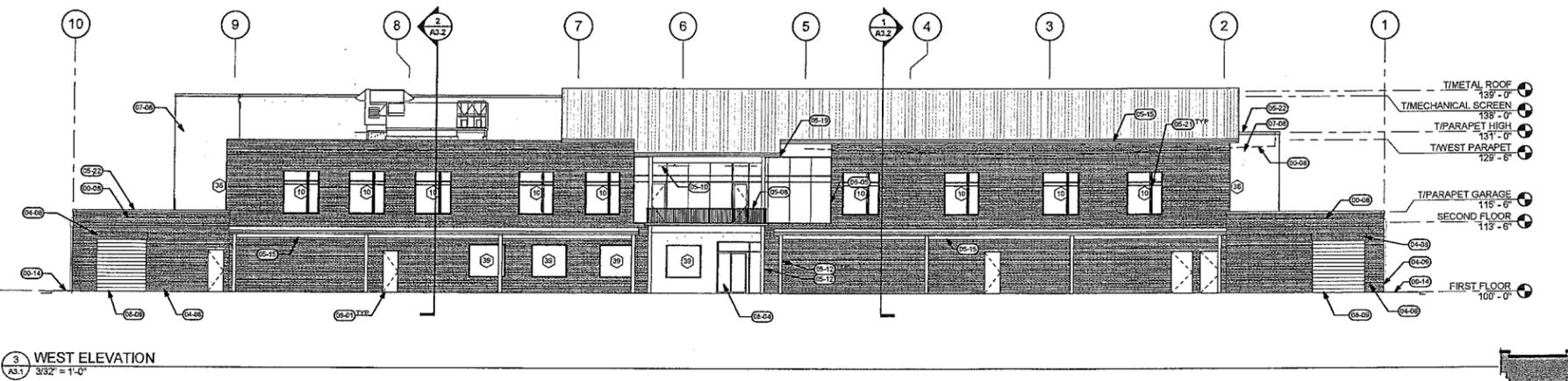
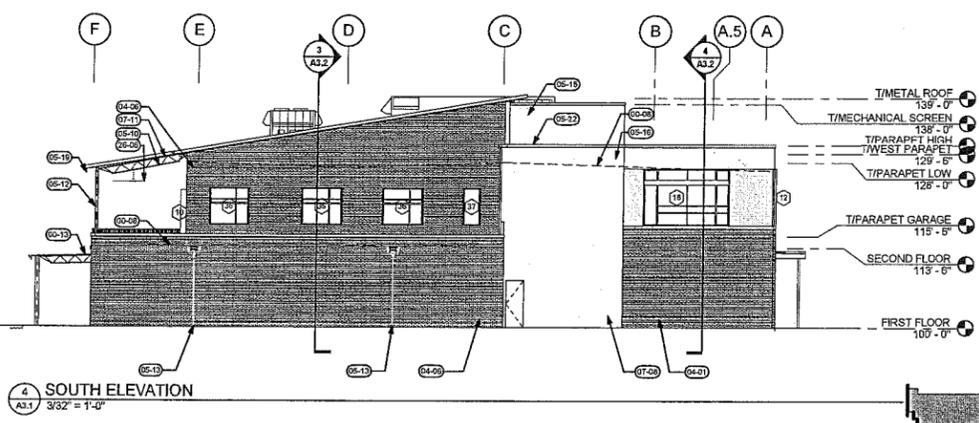
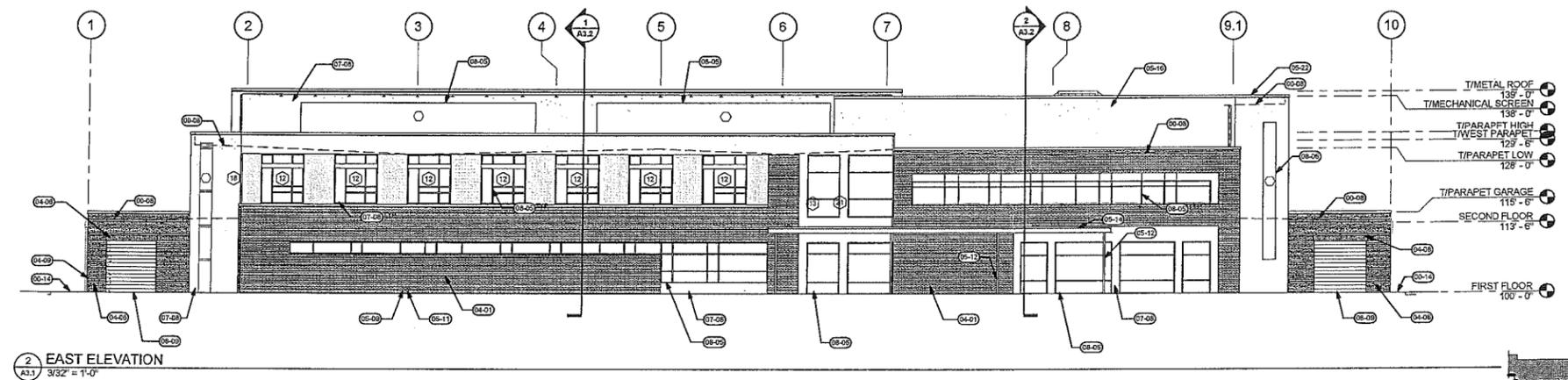
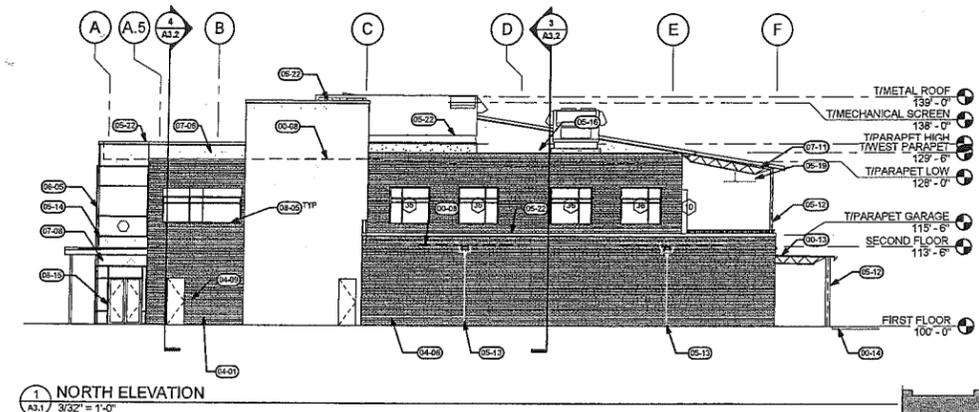
CITY OF ALBANY
333 BROADELBIN ST.
SW ALBANY, OR, 97321



Project

**ALBANY POLICE
DEPARTMENT**
333 BROADELBIN ST.
SW ALBANY, OR, 97321

Mechanical/Electrical
INTERFACE ENGINEERING
705 SW 3RD AVE., SUITE 400
PORTLAND, OR, 97204



- LEGEND**
- ① WINDOW TYPE - SEE A9.3
 - ② WINDOW TYPE, AS NOTED, OPPOSITE
 - ③ WINDOW TYPE, AS NOTED, BLASTGUARD FILM - SEE A9.3
 - ④ INT INTERIOR WINDOW TYPE - SEE A9.3
 - MASONRY VENEER
 - STRUCTURAL MASONRY
 - CONCRETE MASONRY UNIT
 - METAL PANEL WALL

- GENERAL NOTES**
- A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION
 - B. ELEVATION 100'-0" = 217.00' FINISH FLOOR ELEVATION MAIN FLOOR INDICATED IN CIVIL DRAWINGS
 - C. LAYOUT INFORMATION, NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS. REFER TO ENLARGED PLANS WHERE INDICATED FOR ADDITIONAL INFORMATION. ENLARGED PLANS TAKE PRECEDENT OVER PLANS OF SMALLER SCALE

- KEYNOTES**
- 00-08 LINE OF ROOF BEYOND - SEE ROOF PLAN
 - 00-13 CARPORT CANOPY - SEE ROOF PLAN
 - 00-14 LINE OF CURB - SEE CIVIL
 - 04-01 MASONRY VENEER
 - 04-06 STRUCTURAL MASONRY
 - 04-08 STRUCTURAL MASONRY LEVEL BLOCK
 - 04-09 MASONRY SECURITY WALL - SEE CIVIL FOR EXTENT
 - 05-08 VVA CUBE GUARDRAIL
 - 05-09 METAL SCUPPER, SEE DETAILS XX, XX, XX, XX - SEE PLUMBING
 - 05-10 METAL MESH SCREEN ATTACHED TO UNDERSIDE OF STRUCTURE
 - 05-11 OVERFLOW SCUPPER - SEE PLUMBING
 - 05-12 STEEL COLLUM, PAINT P - SEE STRUCTURAL
 - 05-13 STAINLESS STEEL COLLECTOR BOX, TIE TO STEEL DOWNSPOUT
 - 05-14 EXTERIOR CANOPY - SEE ROOF PLAN
 - 05-15 METAL FASCIA W/ INTEGRAL GUTTER
 - 05-16 METAL PANEL SCREEN WALL
 - 05-18 STRUCTURE, PAINT P-X WHERE EXPOSED - SEE STRUCTURAL
 - 05-21 METAL SUNSHADE
 - 05-22 METAL COPING TO MATCH METAL WALL PANEL
 - 07-08 METAL WALL PANEL
 - 07-11 STANDING SEAM METAL ROOF ASSEMBLY
 - 08-01 HOLLOW METAL DOOR AND FRAME
 - 08-04 SLIDING GLASS DOOR - SEE DOOR SCHEDULE
 - 08-05 ALUMINUM-FRAMED STOREFRONT SYSTEM - SEE GLAZING SCHEDULE
 - 08-06 NOTORIZED SECTIONAL OVERHEAD DOOR - SEE DOOR SCHEDULE
 - 08-05 STOREFRONT DOOR ASSEMBLY - SEE DOOR SCHEDULE
 - 26-06 HANKU M EXTERIOR CEILING FAN - SEE ELECTRICAL

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Revision Schedule	
Revision Delta	Issue Date

SHEET TITLE
**BUILDING
ELEVATIONS**

DRAWN BY: CRR
CHECKED BY: CPC
SHEET

A3.1

JOB NO. 2140284.02

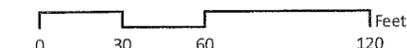
ALBANY POLICE STATION
 2400 block Pacific Blvd SW
 Albany, Oregon

LEGEND

-  Parcels
-  Subject Site

Zoning

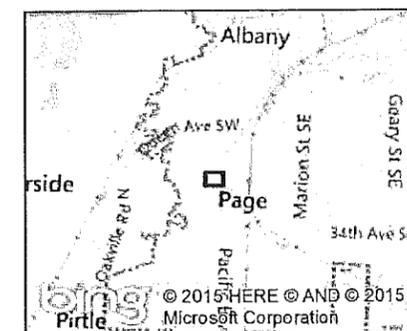
-  LI: Light Industrial
-  OP: Office Professional
-  RM: Residential Medium Density
-  RS-5: Residential SFR 5,000 SF
-  RS-6.5: Residential SFR 6,500 SF



1 inch = 60 feet

SOURCE DATA: Oregon Spatial Data Library, 2014
 GEOGRAPHIC PROJECTION: NAD 83 HARN, Oregon North Lambert Conformal Conic

Date: 9/9/2015 Map Created By: ALD
 File: sRe_13x17-150909 Project No: 2140284.MK



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TOPOGRAPHIC SURVEY
FOR
ALBANY POLICE STATION
LOCATED IN
NE 1/4 SEC. 13, T. 11 S., R. 4 W., W.M.
CITY OF ALBANY, LINN COUNTY OREGON
TAX LOTS 6900 & 7003, ASSESSOR MAP 11S-4W-13AB

JUNE 10, 2015

HORIZONTAL DATUM:

THIS PROJECT IS ON A LOCAL DATUM PLAIN. SEE COORDINATE TABLE FOR SURVEY CONTROL & BOUNDARY MONUMENTS.

VERTICAL DATUM:

VERTICAL DATUM IS NAVD 1929 BASED ON CITY OF ALBANY GPS POINT 8240. SITE TBM IS SURVEY CONTROL POINT #1 AS SHOWN ON THE MAP.

CONTROL & MONUMENT TABLE

POINT NUMBER	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	5000.0000	5000.0000	219.39	cont #12
2	5276.2725	5036.1668	219.81	cont #12
3	5107.7367	4655.4445	214.45	cont #1
4	4868.9139	4639.4298	213.22	cont-mssg
5	5311.2035	4667.0689	215.43	cont-ht
6	5315.7276	4863.7818	214.79	cont-ht
7	5308.8654	4801.6337	214.67	cont-ht
8	5307.5671	4325.2920	214.12	cont-ht
9	5307.7873	4400.0374	213.38	cont-pk
10	4929.6061	4516.9222	215.10	5/8" I.R.
11	4904.3635	4519.7195	213.97	1/2" I.R.
12	4921.7026	4678.9065	214.37	5/8" I.R.
13	4928.9839	4628.9336	213.97	5/8" I.R.
14	4920.3079	4854.5838	215.02	5/8" I.R.
15	4919.3263	4978.8414	216.25	5/8" I.R.
16	4619.2281	4680.4036	214.31	5/8" I.R.
17	5350.8867	5019.3460	216.81	BRASS CAP
18	5271.5524	4922.0372	212.15	5/8" I.R.
19	5273.5964	4682.0532	213.32	1/2" I.R.
20	5272.7422	4631.9644	212.59	1/2" I.R.

UNDERGROUND UTILITIES:

UNDERGROUND UTILITY MARKS WERE VERY SPARSE AT THE TIME OF THE SURVEY. A NEW LOCATE WILL BE COORDINATED AND ANY ADDITIONAL LOCATES WILL BE ADDED TO THE BASE MAP.

LEGEND:

- PROJECT BOUNDARY
- PROPERTY LINE
- STREET CENTERLINE
- EXIST. CURB/GUTTER WITH SIDEWALK
- EXIST. WATER LINE
- EXIST. SEWER LINE
- EXIST. STORM DRAIN
- P.U.E. OR OTHER EASEMENT
- EXIST. FENCE
- EXIST. DITCH
- EXIST. TREE/VEGETATION
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING WATER METER
- EXISTING BLOW-OFF
- EXIST. UTILITY POLE
- EXIST. GUY WIRE ANCHOR
- EXISTING STREET LIGHT
- EXISTING MANHOLE
- EXISTING CLEAN-OUT
- EXISTING CATCH BASIN
- EXISTING FIELD INLET
- FOUND MONUMENT REFERENCE
- EXISTING WETLANDS

REGISTERED PROFESSIONAL LAND SURVEYOR
J. D. KENNEDY
No. 12151
Renewal 12/31/15

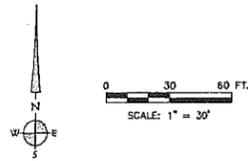
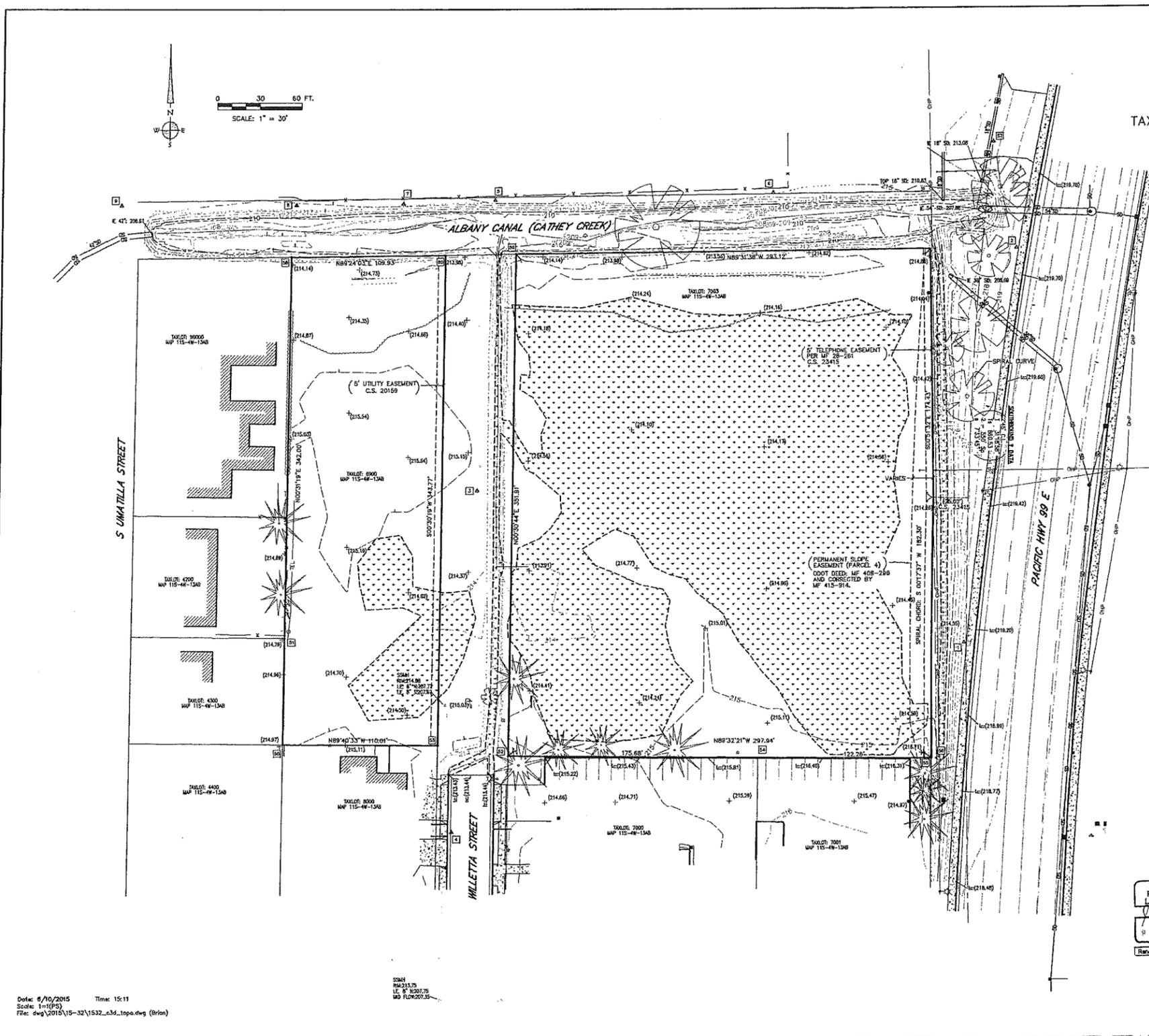
K & D ENGINEERING, Inc.
278 N.W. Hickory Street, P.O. Box 1928
Albany, Oregon 97321
(503) 382-2262

SHEET TITLE:
EXISTING CONDITIONS PLAN

DRAWN BY: --
CHECKED BY: --
SHEET

JOB NO. 2140284.00

CONDITIONAL USE SUBMITTAL: 09/14/2015



Date: 6/10/2015 Time: 15:11
Scale: 1"=30'
File: dwg\2015\15-32\1532_c3d_topo.dwg (Brian)

SWAN
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ALBANY POLICE DEPARTMENT

1175 SE JACKSON ST.
ALBANY, OR 97322

Mechanical/Electrical
INTERFACE ENGINEERING INC
708 SW 3RD AVE, SUITE 40C
PORTLAND, OR 97204
Phone: (503)382-2266
FAX: (503)382-2262

GENERAL NOTES

- ALL PUBLIC IMPROVEMENTS SHALL CONFORM TO THE CURRENT EDITION OF THE CITY OF ALBANY STANDARD CONSTRUCTION SPECIFICATIONS. THE CURRENT VERSION OF THE CITY OF ALBANY STANDARD CONSTRUCTION SPECIFICATIONS CAN BE FOUND ON THE CITY'S WEBSITE www.cityofalbany.net. PUBLIC IMPROVEMENTS SHALL BE CONSTRUCTED UNDER A SEPARATE SET OF PLANS ISSUED VIA A SITE IMPROVEMENT (SI) PERMIT PROVIDING FOR THE PRIVATE CONSTRUCTION OF PUBLIC INFRASTRUCTURE.
- ALL ON-SITE IMPROVEMENTS SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF ALBANY AND THE CURRENT AMERICAN PUBLIC WORKS ASSOCIATION STANDARDS FOR PUBLIC WORKS CONSTRUCTION.
- THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIPES, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES LAYING WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO CONDITION, SIZE AND LOCATION BY UNCOVERING, PROVIDING SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR TO NOTIFY ENGINEER IF THERE ARE ANY DISCREPANCIES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION PREVENTION SEDIMENT CONTROL (EPC) MEASURED IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE CITY OF ALBANY, AND VEGETATION/LANDSCAPING IS ESTABLISHED. FOR GUIDANCE, REFER TO THE CITY OF ALBANY EPC MANUAL WHICH CAN BE FOUND AT www.cityofalbany.net.
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE ROUTED SO THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- CONTRACTOR SHALL ADJUST ALL STRUCTURES IMPACTED BY CONSTRUCTION IMPROVEMENTS TO NEW FINISH GRADES.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) MUST COMPLY WITH O.R.S. 757.541 THROUGH 757.571; EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS SEVENTY-TWO (72) HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE. (OREGON UTILITY NOTIFICATION CENTER: 1-800-332-6344).
- WHERE CONNECTING TO AN EXISTING PIPE, AND PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXPOSE THE END OF THE EXISTING PIPE VERIFY THE LOCATION, SIZE, AND ELEVATION. NOTIFY ENGINEER OF ANY DISCREPANCIES.
- REQUEST BY THE CONTRACTOR FOR CHANGES TO THE PLANS MUST BE APPROVED BY THE ENGINEER.
- ALL CURB RADII ARE 3.0' UNLESS OTHERWISE NOTED.

LEGEND

- PROPERTY LINE
- EXISTING EASEMENT
- VERTICAL CURB
- EDGE OF PAVEMENT
- ROCK WALL

PAVEMENT LEGEND

- 3" AC OVER 12" AGGREGATE BASE
- 4" AC OVER 12" AGGREGATE BASE
- TUFFTRACK GRASSROAD PAVEMENT (OR APPROVED EQUAL) OVER 12" AGGREGATE BASE

KEYNOTES

- VERTICAL CURB PER DETAIL 1/CB.0
- CONCRETE SIDEWALK PER DETAIL 3/CB.0
- 4" WIDE WHITE PARKING STRIPE
- ADA COMPLIANT PARKING STALL PER DETAIL 4/CB.0
- SQUARE CURB RAMP PER DETAIL 7/CB.0
- LANDSCAPE AREA
- 12" WHITE CROSSWALK STRIPE
- 2" MAX. ROCK WALL
- CMU TRASH ENCLOSURE PER ARCHITECTURAL DRAWINGS
- 2" CURB BREAK PER DETAIL 2/CB.0
- COMMERCIAL DRIVEWAY PER SEPARATE PLANS
- CURB STOP PER DETAIL 10/CB.0
- SIDEWALK AND PLAZA AREAS PER LANDSCAPE DRAWINGS
- EXISTING TREE TO BE REMOVED
- CURB RAMP PER DETAIL 11/CB.0
- FENCE PER LANDSCAPE DRAWINGS
- PATIO AREA PER LANDSCAPE DRAWINGS
- STORMWATER PLANTER
- ROLLED CURB PER DETAIL 9/CB.0
- 2" WIDE TRUNCATED DOMES PER DETAIL 8/CB.0
- BIKE PARKING PER LANDSCAPE DRAWINGS

SITE DATA

EAST LOT: ZONE OP	
SITE AREA	109,908 SF (2.52 AC)
VACATED R-O-W	5,858 SF (0.13 AC)
BUILDING FOOTPRINT	23,391 SF (0.54 AC)
PAVED AREA	53,413 SF (1.22 AC)
LANDSCAPE AREA	33,104 SF (0.76 AC) (30.1%)
WEST LOT: ZONE RS-5	
SITE AREA	53,364 SF (1.22 AC)
VACATED R-O-W	20,785 SF (0.48 AC)
PAVED AREA	31,597 SF (0.72 AC)
LANDSCAPE AREA	21,767 SF (0.50 AC) (40.8%)

PARKING DATA

PUBLIC		BIKE	
STANDARD	28 SPACES	INTERIOR (SALLY PORT)	14 SPACES
ACCESSIBLE	4 SPACES	PUBLIC	4 SPACES
POLICE PARKING		POLICE	4 SPACES
STANDARD	131 SPACES		
ACCESSIBLE	2 SPACES		

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REVISIONS:
DATE REVISION REVISION DATE SHEET

SHEET TITLE:
SITE PLAN

DRAWN BY: BTS

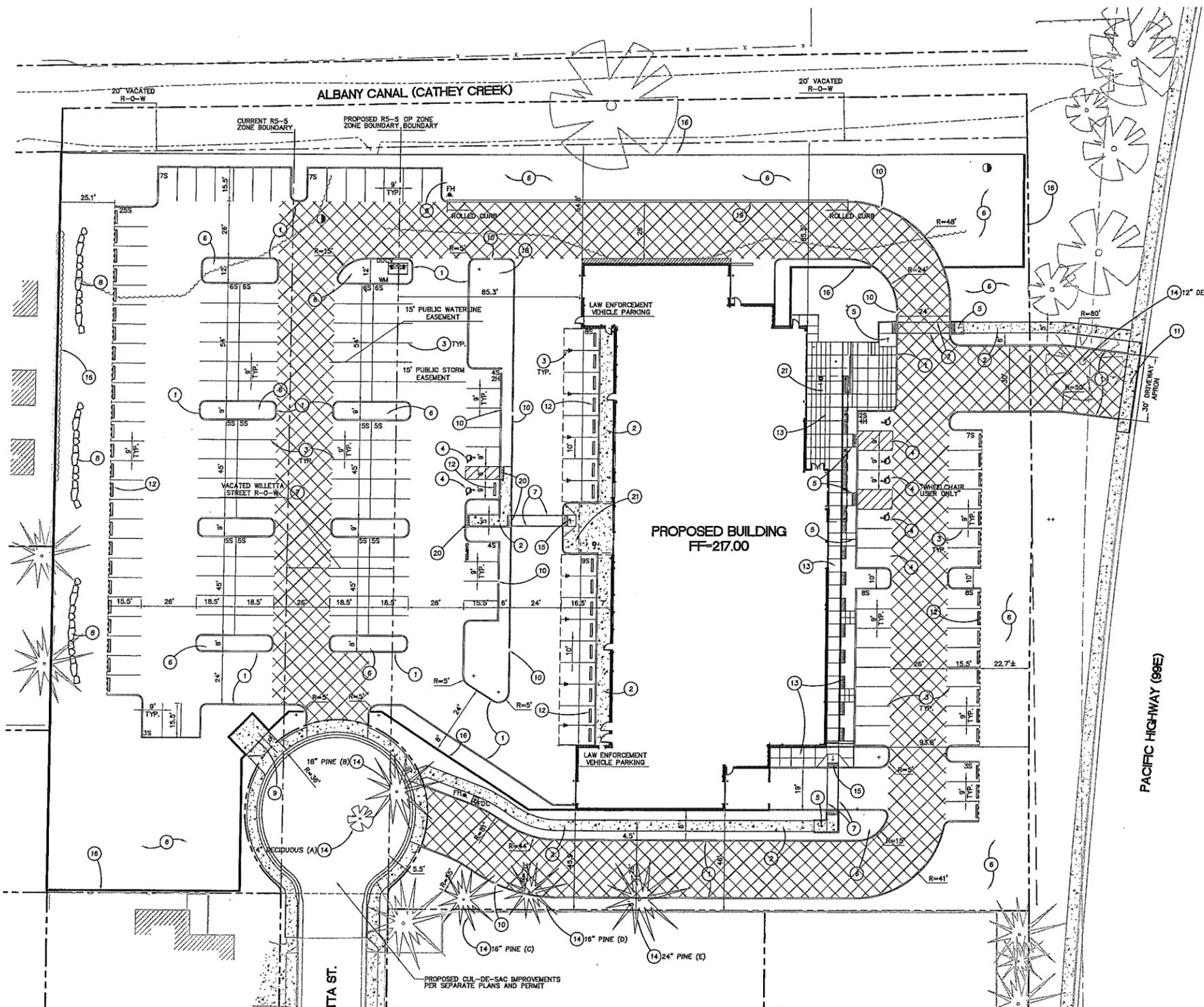
CHECKED BY: RVS

SHEET

C21

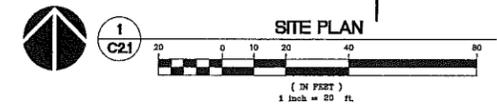
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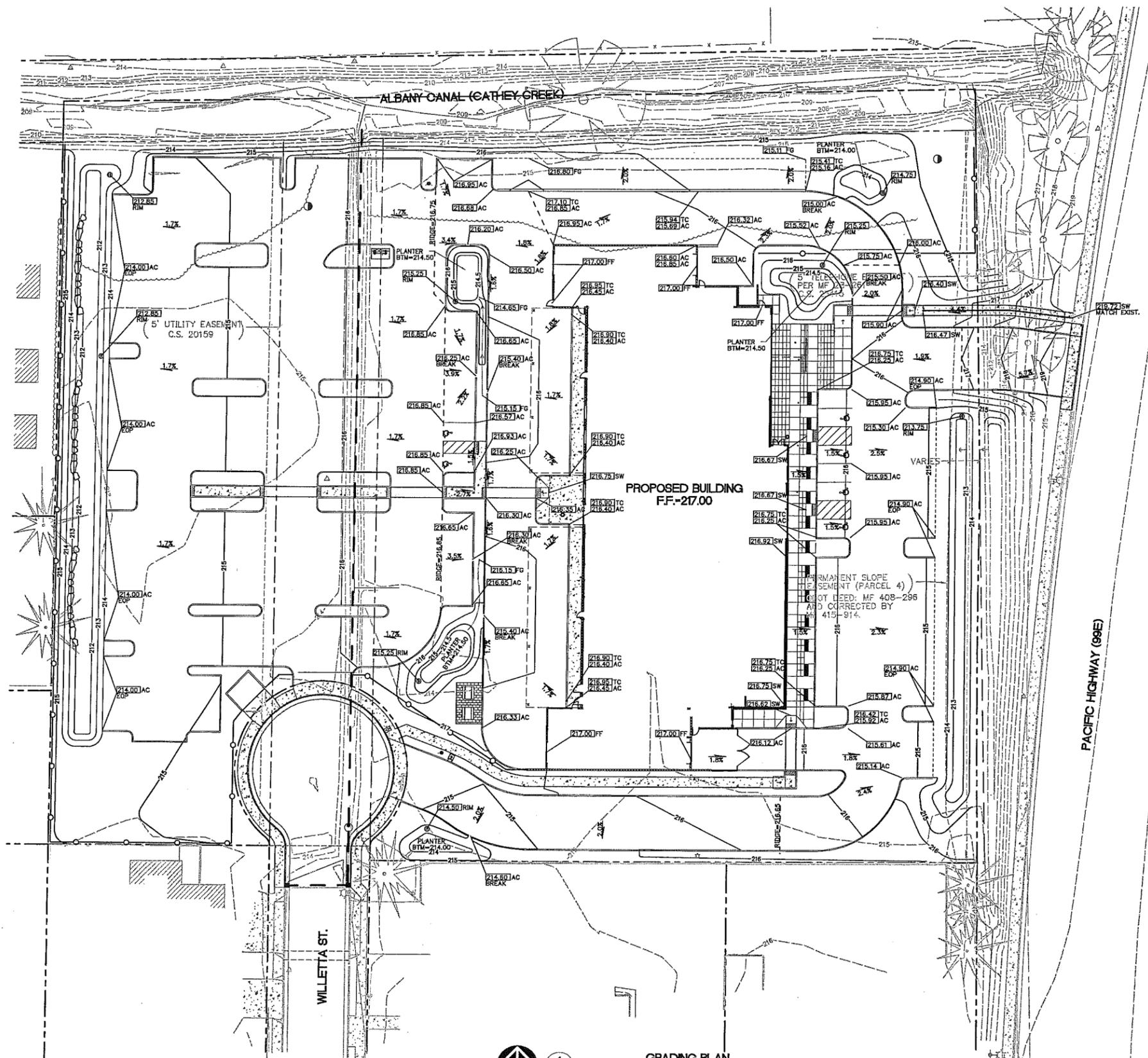
UPDATED 10/16/2015
CONDITIONAL USE SUBMITTAL: 09/14/2015



TREE INVENTORY:

TREE ID #	TYPE	TRUNK DIAMETER	CANOPY (S.F.)	REMOVE/RETAIN	ON-SITE/ROW
A	DECIDUOUS	4" DIAMETER	113 S.F.	REMOVE	ON-SITE
B	PINE	18" DIAMETER	706 S.F.	REMOVE	ON-SITE
C	PINE	16" DIAMETER	452 S.F.	REMOVE (DEAD)	ON-SITE
D	PINE	16" DIAMETER	452 S.F.	REMOVE	ON-SITE
E	PINE	24" DIAMETER	804 S.F.	REMOVE	ON-SITE
F	DECIDUOUS	24" DIAMETER	1,017 S.F.	REMOVE	ROW





GRADING NOTES

- ROUGH GRADING: BRING ALL FINISH GRADES TO APPROXIMATE LEVELS INDICATED. WHERE GRADES ARE NOT OTHERWISE INDICATED, FINISH GRADES ARE TO BE THE SAME AS ADJACENT SIDEWALKS, CURBS, OR THE OBVIOUS GRADE OF ADJACENT STRUCTURE. GRADE TO UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE GRADES ARE GIVEN. ROUND OFF SURFACES, AVOID ABRUPT CHANGES IN LEVELS. ROUGH GRADE TO ALLOW FOR DEPTH OF CONCRETE SLABS, WALKS, AND THEIR BASE COURSES. GRADE FOR PAVED DRIVES AND PAVED PARKING AREAS AS INDICATED AND SPECIFIED HEREIN, AND PROVIDE FOR SURFACE DRAINAGE AS SHOWN, ALLOWING FOR THICKNESS OF SURFACING MATERIAL.
- FINISH GRADING: AT COMPLETION OF JOB AND AFTER BACKFILLING BY OTHER CRAFTS HAS BEEN COMPLETED, REFILL AND COMPACT AREAS WHICH HAVE SETTLED OR ERODED TO BRING TO FINAL GRADES. GRADING TOLERANCES: ROUGH GRADE AT PAVED OR LANDSCAPED AREAS: ±0.1 FT. FINISH GRADE PRIOR TO PLACING FINAL SURFACING: ±0.03 FT.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) MUST COMPLY WITH O.R.S. 757.541 THROUGH 757.571; EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS 72 HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION PREVENTION SEDIMENT CONTROL (EPSC) MEASURED IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE CITY OF ALBANY, AND VEGETATION/LANDSCAPING IS ESTABLISHED. FOR GUIDANCE, REFER TO THE CITY OF ALBANY EPSC MANUAL WHICH CAN BE FOUND AT www.cityofalbany.net
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE SO ROUTED THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- SITE TOPSOIL SHALL BE STOCKPILED DURING CONSTRUCTION AND USED FOR LANDSCAPING.
- THE SURVEY INFORMATION SHOWN AS A BACKGROUND SCREEN ON THIS SHEET IS BASED ON A SURVEY BY K&O ENGINEERING AND IS SHOWN FOR REFERENCE ONLY. CONTRACTOR TO VERIFY ALL EXISTING CONDITIONS WITH HIS OWN RESOURCES PRIOR TO START OF ANY CONSTRUCTION.
- CONTRACTOR TO COORDINATE GRADES AT ENTRANCE WITH ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.
- 1.5% MAXIMUM SLOPE AT ALL ADA-COMPLIANT PARKING SPACES AND LOADING ZONES.
- 5% MAX SLOPE (EXCLUDING RAMPS) AT PEDESTRIAN SIDEWALK CONNECTIONS BETWEEN PUBLIC R.O.W. AND BUILDING ENTRANCES.
- WHERE SLOPES ARE STEEPER THAN 3:1, CONTRACTOR SHALL INSTALL JUTE MATTING. SLOPE SHALL BE PREPARED TO ENSURE COMPLETE AND DIRECT CONTACT OF MATTING WITH SOIL. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

SITE PREPARATION NOTES

- DUE TO THE WIDESPREAD PRESENCE OF CLAYEY SOILS ON-SITE, AFTER STRIPING THE SITE A MINIMUM OF 6" TO REMOVE ROOTS AND SSD, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE OVER-EXCAVATION AN ADDITIONAL 24" BELOW PAVEMENTS AND BUILDING FOUNDATIONS AND FOOTINGS. REFER TO THE GEOTECHNICAL REPORT FOR THE ALBANY FIRE STATION PROVIDED BY FOUNDATION ENGINEERING, INC. FOR RECOMMENDATIONS AND REQUIREMENTS.

LEGEND

PROPERTY LINE	---
EXISTING CONTOUR	--- 214 ---
PROPOSED 1-FT CONTOUR	--- 214 ---
PROPOSED 5-FT CONTOUR	--- 215 ---
EXISTING EASEMENT	---
VERTICAL CURB	---
EDGE OF PAVEMENT	EOP
ASPHALT	AC
CONCRETE	CONC
FINISHED GRADE	FG
FINISHED FLOOR	FF
TOP OF CURB	TC
SIDEWALK	SW

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REVISIONS:

NO.	REVISION	DATE

SHEET TITLE:
GRADING PLAN

DRAWN BY: BTS
CHECKED BY: RYS
SHEET

C2.2

JOB NO. 2140284.00

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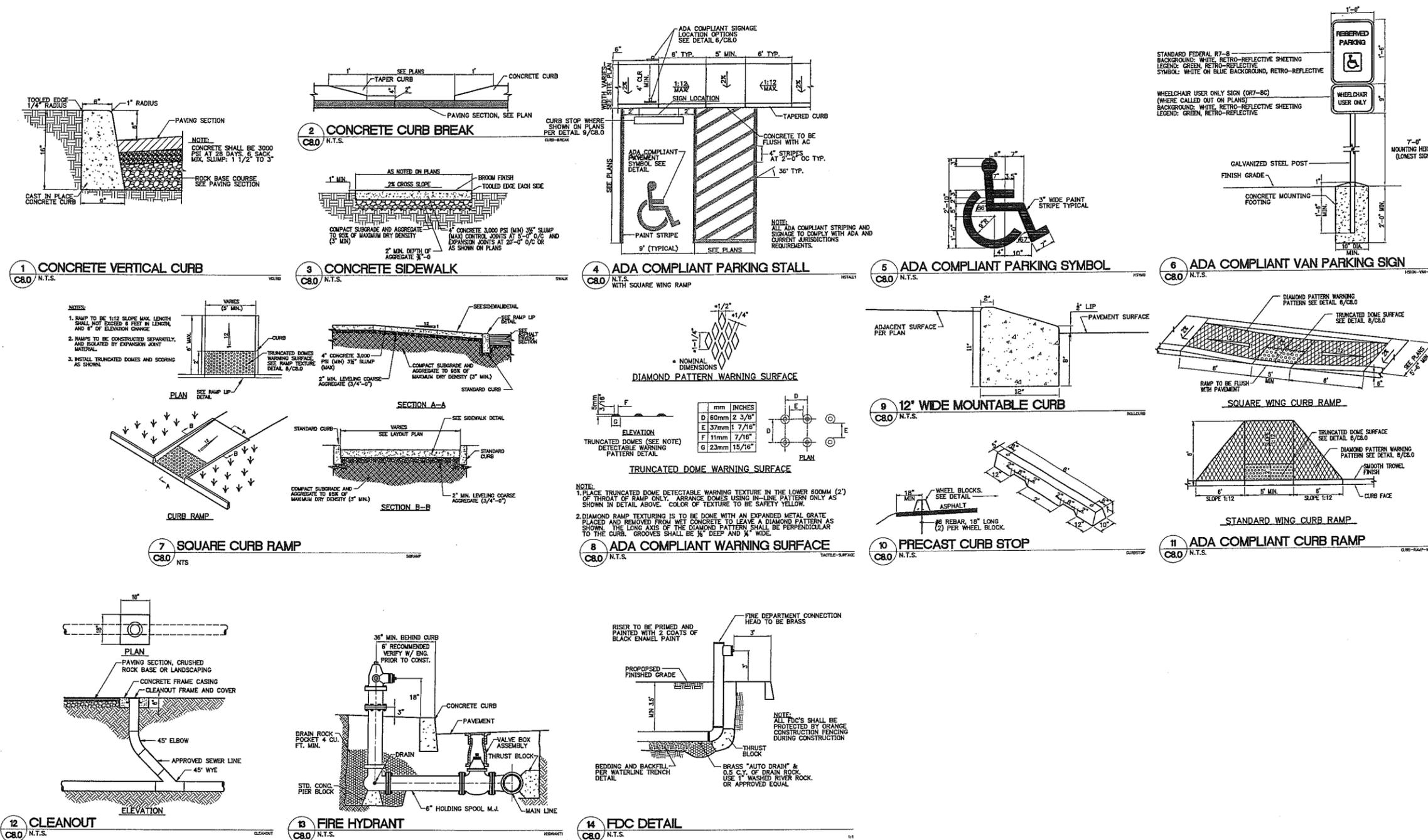
NO.	REVISION	DATE

SHEET TITLE:
SITE DETAILS

DRAWN BY: BTS
CHECKED BY: RVS
SHEET

C8.0

JOB NO. 2140284.00





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Project

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SW ALBANY, OR, 97321

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INTERFACE ENGINEERING
100 SW MAIN STREET
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PORTLAND, OR 97204

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Revision Schedule	
Revision	Issue Date

SHEET TITLE:
**SITE PLAN -
PHOTOMETRICS**

DRAWN BY: Author

CHECKED BY: Checker

SHEET

E1.1PH

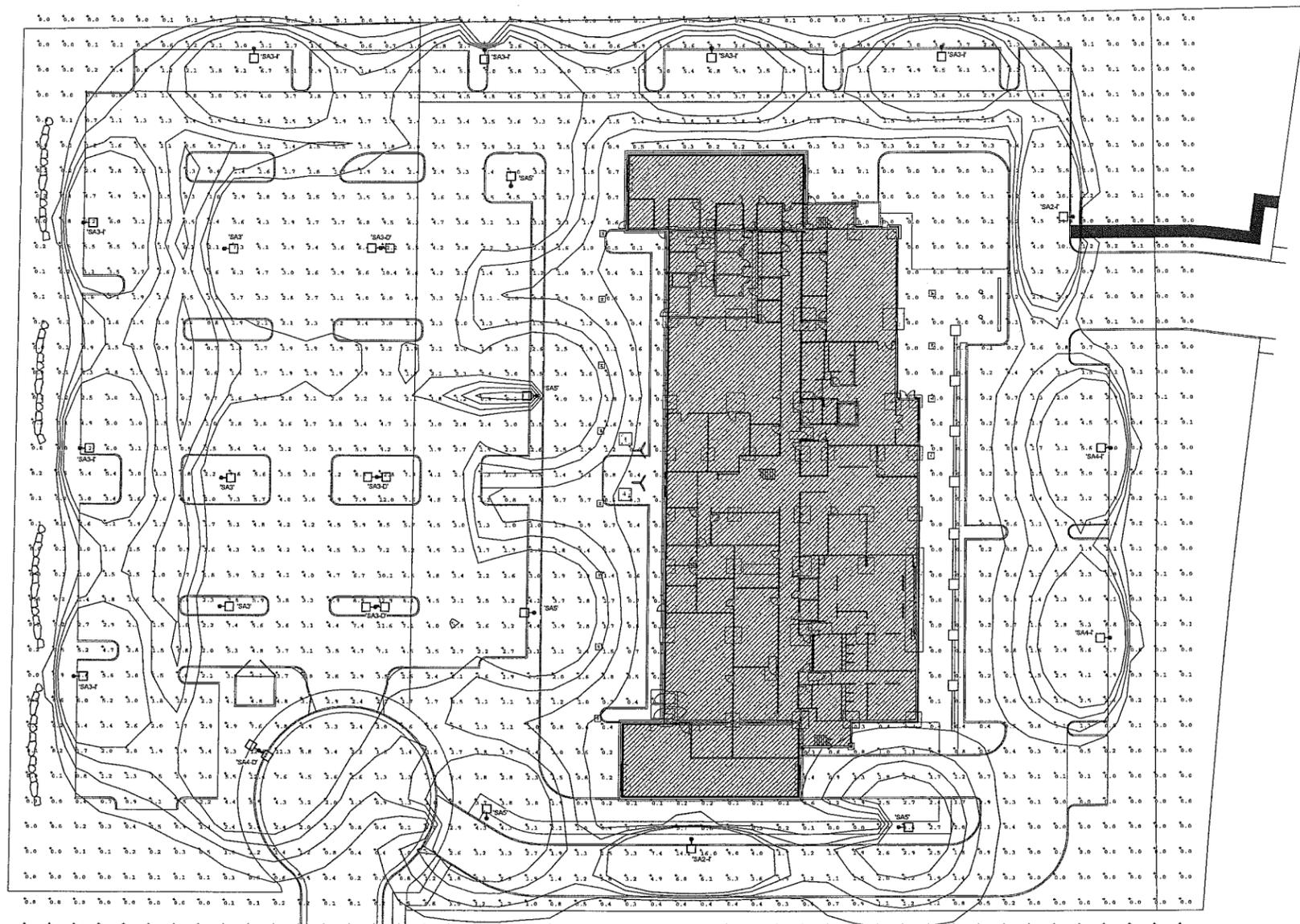
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CONTRACT 7

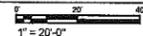
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SITE LUMINAIRE SCHEDULE											
TYPE	DESCRIPTION	HOUSING	SHIELDING	MOUNTING	FINISH	UL/IP RATING	BALLAST	LAMP(S)	INPUT WATTS	MFG/CATALOG #	NOTES
SA24	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 2 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	INTERNAL HOUSESIDE SHIELD	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 11,300 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA3	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 3 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA34	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 3 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	INTERNAL HOUSESIDE SHIELD	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA3-0	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 3 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	DOUBLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA4	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 4 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,800 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA44	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 4 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	INTERNAL HOUSESIDE SHIELD	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,800 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA4-0	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 4 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	DOUBLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,800 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	
SA5	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 5 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 8.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 11,300 LUMENS, 4000K	105W	GARDCO GULLWING GL18 SERIES, OR APPROVED	

NOTES:
1 SPECIFIED MANUFACTURERS ARE APPROVED TO SUBMIT BID. INCLUSION DOES NOT RELIEVE MANUFACTURER FROM SUPPLYING PRODUCT AS DESCRIBED.
2 PROVIDE SUBMITTALS THAT INCLUDE THE LUMINAIRE, LAMP AND BALLAST INFORMATION WITH APPLICABLE OPTIONS CLEARLY CHECKED OR HIGHLIGHTED. SUBMITTALS NOT INCLUDING THIS INFORMATION WILL BE RETURNED AS REJECTED BY THE ENGINEER OF RECORD.
3 REMOTE BALLAST DRIVERS, UL LISTED FOR THEIR APPLICATION. BALLAST DRIVERS MARKED AS UL RECOGNIZED COMPONENT BUT NOT UL LISTED ARE SUBJECT TO REMOVAL AND REPLACEMENT AT NO COST TO OWNER.



1 SITE PLAN - PHOTOMETRICS



CONDITIONAL USE SUBMITTAL: 09/14/15



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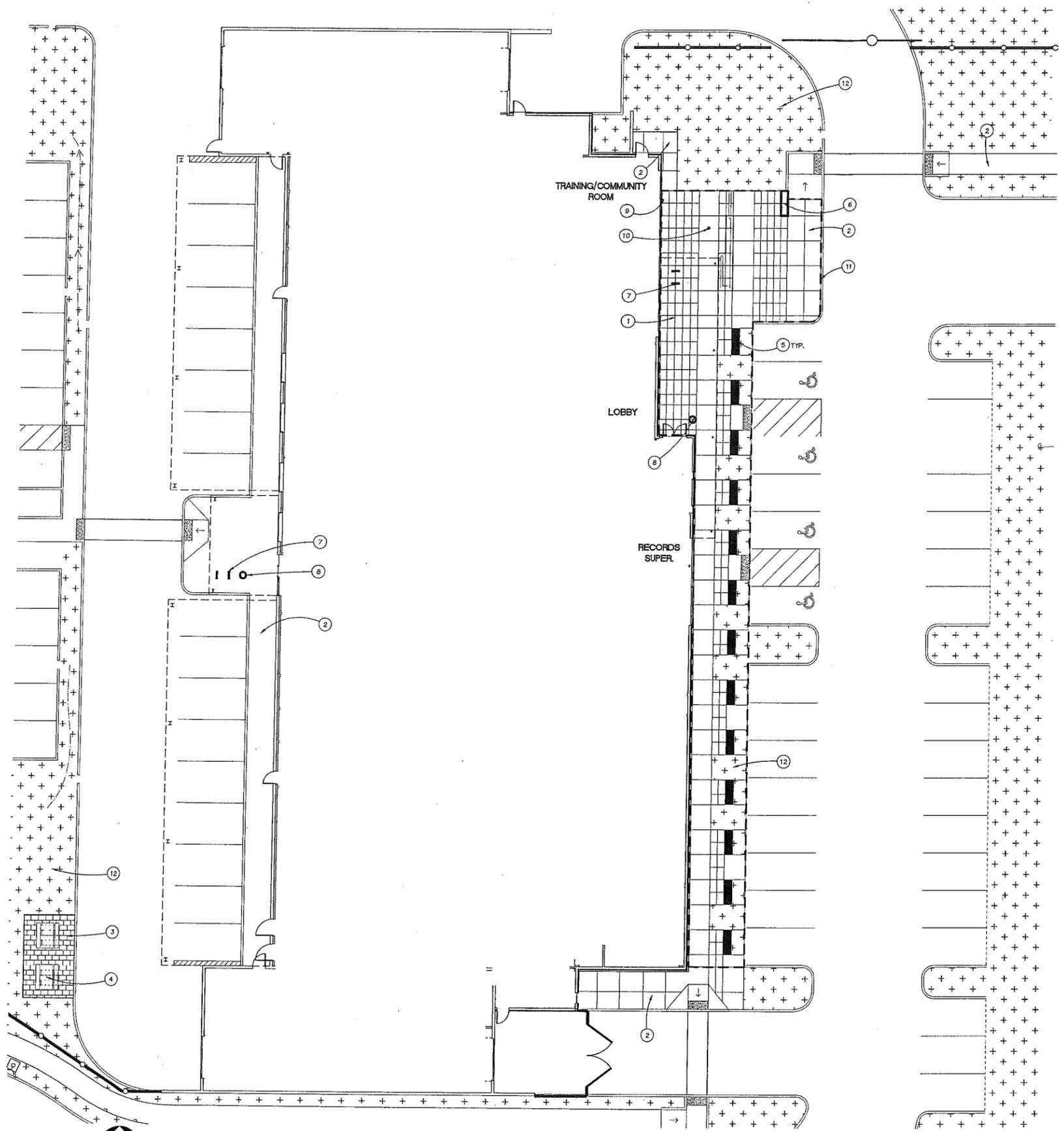
REVISIONS:
DATE REVISIONS REVISION DELTA SHEET CLOSING DATE

SHEET TITLE:
PLAZA LAYOUT AND MATERIALS PLAN

DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L21

JOB NO. 2140284.00



KEYNOTES, LAYOUT AND MATERIALS SCHEDULE

- 1 [Paving Type A pattern] PAVING TYPE A - COLORED CONCRETE
SEE DETAIL X/LB.1
- 2 [Paving Type B pattern] PAVING TYPE B - SCORED CONCRETE
SEE DETAIL X/LB.1
- 3 [Gravel Mulch pattern] GRAVEL MULCH WITH PAVERS UNDER TABLE LEGS
- 4 [Picnic Table symbol] PICNIC TABLE
- 5 [Concrete Wall with Strip Lighting symbol] CONCRETE SCAT WALL WITH STRIP LIGHTING
- 6 [Concrete Sign Wall symbol] CONCRETE SIGN WALL
- 7 [Bicycle Rack symbol] BICYCLE RACK
- 8 [Trash Receptacle symbol] TRASH RECEPTACLE
- 9 [Ash Receptacle symbol] ASH RECEPTACLE
- 10 [Flag Pole symbol] FLAG POLE
- 11 [Plaza Area to Incorporate Storm Water Art Feature Elements symbol] PLAZA AREA TO INCORPORATE STORM WATER ART FEATURE ELEMENTS
- 12 [Area to Be Planted symbol] AREA TO BE PLANTED
SEE PLANTING PLANS L4.0 - L4.4

1 PLAZA LAYOUT AND MATERIALS PLAN
L21
1" = 12' FT



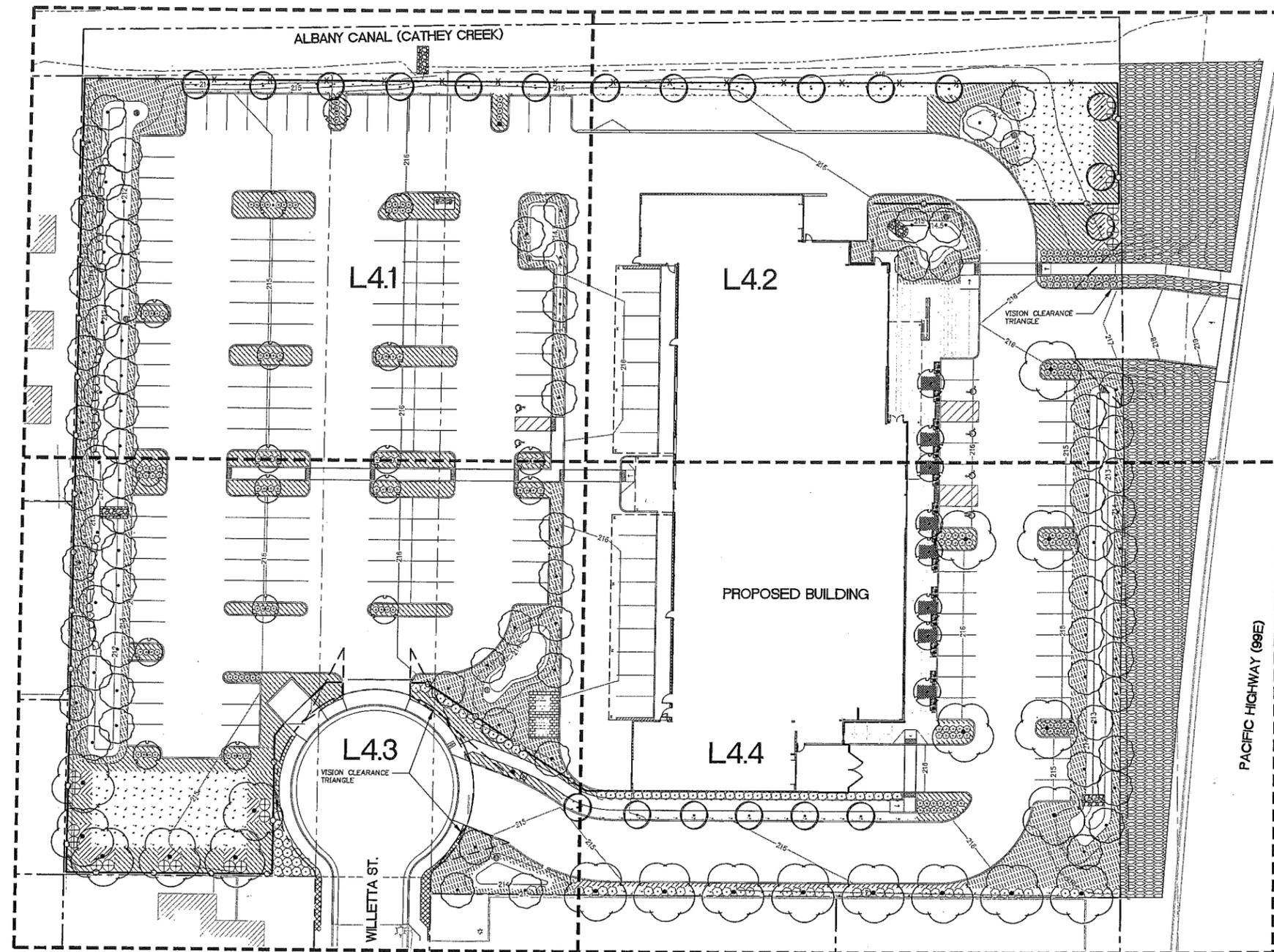
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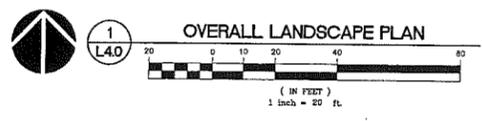
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FAX: (503)382-2262



PLANT SCHEDULE			
TREES	BOTANICAL NAME / COMMON NAME	SIZE	SPACING
	ACER CIRCINATUM / VINE MAPLE	2" CAL. B&B, 10' HT. MIN.	
	GINKGO BILOBA 'PRINCETON SENTRY' / PRINCETON SENTRY GINKGO	2" CAL. B&B, 10' HT. MIN.	
	QUERCUS COCCINEA / SCARLET OAK	2" CAL. B&B, 10' HT. MIN.	
	QUERCUS PALUSTRIS 'GREEN PILLAR' / GREEN PILLAR OAK	2" CAL. B&B, 10' HT. MIN.	
SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	SPACING
	MYRTICA CALIFORNICA / PACIFIC WAX MYRTLE	5 GAL	60" o.c.
	NANDINA DOMESTICA 'GULF STREAM' TM / HEAVENLY BAMBOO	3 GAL	30" o.c.
	VIBURNUM DAVIDII / DAVID VIBURNUM	3 GAL	36" o.c.
	VIBURNUM TINUS 'SPRING BOUQUET' / SPRING BOUQUET LAURESTINUS	5 GAL	48" o.c.
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
	BARK MULCH	N/A	
	JUNIPERUS HORIZONTALIS 'BLUE CHIP' / BLUE CHIP JUNIPER	4" POT	24" o.c.
	PENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS	1 GAL	18" o.c.
	ROCK MULCH	N/A	
	RUBUS CALYCINOIDES / GREEN CARPET RASPBERRY	4" POT	12" o.c.
	ODOT APPROVED SEED MIX	SEED	
STORMWATER AREAS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
	ZONE A PLANTING AREA HERBACEOUS PLANTS CAREX OBNUPTA / SLOUGH SEDGE JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	4" POT 4" POT	12" o.c. 12" o.c.
	ZONE B PLANTING AREA GROUNDCOVER ARCTOSTAPHYLOS UVA-URSI / KINKINNICK JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	1 GAL 4" POT	12" o.c. 12" o.c.
	SHRUBS CORNUS SERICEA / RED TWIG DOGWOOD MAHONIA REPENS / CREEPING MAHONIA ROSA NUTKANIA / NODDIA ROSE SPIRAEA BETULIFOLIA / BIRCHLEAF SPIRAEA SYMPHORICARPOS ALBA / SNOWBERRY	30" HT. MIN. 1 GAL 30" HT. MIN. 1 GAL 1 GAL 1 GAL	48" o.c. 18" o.c. 48" o.c. 24" o.c. 36" o.c.



LANDSCAPE NOTES

- GENERAL NOTES:**
- CONTRACTOR SHALL CONFIRM ALL EXISTING CONDITIONS PRIOR TO COMMENCING WORK AND NOTIFY THE OWNER OR OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES OR CONFLICTS.
 - CONTRACTOR SHALL VERIFY EXISTING TREES IN THE FIELD PRIOR TO COMMENCEMENT OF WORK.
 - CONTRACTOR SHALL VERIFY INVERT ELEVATIONS OF ALL UNDERGROUND UTILITIES AND NOTIFY LANDSCAPE ARCHITECT IF THERE ARE ANY DISCREPANCIES WITH PLANTING ROOT ZONES. TO LOCATE SITE UTILITIES PRIOR TO PROPOSED EXCAVATION CALL 1-800-332-2344.
 - CONTRACTOR SHALL COORDINATE WITH THE OWNER ANY DISRUPTION TO VEHICULAR CIRCULATION PRIOR TO COMMENCEMENT OF ANY WORK.
 - CONTRACTOR SHALL KEEP PEDESTRIAN TRAVEL WAYS AND ACCESS TO ALL STRUCTURES PROTECTED AT ALL TIMES.
 - CONTRACTOR SHALL REPLACE OR REPAIR DAMAGE TO EXISTING CONCRETE CURB, ASPHALT PAVING, OR OTHER STRUCTURES TO PRE CONSTRUCTION CONDITIONS.
 - ALL LANDSCAPE AREAS SHALL BE INSTALLED AND MAINTAINED AS A MINIMUM TO STANDARDS ACCORDING TO CITY REVISED CODE.
 - ALL NEW PLANTING AREAS TO BE IRRIGATED BY AUTOMATIC IRRIGATION SYSTEM. REFERENCE L3.1 FOR IRRIGATION PLAN AND NOTES.

- PLANTING NOTES:**
- ALL EXISTING TREES, PLANTS, AND ROOTS SHALL BE PROTECTED FROM DAMAGE DURING ANY CONSTRUCTION PREPARATION, REMOVAL OR INSTALLATION ACTIVITIES WITHIN AND ADJACENT TO PROJECT LIMITS.
 - IF DISTURBANCE IS NECESSARY AROUND EXISTING TREES, CONTRACTOR SHALL PROTECT THE CROWN AND ALL WORK WITHIN THE TREE DRIFTPZONE SHALL BE LIMITED TO THE USE OF HAND TOOLS AND MANUAL EQUIPMENT ONLY.
 - REPLACE, REPAIR AND RESTORE DISTURBED LANDSCAPE AREAS DUE TO GRADING, TRENCHING OR OTHER REASONS TO PRE CONSTRUCTION CONDITION AND PROVIDE MATERIAL APPROVED BY THE OWNER OR OWNER'S REPRESENTATIVE.
 - EXISTING AREAS PROPOSED FOR NEW PLANT MATERIAL SHALL BE CLEARED AND LEGALLY DISPOSED UNLESS NOTED OTHERWISE.
 - ALL PLANT MATERIAL SHALL BE HEALTHY NURSERY STOCK, WELL BRANCHED AND ROOTED, FULL FOLIAGE, FREE FROM INSECTS, DISEASES, WEEDS, WEED ROT, INJURIES AND DEFECTS WITH NO LESS THAN MINIMUMS SPECIFIED IN AMERICAN STANDARDS FOR NURSERY STOCK, ANSI Z601-2004.
 - ALL LANDSCAPED AREAS SHALL BE COVERED BY A LAYER OF ORGANIC MULCH TO A MINIMUM DEPTH OF 2-INCHES.
 - A SOILS ANALYSIS, BY AN INDEPENDENT SOILS TESTING LABORATORY RECOGNIZED BY THE STATE DEPARTMENT OF AGRICULTURE, SHALL BE USED TO RECOMMEND AN APPROPRIATE PLANTING SOIL AND/OR SPECIFIED SOIL AMENDMENTS.
 - TOPSOIL SHALL BE AMENDED AS RECOMMENDED BY AN INDEPENDENT SOILS TESTING LABORATORY AND AS OUTLINED IN THE SPECIFICATION.

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REVISIONS:

NO.	REVISIONS	REVISION DATE

SHEET TITLE:
**OVERALL
LANDSCAPE
PLAN**

DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L4.0

JOB NO. 2140284.00



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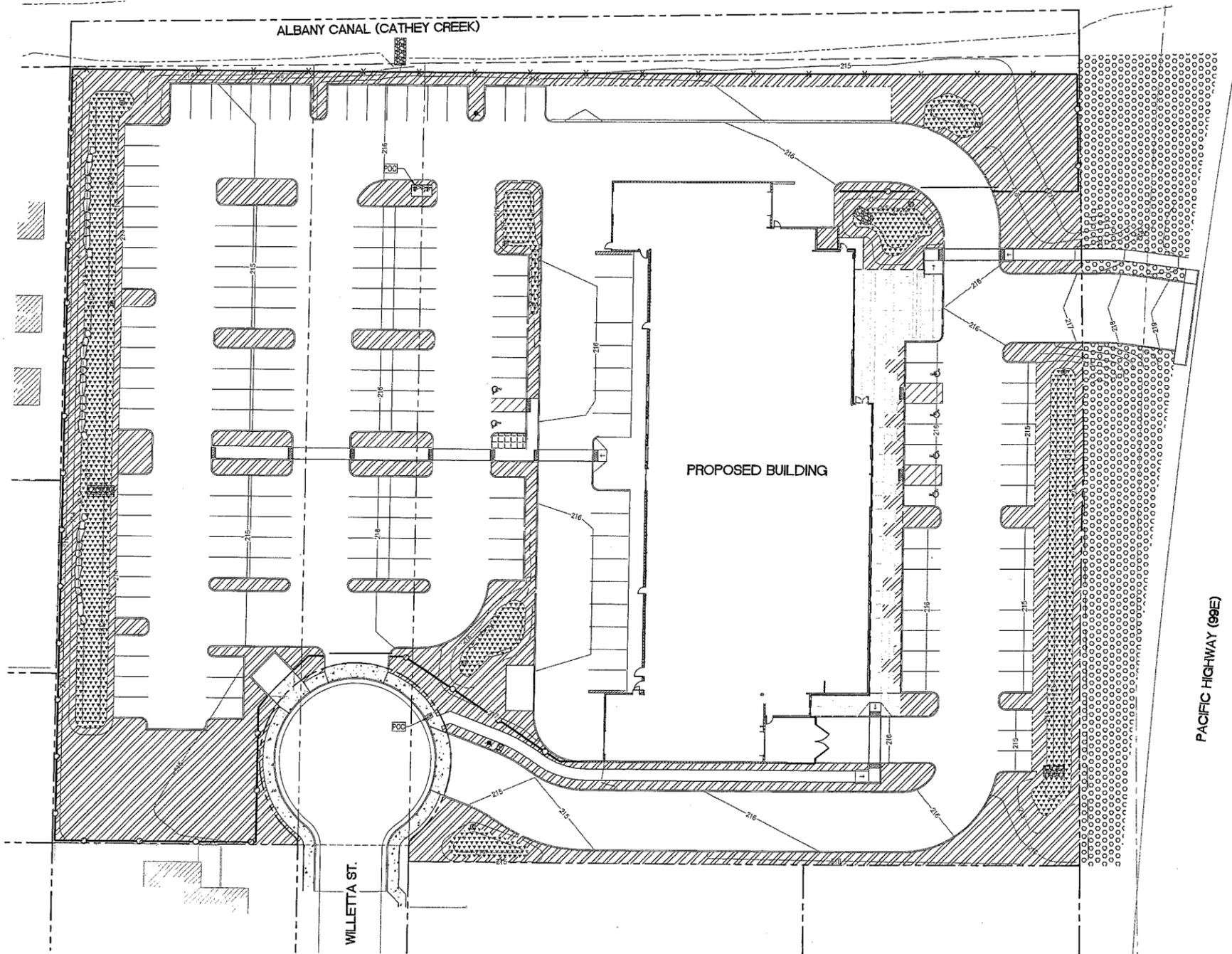
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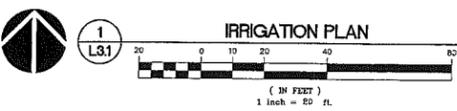


IRRIGATION SCHEDULE

SYMBOL	DESCRIPTION MANUFACTURE / TYPE	SHEET #
POC	POINT OF CONNECTION	L8.1
[Hatched pattern]	SPRAY IRRIGATION, ROTARY SPRAY NOZZLES	
[Dotted pattern]	SPRAY IRRIGATION AT STORMWATER AREAS, ROTARY SPRAY NOZZLES	
[Cross-hatched pattern]	TEMPORARY IRRIGATION	

IRRIGATION NOTES

1. A FULLY AUTOMATIC IRRIGATION SYSTEM TO BE DESIGNED, BUILT AND INSTALLED BY OTHERS TO MAINTAIN ALL LANDSCAPE MATERIAL.
2. ALL NEW LANDSCAPE AREAS TO BE IRRIGATED WITH A FULLY AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
3. IRRIGATION SYSTEM SHALL BE DESIGNED SO THAT PLANTING BEDS, SLOPED BANKS, STORM WATER FACILITIES AND LAWN ZONES ARE ON SEPARATE CONTROL VALVES TO FACILITATE THE DIFFERENT WATER REQUIREMENTS OF EACH AREA.
4. IRRIGATION SHALL BE INSTALLED SIMULTANEOUSLY WITH PLANTING TO ENSURE PLANTS RECEIVE ADEQUATE WATER AT TIME OF INSTALLATION.
5. VALVES SHALL BE WIRED AND INSTALLED PER MANUFACTURER'S RECOMMENDED INSTALLATION PROCEDURES AND CONNECTED TO THE IRRIGATION CONTROLLER.
6. IRRIGATION SYSTEM SHALL PERFORM WITHIN THE TOLERANCES AND SPECIFICATIONS OF MANUFACTURERS RECOMMENDATIONS.
7. ALL IRRIGATION PIPE MATERIAL AND INSTALLATION SHALL CONFORM TO APPLICABLE CODE FOR PIPING AND COMPONENT REQUIREMENTS.
8. SYSTEM SHALL BE DESIGNED TO SUPPLY MANUFACTURER'S SPECIFIED MINIMUM OPERATING PRESSURE TO FARTHEST EMITTER FROM WATER METER.
9. REFERENCE L8.1 FOR IRRIGATION DETAILS FOR POINT OF CONNECTION AND BACKFLOW PREVENTION INFORMATION.
10. IRRIGATION SHALL BE WINTERIZED THROUGH LOW PRESSURE, HIGH VOLUME AIR BLOWOUT CONNECTION THROUGH QUICK COUPLER.



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NO.	ISSUES	REVISION	DATE

SHEET TITLE:
**IRRIGATION
PLAN**

DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L3.1

JOB NO. 2140284.00

REVISIONS:

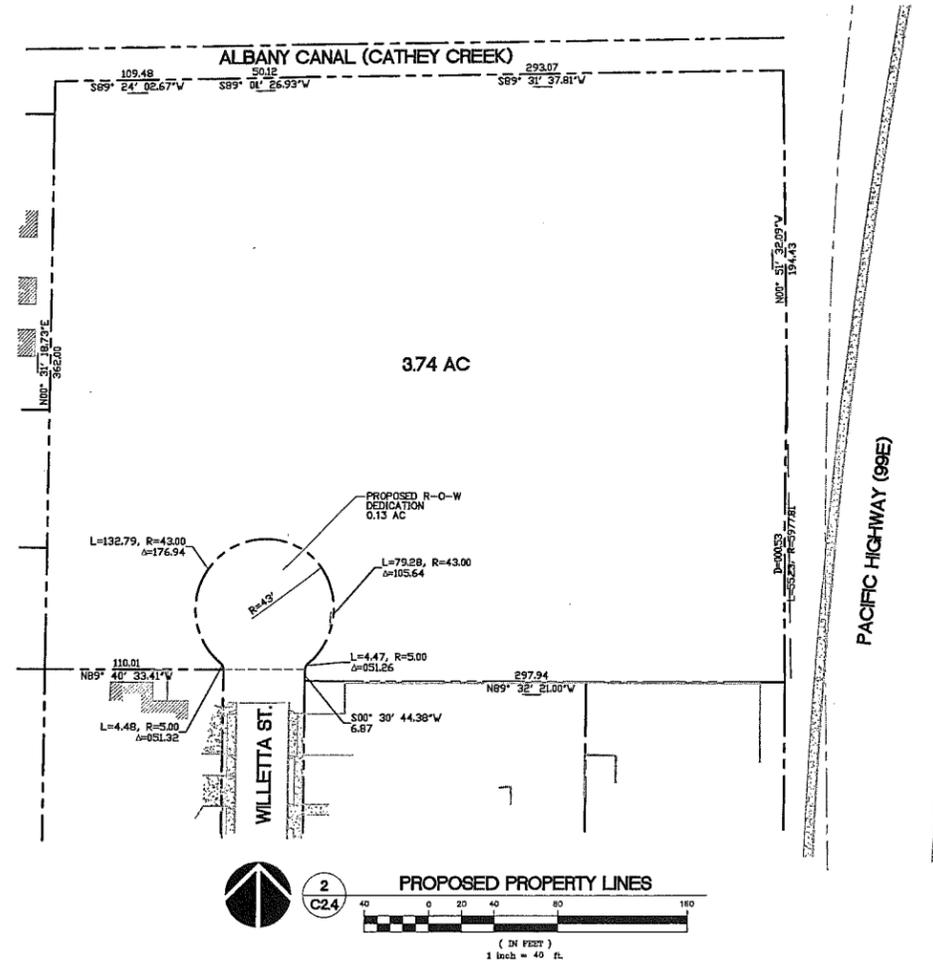
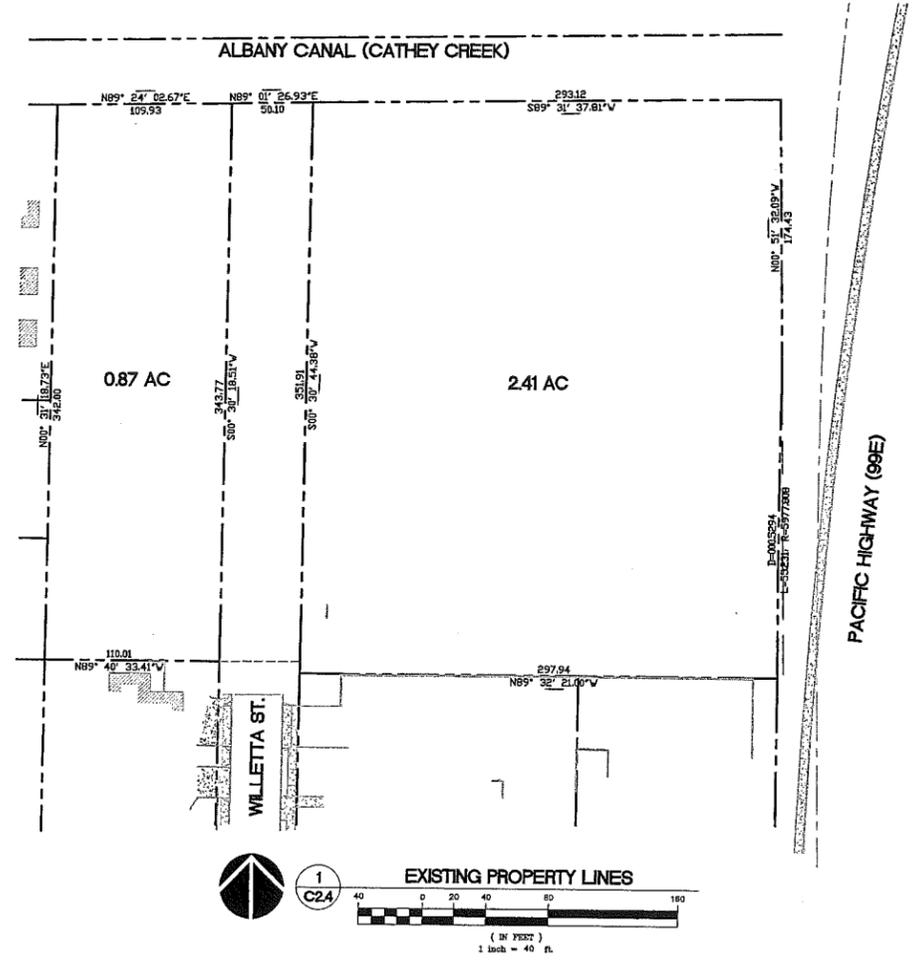
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SHEET TITLE:
TENTATIVE
REPLAT

DRAWN BY: BTS
CHECKED BY: RVS
SHEET

C2.4

JOB NO. 214028400





TO: Albany City Council

VIA: Wes Hare, City Manager
Jeff Blaine, P.E., Public Works Engineering and Community Development Director *JB*

FROM: Bob Richardson, Planning Manager *BR*
Melissa Anderson, Project Planner *MA*

DATE: January 6, 2016, for the January 13, 2016, City Council Meeting

SUBJECT: Public Hearing (Planning Files VC-03-15, CU-08-15, and RL-11-15)

RELATES TO STRATEGIC PLAN THEME:

- A Safe City
- Great Neighborhoods
- Healthy Economy

Action Requested:

Staff recommends that the City Council approve with conditions the proposed land use applications to construct a new Albany Fire Station, as presented in the attached staff report, and adopt an ordinance vacating portions of 6th Avenue SE as proposed by the applicant and described in the ordinance exhibits.

Discussion:

During the January 13, 2016, City Council meeting, the Council will hold a public hearing on the proposal to construct a new 24,265 square-foot fire station to replace existing Fire Station 11 in downtown Albany. As described in detail in the attached staff report to the Planning Commission, the following four land use applications are associated with the subject proposal.

1. A Street Vacation for the westernmost 194 feet of 6th Avenue east of Lyon Street;
2. A Conditional Use Review for a Community Service land use (fire station) in the Hackleman-Monteith zone;
3. A Site Plan Review for a Community Service land use in the Lyon-Ellsworth zone; and
4. A Replat to remove property lines and combine six parcels into one.

The applications have been processed concurrently through the Type IV application review process in accordance with ADC 1.200(3). This process is required for Vacation proposals and includes review and a recommendation by the Planning Commission prior to a final local decision made by the City Council. This process also provides the most opportunity for public participation in the decision-making process.

Following the Type IV process and procedures outlined in Oregon Revised Statutes Chapter 271, on July 8, 2015, the City Council initiated proceedings to vacate a portion of 6th Avenue SE. On November 16, 2015, the Planning Commission held a duly advertised public hearing and voted 7-0 to recommend that the City Council approve with conditions the land use applications noted above.

Budget Impact:

None

MA:rk
Attachments (3)

ORDINANCE NO. _____

AN ORDINANCE VACATING A PORTION OF THE SIXTH AVENUE RIGHT-OF-WAY, IN ALBANY, OREGON; AND ADOPTING FINDINGS.

WHEREAS, the City Council initiated proceedings to vacate a portion of 6th Avenue SE on July 8, 2015; and

WHEREAS, the vacated area will revert to the properties immediately north and south of the 6th Avenue SE right-of-way. The northern half will go to the northern properties and the southern half will go to the southern properties; and

WHEREAS, notices of public hearing were mailed and posted on the site on November 2, 2015; and published in the *Albany Democrat-Herald* on November 2, 2015 and November 9, 2015 as required by state and local law; and

WHEREAS, the Albany Planning Commission held a public hearing on November 16, 2015 and recommended that the City Council approve the proposed street vacation; and

WHEREAS, the Albany City Council held a public hearing on January 13, 2016, reviewed the testimony presented at the public hearing and the findings in the staff report and deliberated on the vacation request (file VC-03-15).

NOW, THEREFORE, THE PEOPLE OF THE CITY OF ALBANY DO ORDAIN AS FOLLOWS:

- Section 1: Subject Property. The western 194.12 feet of the 6th Avenue SE right-of-way between Blocks 6 and 7 of the Eastern Addition to the Albany Subdivision, lying immediately east of the Lyon Street right-of-way is hereby vacated, as presented in Exhibit A of this Ordinance.
- Section 2: Findings. The Findings, Conclusions, and Conditions in the Staff Report concerning VC-03-15 are hereby adopted in support of this decision as presented in Exhibit B of this Ordinance.
- Section 3: Easement Retained. The City retains a public utility easement and storm drainage easement over the areas being vacated.
- Section 4: The City Recorder shall, within 10 days of the effective date of this ordinance, file a certified copy of the ordinance with the Linn County Clerk, Linn County Assessor, and Linn County Surveyor (ORS 271.150). The petitioner for the vacation shall bear the recording costs.

Passed by the Council: _____

Approved by the Mayor: _____

Effective Date: _____

Mayor

ATTEST:

City Clerk

Albany Fire Department - 6th Avenue Vacation



See Staff Report:

Albany Fire Station
Street Vacation, Conditional Use Review, Site Plan Review and Replat
Planning Files VC-03-15, CU-08-15, and RL-11-15

Dated November 6, 2015

CITY OF ALBANY
PLANNING COMMISSION
Municipal Court Room, 333 Broadalbin Street SW
Monday, November 16, 2015
5:15 p.m.

MINUTES

Planning Commissioners present: Linsey Godwin (arrived at 5:20 p.m.), Sue Goodman, Roger Phillips, Cordell Post, Larry Tomlin, Dala Rouse, Dan Sullivan

Planning Commissioners absent: Wendy Ezell (excused), one vacancy

Staff present: Bob Richardson, Planning Manager; David Martineau, Lead Planner; Melissa Anderson, Planner II; Jim Delapoer, City Attorney; Staci Belcastro, City Engineer; Ron Irish, Transportation Systems Analyst; Jeff Blaine, Public Works Engineering and Community Development Director; John Bradner, Fire Chief; Mario Lattanzio, Police Chief; Marilyn Smith, Public Information Officer; Rachel Kutschera, Administrative Assistant I

CALL TO ORDER

Chair Tomlin called the meeting to order at 5:15 p.m.

COMMENTS FROM THE PUBLIC

None

QUASI-JUDICIAL PUBLIC HEARING #1

Development of a New Albany Police Station (VC-02-15, CU-07-15, and RL-10-15):

- 1) Street Vacation of the abutting portion of Willetta Street SW where it terminates at the site, plus the south half of the Cathey Creek right-of-way abutting the site;
- 2) Replat to combine two parcels plus the vacated portions of Willetta Street and the Cathey Creek drainage right-of-way into one parcel under the same ownership
- 3) Conditional Use Review for new construction of a 40,367 square-foot police headquarters; and

Chair Tomlin opened the public hearing at 5:18 p.m.

Declarations by the Commission:

Conflicts of Interest: None
Ex Parte Contact: None
Site Visit: None
Abstain: None
Challenges: None

Staff Report:

Lead Planner David Martineau provided a summary of the case. Staff-proposed conditions of approval were included for the Commission's consideration (see Agenda Packet).

Questions from the Commission:

Commissioner Rouse asked how high fences are allowed to be on residential property. Planner Martineau said they are typically allowed to be six feet high, and four feet in front yard setbacks. Commissioner Rouse asked why an eight-foot fence is allowed on this property. Richardson said staff would look in the development Code and respond.

Commissioner Phillips asked if a fence would be built at a setback from the creek. Martineau said the fence is proposed to be about five feet north of the developed portion of the property where the parking lot is. The creek is not a riparian corridor or protected at this location; it serves as more of a drainage ditch, and there are no plans to build a fence there at this time.

Commissioner Goodman asked if there were any seismic conditions applied to the police and fire stations; Martineau said those conditions are in the building code and will be addressed during that phase of development.

Commissioner Phillips then asked about building height limits at this location. Martineau said the Code allows building height exceptions for public buildings as long as it is declared in the notice of public hearing beforehand, and in this case, it was.

Commissioner Rouse asked if parking lots are allowed on residentially zoned property, and Martineau said they are allowed when tied to an approved use, accomplished via a replat or a mutual agreement. Rouse asked if the parking lot design meets the commercial or residential landscaping criteria, and Martineau said it meets the stricter section of the code, requiring a 25-foot setback. Rouse said in the past, residential zones were not allowed to have parking lots, so why isn't a zone change proposed for this case? Martineau said a zone change isn't needed since approved uses do allow parking lots; Planning Manager Richardson said the use is permitted in the residential zone as long as a Conditional Use Permit is obtained. The proposed police station use is a single property, single owner, and single use. Rouse asked if the code allows a parking lot in a residential zone with a conditional use permit. Richardson said parking is permitted with the use as long as the Conditional Use application is approved.

Applicant Testimony:

Mario Lattanzio, Police Chief for the City of Albany, 1117 Jackson St. SE. Chief Lattanzio thanked the Commission for their time. He summarized the police station history and current status, noting that it was originally built when the population of Albany was much smaller than today. They have made many changes to the building to make it work for the current environment, but there is an overcrowding issue. The property for the new station was purchased by the City in 2009, and the City has worked to determine the correct size, location, and funding sources for the building to convince the public that it is needed. He provided justification for the proposed site and building size, and addressed questions previously asked by the public, such as access on Willetta Street and potential traffic volumes.

Commissioner Rouse commented that she was on City Council when the current station was built, which was better than the one before, and she hopes the new one will be even better than the current one.

Testimony in Favor:

Helene Becker, 1100 30th Pl. SW. Ms. Becker shared her concerns with safety and traffic issues, access from Pacific Boulevard, and periodic flooding on the proposed site. She was told at a previous meeting that traffic would be monitored and reported back to her as to how it would be dealt with, but she isn't sure that has been done. There are also a lot of walkers in the area; disabled people, middle-schoolers, and the cross-country team. She wonders why an access wasn't considered from the current road in front of the nearby industrial building.

Linn Benton also has a lot of traffic during the school year. She feels no information was given to address the traffic issues.

Regarding the flooding, she is glad there will not be a fence to back up water and cause more flooding. The field where the station will be has been used to hold floodwaters, and she is concerned where that water will go if a building is there.

She is also concerned about building setbacks in the neighborhood and light from the building or vehicles shining into homes. She was wondering if the building could be rotated 90 degrees so the majority of the light would not face homes, but at the industrial plaza instead.

She appreciates the maps and explanation of how far back the building would be built, but she was told at a previous meeting that a siren test is performed by the police, and she would prefer it not to face the neighbors.

Commissioner Rouse asked how high Cathey Creek gets in the winter; Ms. Becker didn't know, but does know it goes over the bank into the field where the building is proposed. She said residences constructed along Willetta were going to be multi-family, but due to flooding and the capacity of the sewer system, the homes were designed to be single-family instead. She said the current system doesn't adequately drain the water. Rouse asked to clarify if she meant storm drain, and Ms. Baker said yes, she meant the storm water drain.

Testimony in Opposition:

Dean Williams, 1008 30th Pl. SW. Mr. Williams is against the Conditional Use Permit application because he doesn't believe it satisfies the review criterion that says the transportation system can support the proposed use. He said the development would cause undue congestion on Umatilla, Willetta, and 28th Avenue. Like Mrs. Becker, he was also under the impression that a traffic study was going to be conducted, but hasn't seen one. It appears the majority of the parking lot is directly accessed from Willetta, causing congestion on that street. He said there are adequate sidewalks in some areas, but not on Umatilla or 28th, which is used heavily by students, bicyclists, and walkers. If a secondary access is truly needed, the one from 24th Street would be a safer route, leaving the neighborhood as it is now.

Neutral Testimony: None

Rebuttal Testimony:

Brian Varricchione, Mackenzie, 1515 SE Water Ave. Portland. Mr. Varricchione would like to assure the public and the Planning Commission that the City of Albany has a very robust permit and plan review process where all the issues raised will be reviewed by staff, such as the storm drainage design and plan. Preliminary stormwater treatment plans have been entered into the record.

Regarding traffic impacts, he noted that the access on Pacific will be the primary access, and on the site plans a safe place to stop and look for pedestrians has been designed. Access on Willetta is secondary access only. He noted that there is more control over use of the site because it will be used primarily by City staff. The applications meet all code requirements.

Rouse asked what the soil types are on the property and why it floods. Mr. Varricchione said he wasn't familiar with the soil types, but project staff have surveyed the site, and his civil engineers have incorporated the results into their design. Rouse asked if the exits near Shoen Electric are too close to the site, and Mr. Varricchione said a certain separation between driveways is required and has been met.

Staff Response:

Ron Irish, Transportation Systems Analyst for the City of Albany. Irish said at the time the neighborhood meeting was held, the number of trips generated by the proposed police station was unknown. The applicant submitted a trip generation analysis with the application, which is referenced in the application. It would generate 31 to 41 peak hour trips. He said if development were allowed under the current office-professional zoning, three to four times more traffic would be generated than for the current proposal. A traffic study was not conducted because the peak hour trip estimated didn't require it. He explained how access to and from Pacific and Willetta would be accomplished. They had looked at access via the industrial park, but there are ODOT and Elm Street right-of-ways there, and wetland mitigation to cross Cathey Creek would be required. Willetta has a better situation for signals, right of ways, parking, and access, and is actually one of the wider residential streets in town.

Regarding lack of sidewalks in the area, those streets without them do need sidewalks, as do many other areas in town, but this level of development doesn't require sidewalks. Ultimately, responsibility for sidewalks in those neighborhoods falls to the property owners there.

Phillips noted that the City did a good job of assessing possible traffic impacts. Rouse asked how traffic would be affected if Stone Forest would be developed across the street (a vacant warehouse for sale across the road). Irish said the study looked at parcels not currently in use and an optimized location for the proposed use. Driveways are proposed according to that analysis. If Stone Forest is redeveloped, those access points may be changed based on future analysis.

Procedural questions: None

Chair Tomlin closed the hearing at 6:19 p.m.

Discussion and Decision:

MOTION: Commissioner Post moved to recommend that the City Council approve the applications as proposed by the applicant. Commissioner Sullivan seconded. A vote was taken and the motion passed 7-0.

QUASI-JUDICIAL PUBLIC HEARING #2

Development of a New Albany Fire Station (VC-03-15, CU-08-15, and RL-11-15):

- 1) Street Vacation for the westernmost 194.12 feet of 6th Avenue, east of Lyon Street;
- 2) Conditional Use Review for a Community Service land use (Fire Station); and,
- 3) Replat to remove property lines and combine six parcels into one

Chair Tomlin opened the public hearing at 6:27 p.m.

Declarations by the Commission:

Conflicts of Interest:	None
Ex Parte Contact:	None
Site Visit:	None
Abstain:	None
Challenges:	None

Staff Report:

Planner Melissa Anderson provided a summary of the case. Staff-proposed Conditions of Approval were included for the Commission's consideration (see Agenda Packet).

Questions from the Commission: None

Applicant Testimony:

John Bradner, Fire Chief for the City of Albany, 110 SE 6th Ave. Mr. Bradner thanked the Commission, and summarized the current status of the Fire Station, which is past the end of its life span and needs to be replaced. There are seismic, space, and other issues present that have been discussed over the past several years. The last large earthquake in 1993 didn't cause extensive damage but did highlight the need to upgrade the building in case another earthquake occurs. A seismic evaluation was conducted in 2002 and presented to the City in 2003, but no funding was identified to address the issues. In 2009, a committee of department members and the public was put together to start looking for new sites for the Fire Station. It was narrowed down to the current site including acquiring some neighboring properties. He summarized the current status of the station and land compared to what is needed, which led to the current proposal and the process of purchasing neighboring properties. Outreach to neighbors is ongoing to mitigate concerns. The Fire Department will continue to be a good neighbor, and maintain compatibility with the downtown core.

Commissioner Rouse asked if soil and groundwater testing would be done on the former Richard's Cleaners site. Bradner said extensive testing has already been conducted and they are working with the DEQ (Department of Environmental Quality).

Rouse asked how future expansion needs would be accommodated. Bradner said there is space built into the design of the new building for extra staff, but as the City grows, additional stations would likely be located at different sites to accommodate response times. Current fire department staff at City Hall will be moved to the new station when it is complete.

Commissioner Sullivan asked how building around Hasty Freeze and traffic would be accommodated. Bradner said access would be separated from the Station, and access from Lyon Street to the station would not be provided.

Testimony in Favor: None

Testimony in Opposition: None

Neutral Testimony: None

Rebuttal Testimony: None

Staff Response: None

Procedural questions: None

Chair Tomlin closed the hearing at 6:55 p.m.

Discussion and Decision:

MOTION: Commissioner Rouse moved to recommend that the City Council approve the applications as proposed by the applicant. Commissioner Goodman seconded. A vote was taken and the motion passed 7-0.

APPROVAL OF MINUTES

No minutes were ready to approve.

ACTIVITY UPDATE

No updates were provided.

NEXT PLANNING COMMISSION MEETING DATE

No future meetings are scheduled at this time.

ADJOURN

Hearing no further business, Chair Tomlin adjourned the meeting at 6:58 p.m.

Respectfully submitted,

Signature on File

Rachel Kutschera
Administrative Assistant

Reviewed by,

Signature on File

Bob Richardson
Planning Manager



Community Development Department

333 Broadalbin Street SW, P.O. Box 490
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STAFF REPORT

ALBANY FIRE STATION

Street Vacation, Conditional Use Review, Site Plan Review and Replat Planning Files VC-03-15, CU-08-15, and RL-11-15

<u>HEARING BODY</u>	PLANNING COMMISSION	CITY COUNCIL
<u>HEARING DATE</u>	Monday, November 16, 2015	Wednesday, January 13, 2015
<u>HEARING TIME</u>	5:15 p.m.	7:15 p.m.
<u>HEARING LOCATION</u>	Council Chambers, Albany City Hall, 333 Broadalbin Street SW	

SUMMARY

The City of Albany is proposing to develop a 24,265 square foot fire station to replace existing Fire Station 11. The new fire station will consist of an apparatus bay, living quarters, offices, community room and classroom space, and associated parking. Land use applications include a 1) Street Vacation for the westernmost 194 feet of 6th Avenue east of Lyon Street; 2) Conditional Use Review for a Community Service land use (Fire Station) in the Hackleman-Monteith (HM) zone; 3) Site Plan Review for a Community Service land use (Fire Station) in the Lyon-Ellsworth (LE) zone; and 4) Replat to remove property lines and combine six parcels into one.

The site is located downtown Albany, where existing public streets and utilities are available to serve the proposed development. The site is within the area bounded by Lyon Street SE on the west, Baker Street SE to the east, 5th Avenue SE to the north, and 7th Avenue SE to the south. The site boundary is separated from Baker Street SE and 5th Avenue SE by intervening developed properties. Sixth Avenue bisects the site but the majority of the street is proposed to be vacated to accommodate the development plan. The length of the street vacation is 194 feet and will stop short of 6th Avenue's intersection with Baker Street.

The site is located within two different zoning districts. The western portion of the site is the mixed-use Lyon-Ellsworth District (LE) zone and the eastern portion of the site is the residential Hackleman-Monteith District (HM) zone. A fire station is categorized as a Community Service use and is a permitted use in the LE zone and a conditional use in the HM zone. The site is adjacent to, but west and of outside the Hackleman Historic District.

There are six existing buildings that will be demolished as a part of this development, including a laundromat, the current Fire Station 11, office buildings, and two outbuildings. The proposed development includes extensive landscaping and a plaza fronting on Lyon Street, and a vegetated buffer along the east site boundary to separate the development from adjoining residences. The building itself is proposed to have a brick façade with a generous amount of windows to be an inviting and attractive addition to the downtown Albany streetscape. The structure will be oriented so that the apparatus bay has front and rear doors to both open directly onto Lyon Street and to allow access to and from the east in the 6th Avenue corridor.

Because the request includes a street vacation, conditional use review, and replat, these land use applications are processed concurrently with public hearings at both the Planning Commission and City Council in accordance with ADC 1.200(3). The Planning Commission's recommendation on the proposal will be presented to the City Council, who is the final decision-maker on this request.

Staff recommends the Planning Commission make a recommendation to the City Council to approve the subject land use applications with the conditions outlined in this report.

GENERAL INFORMATION

DATE OF REPORT: November 6, 2015

FILE: VC-03-15, CU-08-15, and RL-11-5

TYPE OF APPLICATIONS: Development of a New Albany Fire Station:
1) Street Vacation for the westernmost 194.12 feet of 6th Avenue, east of Lyon Street;
2) Replat to remove property lines and combine six parcels into one; and,
3) Conditional Use Review for a Community Service land use (Fire Station) in the HM zone
4) Site Plan Review for a Community Service land use (Fire Station) in the LE zone

REVIEW BODIES: Planning Commission and City Council (Type IV) – Concurrent Street Vacation, Conditional Use, and Replat Application

PROPERTY OWNER: City of Albany, 333 Broadalbin St. SE, Albany, OR 97321

APPLICANT: Fire Chief John Bradner, on behalf of the City of Albany; 333 Broadalbin St. SE, Albany, OR 97321

APPLICANT'S REPRESENTATIVE: Jeff Humphries, c/o Mackenzie; 1515 SE Water Avenue, Suite 100, Portland, Oregon 97214

AGENT: Brian Varricchione, Mackenzie, 1515 SE Water Avenue, Suite 100, Portland, Oregon 97214

ADDRESS/LOCATION: 500, 517, 519, 525, 527, 531 & 623 Lyon Street SW;
110, 129, 131 & 133 6th Avenue SW; and,
123 & 177 7th Avenue SW

MAP/TAX LOT: Linn County Assessor's Map No. 11S-03W-07BA; Tax Lots 1000, 1001, 1600, 2000, 2100 & 2701

ZONING: LE (Lyon-Ellsworth) and HM (Hackleman-Monteith)

COMPREHENSIVE PLAN DESIGNATION: Village Center and Low Density Residential

EXISTING LAND USE: Fire Station 11, Office Uses and a Laundromat

SURROUNDING ZONING: North: LE (Lyon-Ellsworth)
South: LE
East: HM (Hackleman-Monteith)
West: LE

SURROUNDING USES: North: Multifamily and Single Family residential
South: Fast Food Drive-thru Restaurant and Single Family residential
East: Multifamily and Single Family residential
West: Democrat-Herald and Mark Thomas GMC Buick

PRIOR HISTORY: SP-26-92: Site plan review for a change of use from a residential use to an office use with joint-use parking, located at 125 6th Ave. SE.
SP-35-92: Site plan review to expand a paved parking area with concurrent variance request to reduce the required buffer/screening area to five feet where 10 feet is required, located at 121 and 125 6th Ave SE.
RL-02-15: Replat to combine three parcels into two parcels, located at 623 & 655 Lyon St. SE and 123 7th Ave. SE.

NOTICE INFORMATION

The applicant coordinated with Planning Division staff to arrange for and publicize a neighborhood meeting, which was held on August 12, 2015. The notice and materials followed the procedures of ADC 1.204. A summary of the meeting is included in Attachment C.59 - C.63.

A Notice of Public Hearing was published in the Democrat Herald newspaper on November 2 and 9 of 2015.

A Notice of Public Hearing was mailed on November 2, 2015 to all property owners located within 1,000 feet of the subject properties, as well as to all interested parties who attended the neighborhood meeting. A notice of public hearing was also posted on the subject site at five locations on November 2, 2015. Lastly, the staff report for the proposal was posted on the City's website and available for public review on November 9, 2015. At the time this staff report was completed, no comments had been received.

APPEALS

Within five days of the City Council's final action on these applications, the Community Development Director will provide written notice of the decisions to the applicant and any other parties entitled to notice. A City Council decision can be appealed to the Oregon Land Use Board of Appeals (LUBA) if a person with standing files a Notice of Intent to Appeal within 21 days of the date the decision is reduced to writing and bears the necessary signatures of the decision makers.

STAFF ANALYSIS

The Albany Development Code (ADC) includes the following review criteria for street vacations, conditional uses and replats, which must be met for this application to be approved. Code criteria are written in *bold italics* and are followed by findings and conclusions.

VACATIONS REVIEW CRITERIA (ADC 2.630)

Criterion (1) The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan city transportation or public facility plan.

FINDINGS OF FACT

- 1.1 The proposed vacation includes the western 194.12 feet of the 6th Avenue SE right of way between Blocks 6 and 7 of the Eastern Addition to the Albany Subdivision, lying immediately east of the Lyon Street right of way (see Attachment C.1 – C.6). The street is classified as a local street.
- 1.2 The properties that the vacated area will revert to are the properties immediately north and south of the right-of-way. The northern half will go to the northern properties and the southern half will go to the southern properties. All of the properties are owned by the City of Albany and are to be combined, along the vacated right-of-way, into a single property through the concurrent replat application.
- 1.3 Albany's Transportation System Plan does not identify 6th Avenue as being a part of the city's arterial/collector street system, nor does it identify it as a critical local street connection.
- 1.4 The following Comprehensive Plan policies have been identified as relevant to this review criterion:

GOAL 11: Public Facilities and Services

- a. Prohibit the construction of structures over public water lines and easements.
 - b. Prohibit the construction of structures over drainage improvements and easements.
 - c. Prohibit the construction of structures over public wastewater lines and easements.
- 1.5 Sanitary Sewer. Sanitary sewer utility maps indicate that an 8-inch public sanitary sewer main lies within the easternmost 140 feet of the portion of the 6th Avenue right-of-way proposed for vacation.
 - 1.6 Water. Water utility maps indicate that a 6-inch public water main lies along the entire length of the portion of the 6th Avenue right-of-way proposed for vacation.

- 1.7 Storm Drainage. Storm drainage utility maps indicate that no public storm drainage piping lies within the portion of the 6th Avenue right-of-way proposed for vacation. This portion of 6th Avenue is improved with curb and gutter to collect and convey stormwater runoff from the street and adjacent properties.
- 1.8 The proposed vacation is being processed concurrently with the proposed construction of a new fire station on property adjacent to both sides of the public street right-of-way. The proposal shows the new fire station building being constructed directly over the area where existing public sanitary sewer and water mains lie within the 6th Avenue right-of-way. The fire station construction proposal includes the abandonment of the portions of these utilities that lie within the area to be vacated.

CONCLUSIONS

- 1.1 City utilities (sanitary sewer and water) exist within the area proposed for vacation.
- 1.2 The applicant is proposing to abandon public sanitary sewer and water facilities that lie within the area to be vacated prior to beginning construction on the proposed fire station. Abandonment must be done in a manner approved by the City's Public Works Department, and appropriate modifications must be made at the point of transition between public and private piping.
- 1.3 No public utility easements are needed over the public utilities on City-owned property.
- 1.4 The proposed vacation does not conflict with Albany's Transportation System Plan.
- 1.5 This criterion is met without conditions.

Criterion (2) The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.

FINDINGS OF FACT

- 2.1 Sixth Avenue is located in an area with a grid street system. The proposed vacation would require drivers to use an alternate route. The nearest alternate routes are 7th Avenue and 5th Avenue. Seventh Avenue is located 220 feet to the south, and 5th Avenue is located 220 feet to the north. Both alternate connections are close enough to avoid negative impacts on access between public rights of way.
- 2.2 The portion of 6th Avenue proposed to be vacated currently provides direct driveway access to four parcels. All of those parcels are being incorporated into one lot as part of the replat being processed with this application for the new fire station site. The vacation will not eliminate street access to any other existing properties.
- 2.3 Vacation of 6th Avenue would not land lock or remove access to any existing parcels.
- 2.4 Existing public utilities currently exist within the areas to be vacated. The existing public right-of-way currently provides access to these utilities.
- 2.5 The proposal for the construction of the new fire station indicates that the public utilities (sewer and water) that will lie in the area of the proposed development will be abandoned.

CONCLUSIONS

- 2.1 The proposed street vacation will not negatively impact access between public right-of-way, and will not eliminate street access to any parcels.
- 2.2 Access will be available to these utilities through City-owned property until the fire station is constructed; and the utilities must be abandoned prior to construction. Therefore, there will be no negative impact to access to existing public utilities.
- 2.3 This criterion is met without conditions.

Criterion (3) The requested vacation will not have a negative effect on traffic circulation or emergency service protection.

FINDINGS OF FACT

- 3.1 As discussed under ADC 2.440(2), above, the vacation will not have a negative impact on access between rights-of-way or remove access to any existing parcels.

- 3.2 The vacation will have a positive effect on emergency service protection by providing the fire station with a more direct connection to Ellsworth street, and allowing the station to be designed for pull through movements from Baker Street into the emergency vehicle bays. Emergency vehicles returning to the station are currently required to block traffic on Lyon Street to back into emergency vehicle bays.

CONCLUSION

- 3.1 The requested vacation will not have a negative effect on traffic circulation and will improve emergency vehicle service protection.
- 3.2 This criterion is met without conditions.

Criterion (4) *The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.*

FINDINGS OF FACT

- 4.1 The vacated right of way will be converted into a portion of the new fire station, which will be required to meet the development code standards.

CONCLUSION

- 4.1 The vacated right of way will be brought into development code compliance with development of the site.
- 4.2 This criterion is met without conditions.

Criterion (5) *The public interest, present and future, will be best served by approval of the proposed vacation.*

FINDINGS OF FACT

- 5.1 The vacated right of way will provide land necessary for the construction of a proposed fire station.
- 5.2 It is in the public interest to have a new fire station because existing Fire Station 11 is inadequate to serve the needs of the Albany Fire Department.

CONCLUSION

- 5.1 Approving the right of way vacation will allow the new fire station to be completed, which will better serve the public interest.

STREET VACATION CONCLUSION

The proposed street vacation meets all applicable review criteria without conditions.

CONDITIONAL USE REVIEW CRITERIA (ADC 2.250)

Criterion (1) *The proposed use is consistent with the intended character of the base zone and the operating characteristics of the neighborhood.*

FINDINGS OF FACT

- 1.1 The proposal is to develop a 24,265 square foot new fire station to replace existing Fire Station 11. The proposed new fire station aims to improve the appearance, effectiveness, and reach of the existing use. The new station is a public safety service and will protect and enable healthy growth of the surrounding community.
- 1.2 The site is located partially within two different zoning districts. The western portion of the site is the mixed-use Lyons-Ellsworth District (LE) zone and the eastern portion of the site is the residential Hackleman-Monteith District (HM) zone. See Attachment C.66 Zoning Map and C.72 Site Plan (sheet C2.1), for an illustration of the zoning district boundaries in relation to the development site. The site is adjacent to but west of and outside the Hackleman Historic District.
- 1.3 *“The LE district is intended primarily as a location for development that serves the Historic Downtown district and Downtown Central Business district. This district is the most desirable location in the Central Albany area for parking structures with ground-floor commercial uses (ADC 5.030(5)).”* The western portion of the

proposed site, as shown in Attachment C.66 Zoning Map and C.72 Site Plan (sheet C2.1), is within the LE zoning district and is subject to its development standards.

- 1.4 *"The HM district is intended primarily to preserve the existing single-family residential character of the Hackleman and Monteith National Register Historic Districts. Conversion of single-family residential structures to other uses, including multi-family residential, is not allowed (ADC 3.020(7))."* The eastern portion of the proposed site, as shown in Attachment C.66 Zoning Map and C.72 Site Plan (sheet C2.1), is within the HM zoning district and is subject to its development standards. There are no existing single-family residences in the HM zone proposed for conversion to other uses as part of this proposal.
- 1.5 The proposed fire station is listed as a Community Service land use category per ADC 22.190. Community service land uses are allowed in the LE zone through site plan review and through conditional use review in the HM zone. Site plan review criteria under ADC 2.450 address the adequacy of public utilities, transportation systems, access, and parking, applicability of special purpose overlay districts, the design and operating characteristics of the proposed development and the compatibility with surrounding development and land uses. Conditional use criteria under ADC 2.250 also address the adequacy of public utilities, transportation systems, access, and parking, applicability of special purpose overlay districts, compatibility between the existing and anticipated uses, and consistency between the proposed use and the intended character of the base zone and the operating characteristics of the neighborhood. Conditional use criteria also take into account impacts on the livability of nearby residentially zoned land. Although the conditional use criteria could be considered more stringent, the site plan review criteria are addressed separately later in this report.
- 1.6 The site is located in downtown Albany, where existing public streets and utilities are available to serve the proposed development. The site is within the area bounded by Lyon Street SE on the west, Baker Street SE to the east, 5th Avenue SE to the north and 7th Avenue SE to the south. The site boundary is separated from Baker Street SE and 5th Avenue SE by intervening developed properties. Sixth Avenue bisects the site but the majority of the street is proposed to be vacated to accommodate the development plan.
- 1.7 There are six existing buildings within the development area that will be demolished as a part of this development, including a laundromat, the current Fire Station 11, office buildings, and two outbuildings.
- 1.8 Nearby land uses include a mix of commercial, office, and residential uses. The *Hasty Freez* drive-thru restaurant is southwest of the site, the Democrat-Herald newspaper office, and Mark Thomas GMC Buick auto sales to the west across Lyon Street, and multi-family and single-family residences are to the north, east, and south. A vegetated buffer is proposed along the east site boundary to separate the development from adjoining residences.
- 1.9 The new fire station will consist of an apparatus bay, living quarters, offices, community room and classroom space and associated parking. The proposed development includes extensive landscaping and a plaza fronting on Lyon Street and a vegetated buffer along the east site boundary to separate the development from adjoining residences.

CONCLUSIONS

- 1.1 There is currently a fire station on the site (assuming approval of the concurrent replat application) and the proposed development will improve the appearance, effectiveness, and reach of the existing use.
- 1.2 The proposed fire station is categorized as a Community Service use, which is allowed in the LE zone and the HM zone through the conditional use review process. Through this land use review, the conditional use process and review criteria are addressed.
- 1.3 The proposed fire station is located on an arterial street, surrounded primarily by commercial uses, and the use has been operating in this approximate location for a number of years.
- 1.4 Therefore, the proposed new fire station is consistent with the intended character of the base zone and the operating characteristics of the neighborhood. This criterion is met without conditions.

Criterion (2) *The proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, setbacks, and landscaping or the proposal mitigates difference in appearance or scale through such means as setbacks, screening, landscaping or other design features.*

FINDINGS OF FACT

2.1 **Existing and Anticipated Uses:** The site is located downtown Albany, where existing public streets and utilities are available to serve the proposed development. The site consists of six lots totaling an area of 1.63 acres. There are six existing buildings within the development area that will be demolished as a part of this development, including a laundromat, the current Fire Station 11, office buildings, and two outbuildings.

Nearby land uses include a mix of commercial, office and residential uses. The *Hasty Freez* drive-thru restaurant is southwest of the site, the Democrat-Herald newspaper office and Mark Thomas GMC Buick auto sales to the west across Lyon Street, and multi-family and single-family residences are to the north, east, and south. A vegetated buffer is proposed along the east site boundary to separate the development from adjoining residences.

The new fire station will consist of an apparatus bay, living quarters, offices, and community room and classroom space. There are two dedicated parking lots for this fire station, both of which are on the side or rear of the building. The proposed development includes extensive landscaping and a plaza fronting on Lyon Street and a vegetated buffer along the east site boundary to separate the development from adjoining residences.

The building orientation will maximize exposure to Lyon Street SE to maintain and enhance a safe and inviting section of the street for pedestrian and motor vehicles. As illustrated on Attachments C.76 and C.90, Sheets C2.1 and A3.1, the main public entrance is clearly defined by a canopy with entry plaza, multiple windows, glass overhead doorways, and a decorative cornice on the parapet. The plaza extends out to the public sidewalk on Lyon Street. There will be two rows of trees lining the south side of the building, and trees lining the parking area and islands.

The building itself is proposed to have a brick façade with a generous amount of windows and a decorative cornice on the parapet, and is intended to be an inviting and attractive addition to the downtown Albany streetscape. The structure will be oriented so that the apparatus bay has front and rear doors to both open directly onto Lyon Street and to allow access to and from the east in the 6th Avenue corridor.

While the site itself will be larger than the majority of other developments in the area, the building and site features will meet applicable development standards and be developed to a human scale that is responsive to its surroundings, with the structure located on Lyon Street and parking and landscaping to the rear away from existing residences. The site will integrate well into this transition area between mixed-use and residential zones. The site will be landscaped and situated in a manner that will mitigate the size of the site.

2.2 **Setbacks, Intensity, and Lot Coverage in the HM zone:** As illustrated on Attachments C.76 and C.90, Sheets C2.1 and A3.1, and noted in the table below, the proposed fire station complies with all applicable development standards for the portion within the HM zone.

ADC TABLE 3-1: DEVELOPMENT STANDARDS HACKLEMAN-MONTEITH (HM) ZONING DISTRICT (EXCERPTS FOR HM ZONE AS APPLICABLE TO NON-RESIDENTIAL PROJECTS)		
Standard	Allowance	Proposed
Lot Depth, minimum	65'	76' (portion within HM zone)
Minimum Front Setback ¹	15'	N/A (front in LE zone on Lyon St.)
Maximum Front Setback	None	N/A (front in LE zone on Lyon St.)
Minimum Interior Setback ¹	6'	41'
Minimum Building Separation	N/A	N/A
Maximum Height	30'	30'
Maximum Lot Coverage	60%	58% (portion within HM zone)
Minimum Open Space	N/A	N/A
Minimum Landscaped Area	All yards adjacent to streets	All yards adjacent to streets

(1) Additional setbacks may be required, see Sections 3.230-3.330 and the buffer matrix at 9.210.

- 2.3 Setbacks, Intensity and Lot Coverage in the LE zone: As illustrated on Attachments C.76 and C.90, Sheets C2.1 and A3.1, and noted in the table below, the proposed fire station complies with all applicable development standards for the portion within the LE zone

ADC TABLE 5-2: DEVELOPMENT STANDARDS LYONS-ELLESWORTH (LE) ZONING DISTRICT		
Standard	Allowance	Proposal
Minimum Lot Size	2,000 SF	46,520 SF (portion within LE zone)
Maximum Building Footprint	None	15,400 SF (portion within LE zone)
Lot Width, minimum	20'	450'
Lot Depth, minimum	50'	67'
Landscaped Area	All yards adjacent to streets	All yards adjacent to streets
Minimum Open Space	None	25.6% (portion within LE zone)
Maximum Front Setbacks	None	10'
Minimum Front Setback ¹	0'	10'
Minimum Interior Setback ¹	0'	74'
Minimum Setback to Garage Entrance	10'	26'
Height, maximum	60'	30'
Lot Coverage, maximum	100%	74.4% (portion within LE zone)

(1) Additional setbacks may be required, see Sections 3.230-3.330 and the buffer matrix at 9.210.

- 2.4 Special Setbacks: ADC 3.330 and 5.210, "Special Setbacks for Education and Religious Institutions, Public and Semi-Public Buildings," requires any new construction of an educational institution, religious institution, or public or semi-public building to be set back at least 25 feet from any property line adjoining or directly across a public right-of-way from any residential district. No required front or interior yard of the lot on which such building or use is located shall be used for stockpiling or storage of materials or equipment.

As illustrated on Attachment C.76, Sheet C2.1, the proposed building would be located upwards of 40 feet from any property line within the HM zone (on the rear of the site). No materials or equipment will be stockpiled or stored in the front or interior yard of the lot.

- 2.5 Vehicle Parking – Required Number: The parking requirements under ADC Table 9-1 do not identify the minimum number of spaces required for fire stations and no similar uses are listed in the table. Therefore, the Community Development Director may approve alternative parking standards. Using information from the existing Fire Station 11 (company data on parking demand) to determine the required parking standard. The existing facility is approximately 14,500 square feet and has 16 on-site parking spaces, or approximately one space per 900 square feet. However, the existing facility provides inadequate parking, so the applicant proposes a parking standard requiring one space per 600 square feet (an increase of 50% over the existing amount).

Based on the proposed building size of 24,265 square feet, this would equate to a minimum of 41 spaces. As illustrated on Attachment C.76, Sheet C2.1, the proposed site plan provides 28 parking spaces for public/visitor use and 19 secure spaces for fire department staff, totaling 47 on-site vehicle parking spaces, which would exceed the minimum number required.

Alternatively, if compared to the parking space requirements for an office use, the amount of parking proposed for the fire station will still exceed the minimum required per code. Office uses require one parking space for every 400 square feet of net floor area per ADC 9.020, Table 9-1. Based on Attachment C.73, Sheet T1.2, the useable square footage of the proposed fire station is 15,886 square feet. This square footage total omits the apparatus bay and the mezzanine, which will be used for storage because these areas are not usable areas that would create the need for parking spaces. The parking spaces are provided to serve the employees and the public who will use the facility. Those areas include the office area (7,669 s.f.), living quarters (6,603 s.f.),

fitness room (546 s.f.) and the community room (1,068 s.f.), which totals 15,886 square feet. Using the parking ratio for office use, a total of 40 parking spaces would be required. The total number of parking spaces proposed for the fire station is 45 spaces, which exceeds the minimum that would be required for an office use by five spaces.

Another comparison can be made using the trip generation estimate report (Attachment C.64 - C.65), which concludes that redevelopment of the existing facility and the neighboring properties that will encompass the new fire station to generate a net decrease of approximately 310 daily trips and a total of 33 PM peak hour trips. Assuming everyone drove to the site, the 47 on-site parking spaces would be able to accommodate the 33 peak hour trips.

Furthermore, the site is located within the LE zone where on-street parking may count toward the minimum parking requirement (per ADC 9.025)As illustrated on Attachment C.76, Sheet C2.1, the proposed on-street spaces on Lyon Street could accommodate nine additional vehicle parking spaces (each 25 feet in length). All vehicle parking spaces are standard size; no compact vehicle parking spaces are proposed.

- 2.6 Parking Lot Design and Construction. Parking lots must be paved, landscaped, and provided with approved drainage. ADC 9.120(3) states that all areas of a parking lot shall have a durable, dust-free surface of asphalt, cement concrete, or other materials approved by the Director of Public Works. ADC 9.130, Table 1, includes dimensional standards for parking lots. The ADC requires a stall depth of 18.5 feet for spaces that are 9 feet wide and angled at 90 degrees. A 3-foot parking overhang with wheel bumpers or 7.5-foot sidewalk must be provided at the front of each space fronting a sidewalk, and can be subtracted to determine actual stall depth. Dead-end back up areas shown on the site plan are at least 5 feet deep.

As illustrated on Attachment C.76, Sheet C2.1, the proposed parking lots will not have any compact spaces and accessible spaces will exceed the minimum dimensions specified in the Oregon Structural Specialty Code. All parking spaces in the public parking lot are proposed to have a width of nine feet, while those within the secure parking area have a width of 10 feet. All aisle widths are a minimum of 24 feet, and a 20-foot storage area is provided for the driveway to 7th Avenue. All parking spaces have a minimum depth of 18.5 feet, with the exception of the secure parking area, in which some spaces have a depth of 17.5 feet due to the provided bumper overhang. The spaces with a width of 10 feet have a 24-foot aisle while those with narrower spaces have a 26-foot aisle. A dead-end backup of 5 feet is provided in the secure parking area.

The proposed parking lot and aisles are proposed to be concrete or asphalt in accordance code requirements. The parking lot has been designed to convey stormwater to vegetated stormwater planters prior to discharge to the municipal storm drain system. The parking lot is encircled by perimeter curbing, with the exception of areas discharging to stormwater planters and those parking areas are integrated with the plaza area. The parking spaces abutting the sidewalk leading to 7th Avenue are designed with secured wheel bumpers. The parking lot design allows vehicles to back up and reverse direction on-site without requiring backing movements within a public street. Parking spaces are separated by a 4-inch-wide stripe. ADA accessible spaces are appropriately denoted with painted and signed designations

- 2.7 Loading Standards. ADC 4.260 requires 500 square feet of loading area for non-residential buildings over 20,000 gross square feet. Deliveries can be accommodated via the paved area near the apparatus bay's east entrance, which is 2,800 square feet in size, which meets this standard.

- 2.8 Bicycle Parking. ADC 9.120(13)(c) states that commercial or office development must provide at least two bicycle parking spaces plus one space per 10 automobile spaces required. At least one-half of required bicycle parking spaces must be sheltered and the dimensional details of ADC 9.120(e) through (h) must be met.

Based on the required 41 parking spaces, a total of seven bicycle spaces are required. As illustrated on Attachments C.76 and C.82, Sheets C2.1 and L2.2, parking for 12 bicycles has been provided south of the building near the plaza. The racks are proposed to be located so that the canopy provides weather protection to four of the racks (or eight bike parking spaces). A condition of approval will require a detail of the bicycle parking to show that dimensional requirements listed in ADC 9.120(e) through (h) can be met.

- 2.9 Outside Storage. ADC Section 3.390 states that any refuse container or refuse disposal area which would otherwise be visible from a public street, customer or resident parking area, any public facility, or any

residential area, shall be screened from view by placement of a sight-obscuring fence, wall or hedge at least 6 feet in height. All refuse materials shall be contained within the screened area. No refuse container or refuse disposal area shall be placed within 15 feet of a dwelling window.

As shown in Attachment C.72, Sheet C2.1 and drawings (Attachment C.68 and C.69), a fenced trash enclosure will be located on site, northeast of the building. The enclosure will be screened from neighboring properties by a 6-foot-tall chain link fence (with slats), landscape buffering, and screening as illustrated on Attachment C.84, Sheet L4.1. The enclosure will be located over 13 feet from the property line and more than 15 feet from dwelling windows.

2.10 Buffering and Screening. ADC Section 9.210 requires buffering and screening in order to reduce the impacts on adjacent uses which are of a different type, buffering and screening is required in accordance with a matrix contained within the section. According to ADC 9.240, the minimum improvements within a buffer area consist of the following:

- (1) *At least one row of trees. These trees will be not less than 10 feet high at time of planting for deciduous trees and spaced not more than 30 feet apart and 5 feet high at time of planting for evergreen trees and spaced not more than 15 feet apart. This requirement may be waived by the Director when it can be demonstrated that such trees would conflict with other purposes of this Code (e.g. solar access).*
- (2) *At least five, 5-gallon shrubs or ten 1-gallon shrubs for each 1,000 square feet of required buffer area.*
- (3) *The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, evergreen shrubs).*

According to ADC 9.250, where screening is required or provided, the following standards apply in addition to conditions (1) and (3) above:

- (1) *One row of evergreen shrubs that will grow to form a continuous hedge at least 4 feet tall within two years of planting, or*
- (2) *A fence or masonry wall at least 5 feet tall constructed to provide a uniform sight-obscuring screen, or*
- (3) *An earth berm combined with evergreen plantings or a fence that forms a sight and noise buffer at least 6 feet tall within two years of installation.*

Albany Development Code Table 9-4 (Buffer and Screening Matrix) dictates that a 10-foot buffer with screening is required when abutting dwellings. This is applicable on the eastern and northern portions of the site that abut houses in the HM zone. A 10-foot buffer (but no screening) is required on Lyon Street since it is an arterial street (unless it conflicts with other sections of the code). No other buffering or screening is required in the LE zone.

As illustrated in Attachments C.84 and C.85, Sheets L4.1 and L4.2, landscaping meeting the buffering requirements is provided along the eastern site boundary adjacent to residences within the HM zone. The landscaping consists of a row of trees closer than 30 feet apart, shrubs, and groundcover (some of which will be within stormwater facilities). Similarly, trees, shrubs, and groundcover are proposed along Lyon Street. A chain-link fence with slats is proposed to meet the screening requirement along the eastern site boundary adjacent to residences within the HM zone.

The building and parking lots are setback at least 10 from Lyon Street. As illustrated in Attachments C.84 and C.85, Sheets L4.1 and L4.2, landscaping meeting the buffering requirements is provided along the western site boundary adjacent to Lyon Street, except where the main entrance and driveways are located.

2.11 Landscaping. ADC 9.140 requires *all required front and interior setbacks, exclusive of accessways and other permitted intrusions, to be landscaped before an occupancy permit will be issued. Minimum landscaping acceptable for every 1,000 square feet of required setbacks in all commercial industrial districts is as follows:*

- (a) *One tree at least 6 feet tall for every 30 feet of street frontage.*
- (b) *Five 5-gallon or eight 1-gallon shrubs, trees or accent plants.*
- (c) *The remaining area treated with suitable living ground cover, lawn, or decorative treatment of bark, rock, or other attractive ground cover.*

Attachment C.84 and C.85, Sheets L4.1 and L4.2 depict the proposed landscaping, which consists of street trees and on-site trees abutting the Lyon Street and 7th Avenue frontages, together with multiple shrubs and ground cover plantings. However, the LE zone has no required front or interior setbacks, so this standard does not apply.

2.12 Parking Lot Landscaping. ADC 9.150 requires the following:

- (1) Planter Bays. *Parking areas shall be divided into bays of not more than 12 parking spaces. Between or at the end of each parking bay there shall be curbed planters at least 5 feet wide. Each planter shall contain one tree at least 10 feet high and decorative groundcover containing at least two shrubs for every 100 square feet of landscape area.*
- (2) Entryway Landscaping. *Entryways into parking lots shall be bordered by a minimum 5-foot wide landscape planter strip meeting the same landscaping provisions as for planter bays, except that no sight-obscuring trees or shrubs are permitted.*
- (3) Parking Space Buffers. *Parking areas shall be separated from the exterior wall of a structure by pedestrian entranceways or loading areas or by a 5-foot strip of landscaping materials.*

As illustrated on Attachment C.84 and C.85 (Sheets L4.1 and L4.2), no more than 12 contiguous parking spaces comprise each bay. The proposed parking lot planters have widths of at least seven feet (excluding curbs), exceeding the five-foot minimum standard. Each planter bay would have one canopy tree and at least six shrubs, which exceeds the requirement for two shrubs per 100 square feet. Landscape planters are proposed next to the parking lot entrances with plantings that provide canopy trees and multiple low-growing shrubs, well in excess of the requirement for two shrubs per 100 square feet.

As illustrated in Attachment C.76 (Sheet C2.1), the parking areas are separated from the building by at least five feet, with the exception of the north wall, which has a 4.5-foot separation. However, at this location, the curb stops are placed within the parking stalls to provide sufficient space for bumper overhang that vehicles do not interfere with the building. A condition of approval will require the applicant to provide the Planning Division with a final landscape plan prior to issuance of a building permit.

2.13 Landscaping and Vision Clearance Areas. No trees, shrubs, fences, or signs may be located within any vision clearance area, which prohibits structures, or planting that would impede visibility between the heights of two feet and eight feet. A clear vision area consists of a triangular area, two sides of which are lot lines or a driveway, and the third side of which is a line across the corner of the lot joining the non-intersecting ends of the two sides (per ADC 12.180).

For this use, the measurement along each driveway edge is 20 feet. It is unclear from the applicant's landscaping plan whether landscaping will impede visibility at these intersections. A condition of approval will require the applicant to provide the Planning Division with a detail showing the vision clearance area and the plantings contained therein prior to issuance of a building permit.

2.14 Irrigation. ADC 9.160 requires that all required landscape areas be provided with a piped underground irrigation system, unless a licensed landscape architect or certified nurseryman submits written verification that the proposed plant materials do not require irrigation. Irrigation systems installed in the public right-of-way require an encroachment permit.

Attachment C.83 (Sheet L3.1) illustrates the irrigation requirements for all of the landscaped areas. The landscaped area and the stormwater treatment areas will be irrigated using rotary spray nozzles. Final design of the irrigation system is proposed to be performed at the time of permitting. A condition of approval will require the applicant to provide the Planning Division with a final landscape and irrigation plan prior to issuance of a building permit.

2.15 ADC 9.207, Tree Protection, applies to proposals "*felling of 5 or more trees larger than 25 inches in circumference (approximately 8 inches in diameter) on a lot or property in contiguous single ownership in excess of 20,000 square feet in any zone.*"

As illustrated on Attachment C.75 (Sheet C2.0), the proposed development would result in the removal of ten trees in excess of 8-inch diameter on four lots within the site area to accommodate the proposed fire station,

parking areas, stormwater management facilities, and required landscaping. Three trees will be removed from a 10,803 square foot lot (tax lot 1000); two trees will be removed from a 5,505 square foot lot (tax lot 1001); one tree will be removed from a 8,360 square foot lot (tax lot 2100); and four trees will be removed from a 13,550 square foot lot (tax lot 2701). Each of these lots is less than 20,000 square feet in size and less than five trees in excess of 8-inch diameter are proposed to be removed from any one lot. Therefore, this criterion is not applicable.

- 2.16 Design Standards. See review of Design Standards for Commercial and Institutional Uses following this Conditional Use section in the staff report below.

CONCLUSIONS

- 2.1 While the site itself will be larger than the majority of other developments in the area to accommodate the proposed building, there is no maximum lot size limit in the LE and HM zones. Additionally, the building and site features will be developed to a human scale that is responsive to its surroundings, with the structure located on Lyon Street, and parking and landscaping to the rear away from existing residences.
- 2.2 The site will integrate well into this transition area between mixed-use and residential zones. All minimum development standards have been met for setbacks, height and lot coverage.
- 2.3 The site will be landscaped and building situated in a manner that will mitigate its' relatively large size. All minimum landscaping and buffering standards have been met.
- 2.4 Because the parking standards do not specify a ratio for a fire station, the Community Development Director may approve alternative parking standards. A total of 47 vehicle parking spaces are proposed to serve the staff and the public on-site and nine additional spaces are available on Lyon Street in front of the station. This amount has been determined by City staff to provide more than the minimum amount necessary on -site to serve Fire Department staff and the public. The minimum parking lot design and construction standards have been met.
- 2.5 A total of 12 bicycle parking spaces will be provided near the main entrance and plaza and more than half of those will be covered.
- 2.6 The outside refuge container will be screened from public view and located more than 15 feet away from dwelling windows.
- 2.7 The tree felling criteria do not apply to this proposal.
- 2.8 Given the above, and with the following conditions of approval, the proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, and setbacks. Differences in appearance or scale of this relatively large building and site are sufficiently mitigated through such means as setbacks, screening, landscaping, and building design. As such, the proposal is consistent with criterion two of ADC 2.250, as conditioned.

CONDITIONS

- 2.1 Prior to issuance of a building permit, the applicant shall submit for review and approval by the Community Development Department, a detail of the bicycle parking to show that the design and dimensional requirements listed in ADC 9.120(e) through (h) can be met.
- 2.2 Prior to issuance of a building permit, the applicant shall submit for review and approval to the Community Development Department, a final landscape and irrigation plan, along with a detail showing the vision clearance area and the plantings contained therein.
- 2.3 Prior to issuance of a certificate of occupancy, the building and all site improvements shall be constructed in accordance with approved plans.

Criterion (3) *The transportation system can support the proposed use in addition to the existing uses in the area. Evaluation factors include street capacity and level of service, on-street parking impacts, access requirements, neighborhood impacts and pedestrian safety.*

FINDINGS OF FACT

- 3.1 The proposed development will demolish several existing structures and construct a 28,838 square foot Fire Station. The project is located on the east side of Lyon Street between 5th Avenue and 6th Avenue.
- 3.2 Access to the development will be provided by Lyon Avenue, 6th Avenue, and 7th Avenue. The site's driveway connection to Lyon Street will be limited to exit movements by emergency vehicles.
- 3.3 Sixth Avenue and 7th Avenue are classified as local roads and are constructed to city standards. Improvements include curb, gutter, and sidewalk; a vehicle travel lane in each direction; and on street parking.
- 3.4 Lyon Street is classified as a principal arterial road, is under the jurisdiction of the Oregon Department of Transportation (ODOT), and is constructed to city standards. The road is part of a one-way couplet with Ellsworth Street. Improvements include curb, gutter, and sidewalk; two vehicle travel lanes in each direction; and on street parking.
- 3.5 The proposed development would result in the following changes to the street system:
 - a) The vacation and closing of 6th Avenue from Lyon Street east approximately 195 feet east. This vacation is necessary to create a building site large enough to accommodate the construction of the new fire station. The vacation will also allow the new station to be designed so that the emergency vehicle exit bays are aligned with 6th Avenue to the west. That design will provide the station with improved access to destinations located to the south and west of the station.
 - b) Elimination of two existing driveway connections to Lyon Street. The existing fire station driveway to Lyon Street and a driveway north of 6th Avenue serving a private parking lot are shown on the site plan as being removed.
 - c) The construction of street, striping, and new sign improvements on Lyon Street. A variety of improvements will be needed for the construction and function of the new fire station. These improvements will include curb extensions, storm drainage system modifications, a recessed on street parking bay, crosswalk and stop bar striping, and flashing emergency vehicle warning signs. Because Lyon Street is part of the highway system, all of these changes will need to be reviewed and approved by ODOT. ODOT has reviewed preliminary concept plans for the planned changes and indicated that they appear to be feasible (see Attachment B). All of the changes are shown on the site plan (Attachment C.76, Sheet C2.1).
 - d) Elimination of an existing driveway to 7th Avenue, and construction of a new approach. An existing driveway to a single family home will need to be removed and replaced with a new approach on 7th Avenue located to provide access to the public parking lot for the fire station.
 - e) Modifications to 6th Avenue from Baker Street to the east boundary of the fire station. These changes are needed to allow this section of 6th Avenue to function as an emergency vehicle access to and from the rear of the new station. The design of the station will allow for "pull through" movements by emergency vehicles and eliminate the current requirement for returning vehicles to block traffic on Lyon Street in order to back into station parking bays.
- 3.6 The design and location of proposed site driveways comply with the standards contained in ADC 12.100.
- 3.7 A trip generation study was submitted with the application. The study was performed by Mackenzie and is dated September 9, 2015 (see Attachment C.64 – C.65). The study estimated that the new Fire Station would generate 87 vehicle trips per day, of which 9 would occur during the peak PM traffic hour.
- 3.8 Previous uses on the site included a 14,500 square foot fire station, an accountant office, an investment office, and a dry cleaner. Those uses combined were estimated to generate a total of 407 vehicle trips per day, of which 42 occurred during the peak PM traffic hour.
- 3.9 Based on the trip generation study, the new fire station will result in a net decrease in both daily and peak PM traffic volumes when compared to the previous uses on the site.

- 3.10 Because the street system could accommodate the previous uses on the site and traffic volumes for the proposed use are less, the system can also accommodate the proposed development.

CONCLUSIONS

- 3.1 The public streets that adjoin the development are currently constructed to city standards.
- 3.2 The development will result in a reduction in vehicle trips to the public street system when compared to the previous uses on the site.
- 3.3 The development will eliminate two existing driveway approaches to Lyon Street and the east leg of the Lyon/6th intersection. New curb, gutter, and sidewalk will be installed on the east side of Lyon Street from 7th Avenue north to 5th Avenue. Improvements will include the construction of a new driveway approach for the fire station at 6th Avenue; curb extensions; a recessed parking bay, storm drainage improvements; installation of a crosswalk and related pavement striping, and traffic control signs. Because Lyon Street is part of the state highway system, all of those changes will need to be approved and permitted by ODOT.
- 3.4 The development will remove an existing driveway to 7th Avenue and replace it with a new driveway that will provide access to the fire station's public parking lot.
- 3.5 The development will modify improvements to 6th Avenue from Baker Street west to the boundary of the fire station site to allow that portion of 6th Avenue to function as an emergency vehicle access to and from the rear of the new station.
- 3.6 The design and location of proposed site driveways comply with the standards contained in ADC 12.100.
- 3.7 The public street system can accommodate the proposed development.
- 3.8 This criterion can be met with the following conditions:

CONDITIONS

- 3.1 The applicant shall obtain the approval and all necessary permits from ODOT prior to performing any work within Lyon Street right of way.
- 3.2 Prior to issuance of a certificate of occupancy, the applicant shall construct street, driveway, and drainage improvements on Lyon Street as shown on the site plan. The design of the improvements shall be approved by both ODOT and the city engineer.
- 3.3 Prior to issuance of a certificate of occupancy, the applicant shall remove the site's existing driveway to 7th Avenue and replace it with a new driveway and sidewalk improvements as shown on the site plan.
- 3.4 Prior to issuance of a certificate of occupancy, the applicant shall make the modifications to 6th Avenue between Baker Street and the site's east boundary as shown on the site plan.

Criterion (4) Public services for water, sanitary and storm sewer, water management, and for fire and police protection, can serve the proposed use.

FINDINGS OF FACT

Sanitary Sewer:

- 4.1 City utility maps show 8-inch public sanitary sewer mains in Lyon Street, Baker Street, 6th Avenue, and 7th Avenue. A portion of the public main in 6th Avenue will be abandoned as part of this project in conjunction with the associated street vacation.
- 4.2 ADC 12.470 requires all new development to extend and/or connect to the public sanitary sewer system if the property is within 300 feet of a public sewer line.
- 4.3 The proposed new building will be served by a sewer service lateral that will be extended into the site from the public main in 6th Avenue.

Water:

- 4.4 City utility maps show a 4-inch public water main in Lyon Street, a 12-inch main in Baker Street, a 6-inch main in 6th Avenue, and a 12-inch main in 7th Avenue. A portion of the public main in 6th Avenue will be abandoned as part of this project in conjunction with the associated street vacation.
- 4.5 ADC 12.410 requires all new development to extend and/or connect to the public water system if the property is within 150 feet of an adequate public main.
- 4.6 The proposed new building will be served by a water service that will be connected to the public main in Lyon Street.

Storm Drainage:

- 4.7 City utility maps show 27-inch public storm drainage main in Lyon Street, and a 24-inch main in Baker Street. The facilities within the Lyon Street right-of-way are under the jurisdiction of ODOT.
- 4.8 The applicant is required to submit a drainage plan, including support calculations, as defined in the City's Engineering Standards. The applicant is responsible for making provisions to control and/or convey storm drainage runoff originating from, and/or draining to, any proposed development in accordance with all City standards and policies as described in the City's Engineering Standards. In most circumstances, detention will be required unless it can be satisfactorily demonstrated by the applicant that there is no adverse impact.
- 4.9 The applicant's preliminary storm drainage plan shows connections to both the storm drainage main in Lyon Street and the main in Baker Street.
- 4.10 The applicant is proposing to provide the required on-site stormwater quality facilities by constructing a number of stormwater quality ponds. A Stormwater Quality permit must be obtained by the applicant before this work is begun.
- 4.11 The City's Engineering staff has reviewed the applicant's preliminary stormwater and stormwater quality plans and has determined that they are generally acceptable.

Fire Services:

- 4.12 According to the Albany Fire Marshal's Office, approved fire apparatus roadways must extend to within 150 feet of all exterior portions of the structure as measured by an approved route of travel around the exterior of the structure (OFC 503.1.1). Civil site plan submittals for all future building and planning permit approvals shall show fire protection details as required on the "Fire Site Plan Submittal Checklist" located on the City of Albany's web site.
- 4.13 Fire hydrants for this proposed project will be based on the following requirements:
 - a. Fire hydrant location: All portions of buildings constructed or moved into the City shall be located within 400 feet (600 feet for fire sprinkler-protected buildings) of a fire hydrant located on a fire apparatus access road using an approved route of travel. (OFC 508.5.1)
 - b. Required hydrants based on the required fire flow as calculated in accordance with OFC 503.7 and OFC Appendix B. The minimum number of fire hydrants is determined by OFC Table C105.1.
 - c. Required fire hydrant spacing will be based upon required fire flows as determined by OFC Appendix C105.1 and Table C105.1. Please note that dead end roads require a reduced spacing.
 - d. Fire hydrant spacing along new/required fire apparatus access roads. OFC Section C103.1 requires the placement of additional hydrants along all required fire access roads that are adjacent to any proposed building (and any future additions) and circulating through the property with spacing requirements per Appendix C 105.1. (See 2009 ICC Commentary, Appendix C-1, Section C103.1)

CONCLUSIONS

- 4.1 The proposed stormwater quality facilities can accommodate the proposed development.
- 4.2 The existing public utilities in this area will be able to accommodate the proposed development.

- 4.3 While the City's Public Works Department has reviewed the applicant's preliminary utility plans, the final design details will be reviewed as part of the Permit for Private Construction of Public Utilities.
- 4.4 This criterion can be met with the following conditions:

CONDITIONS

- 4.1 Prior to issuance of building permits for the proposed project, the applicant shall abandon the public sewer and water mains that lie within the 6th Avenue right-of-way that will be vacated in association with this project.
- 4.2 Prior to issuance of building permits for either of the proposed developments, the applicant shall obtain a Stormwater Quality permit from the City's Engineering Department, and the permit must be approved.
- 4.3 Prior to issuance of occupancy permits, the applicant must construct the required stormwater quality facilities, generally as shown on the applicant's preliminary utility plan.
- 4.4 Prior to issuance of building permits, plans showing the location and placement of fire access routes and hydrants (as needed) serving the development shall be reviewed and approved by the City of Albany Fire Marshal. Adequate water supply must be available on site prior to construction, or as otherwise approved by the Fire Marshal.

Note: Alternatively, the applicant may provide financial assurances for the required public infrastructure in order to obtain building permits for the project in Accordance with ADC 12.590 and 12.600.

Criterion (5) *The proposal will not have significant adverse impacts on the livability of nearby residentially zoned lands due to: (a) Noise, glare, odor, litter, or hours of operation, or (b) Privacy and safety issues.*

FINDINGS OF FACT

- 5.1 The Development Code classifies fire stations as a Community Service use type, and the proposed fire station will replace an existing fire station already on-site. As such, the new building itself will not result in any significant changes in off-site impacts. Noise, dust, and vibration impacts will be similar to those from the existing fire station, primarily originating from emergency vehicle trips to and from the facility, plus infrequent use of the emergency generator during power failures and occasional test cycles.
- 5.2 Lighting. As depicted on Attachment C.91 (Sheet E1.1PH), lighting glare is minimized through the use of shielded light fixtures as illustrated on the photometrics plan. All overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors. Light pollution will be mitigated by utilizing appropriate shielding on light fixtures and planting trees along the borders of the parking lots.
- 5.3 The facility is not anticipated to generate odors other than those from the trash enclosure, which is located over 13 feet from the property line and more than 15 feet from dwelling windows as required by ADC 3.390. The trash receptacle will be screened from neighboring properties by landscaping (see Attachment C.84, Sheet L4.1) and a chain-link fence with slats. The landscape plan (Attachment C.84 and C.85, Sheets L4.1 and L4.2) includes a variety of parking lot trees and shrubs lining the exterior of the parking lot and site with the intention of segregating the neighbors from any noise, glare, odors, and dust associated with the fire station.
- Noise levels are expected to be similar to current levels. Noise primarily originates from emergency vehicle trips to and from the facility, plus occasional use of the emergency generator. The emergency generator has been sited north of the building as far from residences as possible to minimize noise impacts on neighboring properties. The generator will only operate during periods of power failure and during periodic test cycles (required to occur at least monthly for 30 minutes).

CONCLUSIONS

- 5.1 As explained above and described in the application, the proposal would not create significant adverse impacts on residentially zoned property related to noise, glare, odor, litter, hours of operation, and would not create any privacy or safety issues. The improvements proposed for the replacement fire station are expected to increase the livability of neighboring uses because of enhanced screening and landscaping, use of appropriate shielding

on lighting fixtures, and because the apparatus bay would be farther away from adjoining residences than the existing fire station. As such, the proposal complies with criterion 5, above.

Criterion (6) Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.

FINDINGS OF FACT

- 6.1 *Article 4 Airport Approach district:* Figure 4-1 of ADC Article 4 shows that the subject property is not located in the Airport Approach district.
- 6.2 *Article 6 Steep Slopes, Comprehensive Plan Plate 7:* shows that there are no steep slopes on the subject property.
- 6.3 *Article 6 Floodplains, Comprehensive Plan Plate 5:* does not show these properties in a 100-year floodplain. FEMA/FIRM Community Panel No. 41043C0213G, dated September 2010, shows that this property is in Zone X, an area determined to be outside any 500-year floodplain.
- 6.4 *Article 6 Wetlands, Comprehensive Plan Plate 6:* does not show wetlands on the properties.
- 6.5 *Article 7 Historic Districts, Comprehensive Plan Plate 9:* shows the subject properties are located immediately west of the Hackleman Historic District, but not within a designated historic district.

CONCLUSIONS

- 6.1 There are no other known special features of this site to consider with this conditional use application.
- 6.2 This criterion is met without conditions.

CONIDITIONAL USE REVIEW CONCLUSION

The proposed development meets or can meet with conditions all applicable review criteria.

COMMERCIAL AND INSTITUTIONAL DESIGN STANDARDS (ADC 8.320-8.390)

In addition to the review criteria above, the following Design Standards must be met. **Note:** If there is a checked box symbol (☑) preceding a standard, it means that staff has compared the applicant's findings and plans to the standard(s) and find the standard(s) is met without comment. If the box is unchecked (☐), staff has provided findings and conclusions as to the reason(s) why the standard is not met, and has added a condition. "NA" preceding the standard means it is not applicable to this particular development

N/A **Relationship to Historic Overlay Districts (ADC 8.320).** *Applicable only for residential property inside the Monteith or Hackleman Historic Overlay Districts, see Article 7 for additional historic review criteria.*

FINDINGS OF FACT: The development site is adjacent to but west of and outside the Hackleman Historic District. Therefore, this criterion does not apply.

☑ **Building Orientation (ADC 8.330).**

- (1) *New commercial buildings shall be oriented to existing or new public streets. Building orientation is demonstrated by placing buildings and their public entrances close to streets so that pedestrians have a direct and convenient route from the street sidewalk to building entrances.*
 - (a) *On sites smaller than 3 acres, commercial buildings shall be oriented to the public street/sidewalk and off-street parking shall be located to the side or rear of the building(s), except where it is not feasible due to limited or no street frontage or where there are access restrictions. Buildings on larger sites may be setback from the public street and oriented to traffic aisles on private property, if the on-site circulation system is developed like a public street with pedestrian access, landscape strips and street trees.*
- (2) *At least one major public entrance shall be visible from the abutting public street. Corner entrances may be used to provide orientation to two streets. Customer entrances should be clearly defined, highly visible, using features such as canopies, porticos, arcades, arches, wing walls, and planters.*

FINDINGS OF FACT: This site area is 1.63 acres. The primary frontage of the building will be on Lyon Street SE. The proposed orientation will maximize exposure to Lyon Street SE to maintain and enhance a safe and inviting section of the street for pedestrian and motor vehicles. As illustrated on Attachment C.76 and C.90, (Sheets C2.1 and A3.1), the main public entrance is clearly defined by a canopy, multiple windows, and an entry plaza.

The proposed orientation will provide emergency vehicle access to Lyon Street SE directly from the apparatus bay within the fire station. Furthermore, the front of the public portion of the building has an entrance approximately ten feet behind the front property line. Landscaping between the right-of-way and the building will be established on both sides of the driveway and will extend the width of the property.

There are two dedicated parking lots for this fire station, both of which are on the side or rear of the building, not between the front of the building and the sidewalk. The parking area on the south side of the building is composed of 28 standard stalls that will be available for the public. The parking lot on the north and east side of the building includes 19 standard parking stalls that are dedicated as official use areas by the fire station, including employee parking. In addition, there will be on street parking available on Lyon Street SE on both sides of the apparatus bay driveway. These standards are met.

General Building Design (ADC 8.340). *New commercial buildings shall provide architectural relief and interest, with emphasis at building entrances and along sidewalks, to promote and enhance a comfortable pedestrian scale and orientation. Blank walls shall be avoided, except when not feasible.*

- (1) *Ground floor windows shall be provided along frontages adjacent to sidewalks. The main front elevation(s) of buildings shall provide windows or transparency at the pedestrian level in the following minimum proportions: Lyon-Ellsworth (LE) District—50 percent transparency. The minimum window and door requirements are measured between 2 and 8 feet from the ground. Only the glass portion of doors may be used in the calculation.*
- (2) *Walls that are visible from a public street shall include a combination of architectural elements and features such as offsets, windows, entry treatments, wood siding, brick stucco, synthetic stucco, textured concrete block, textured concrete, and landscaping.*

FINDINGS OF FACT: As illustrated on Attachment C.90 (Sheet A3.1) and C.69 – C.70, blank walls are minimized on façades, which are publicly visible. The building elevations make use of a brick façade with fiber cement accent panels, a cornice, substantial windows, glass rollup doors in the apparatus bay, and sunshades and canopies over selected windows and doors.

In the Lyon-Ellsworth (LE) zone, fifty percent of the Lyon Street elevation is required to be transparent between a height of 2 and 8 feet above ground. The building frontage on Lyon Street has a length of 172 feet; therefore, a minimum of 516 square feet of windows are required in this height range. As illustrated on Attachment C.87 and C.89, Sheets A2.1 and A3.1, the proposed building design provides 617 square feet or 60% transparency from the glass overhead doors, storefront window system, and vestibule. The upper floor windows mirror the style and locations of those on the first floor to the extent possible. These standards are met.

Street Connectivity and Internal Circulation (ADC 8.350). *The standards apply to both public and private streets.*

- (1) *New commercial buildings may be required to provide street or driveway stubs and reciprocal access easements to promote efficient circulation between uses and properties, and to promote connectivity and dispersal of traffic.*
- (2) *The internal vehicle circulation system of a commercial development shall be a continuation of the adjacent public street pattern wherever possible and promote street connectivity. The vehicle circulation system shall mimic a traditional local street network and break the development into numerous smaller blocks.*
- (3) *Traffic lanes shall be internal to the site and shall not be located between the building(s) and the sidewalk(s), except as provided in (4) below.*

- (4) *Where drop-off facilities are provided, they shall be designed to meet the requirements of the Americans with Disabilities Act (ADA), but still provide for direct pedestrian circulation.*
- (5) *Internal roadways shall be designed to slow traffic speeds. This can be achieved by keeping road widths to a minimum, allowing parallel parking, and planting street trees to visually narrow the road.*

FINDINGS OF FACT: The proposal was evaluated for opportunities to interconnect the site with the adjoining *Hasty Freez* drive-thru restaurant property to the southwest in order to minimize the number of driveways on the abutting streets. However, due to the orientation and circulation pattern of the existing *Hasty Freez* business it was not possible to connect the sites. The applicant's design team coordinated with Albany Engineering and ODOT staff and determined to restrict parking area access to Baker Street and 7th Avenue rather than connecting the parking lots to Lyon Street.

The apparatus bay driveways on Lyon Street and Baker Street are midway between 5th Avenue and 7th Avenue, thereby mimicking the location of 6th Avenue, which is proposed to be vacated to accommodate the proposed facility.

No drop-off facilities are proposed for this development and no traffic aisles are proposed to be located between the building and the sidewalk. The apparatus bay driveway is perpendicular rather than parallel to Lyon Street and will function more similarly to an intersecting street than to a drive aisle.

Traffic will likely be generally slow due to the relatively small parking lots. Internal circulation within parking areas is designed to discourage aggressive driving and ensure a safe environment for pedestrian and motor vehicle operators. The use of landscaping and islands will create a pedestrian-focused environment and an aesthetically pleasing and environmentally friendly design. These criteria are met.

Pedestrian Amenities (ADC 8.360).

- (1) *All new commercial structures and improvements to existing sites shall provide pedestrian amenities. The number of pedestrian amenities shall comply with the following sliding scale.*

<u>Size of Structure or Improvement</u>	<u>Number of Amenities</u>
<i>Less than 5,000 square feet</i>	<i>1</i>
<i>5,000 – 10,000 square feet</i>	<i>2</i>
<i>10,001 – 50,000 square feet</i>	<i>3</i>

- (2) *Acceptable pedestrian amenities include the following improvements. No more than two of any item may be used to fulfill the requirement:*

- (a) *Sidewalks at least ten feet wide with ornamental treatments (e.g., brick pavers), or sidewalks which are 50 percent wider than required by the Code.*
- (b) *Benches and public outdoors seating for at least four people.*
- (c) *Sidewalk planter(s) enclosing a total of eight square feet.*
- (d) *Pocket parks or decorative gardens (minimum usable area of 300 square feet).*
- (e) *Plazas (minimum usable area of 300 square feet).*
- (f) *Street trees that are 50 percent larger than required by the Code.*
- (g) *Weather protection (awnings, etc.).*
- (h) *Other pedestrian amenities that are not listed but are similar in scale and benefit.*

- (3) *Pedestrian amenities shall comply with the following standards:*

- (a) *Amenities shall be located outside the building main entrance, along pedestrian corridors, or near transit stops. Amenities shall be visible and accessible to the general public from an improved public or private street. Access to pocket parks, plazas, and sidewalks must be provided via a public right-of-way or a public access easement.*

- (b) Amenities are not subject to setback requirements.
- (c) Amenities are consistent with the character and scale of surrounding developments. For example, similarity in awning height, bench style, planter materials, street trees, and pavers is recommended to foster continuity in the design of pedestrian areas. Materials should be suitable for outdoor use, easily maintained, and have a reasonably long life cycle (e.g., 10 years before replacement).

FINDINGS OF FACT: The proposed building size of 24,265 square feet requires three pedestrian amenities. As illustrated in Attachment C.76 (Sheet C2.1), proposed amenities include a plaza with seating walls, weather protection over the main entryway, footbridges over the stormwater facility, sidewalk planters, and street trees. The plaza, weather protection, and footbridges are all near the main building entrance. A consistent theme has been utilized for the amenities to ensure harmony with the building design. This standard is met.

- N/A (4) On sites at major transit stops provide the following:
- (a) Either locate buildings within 20 feet of the transit stop, a transit street or an intersecting street or provide a pedestrian plaza at the transit stop or a street intersection.
 - (b) A reasonable direct pedestrian connection between the transit stop and building entrances on the site.
 - (c) A transit passenger landing pad accessible to the disabled.
 - (d) An easement or dedication for a passenger shelter if requested by the transit provider.
 - (e) Lighting at the transit stop.

FINDINGS OF FACT: The site is not located at a major transit stop. The fire station site is 0.2 miles from the Albany Transit Station, which is located at 112 10th Avenue SW; however, the transit station can be reached via the abutting public sidewalk on Lyon Street. This is not applicable.

Pedestrian Connections (ADC 8.370)

- (1) New retail, office and institutional buildings at or near existing or planned transit stops shall provide for convenient pedestrian access to transit.
- (2) Walkways shall be provided connecting building entrances and streets adjoining the site.
- (3) Pedestrian connections to adjoining properties shall be provided except where such a connection is impractical. Pedestrian connections shall connect the on-site circulation system to existing or proposed streets, walkways, and driveways that abut the property. Where adjacent properties are undeveloped or have potential of redevelopment, streets, access ways and walkways on site shall be laid out or stubbed to allow for extension the adjoining property.

FINDINGS OF FACT: Although the proposed fire station site is 0.2 miles from the transit stop at the Albany Transit Station (112 10th Avenue SW), it can be reached via the abutting public sidewalk on Lyon Street. An accessible walkway is proposed between the main building entrance and the public sidewalk, and sidewalks will also connect to 7th Avenue and Baker Street.

The applicant investigated opportunities to interconnect the site with the adjoining *Hasty Freez* drive-thru restaurant property in order to minimize the number of driveways on the abutting streets. However, due to the orientation and circulation pattern of the existing *Hasty Freez* business it was not possible to connect the sites. Due to the location, orientation, and usage of existing developed neighboring properties it was deemed impractical to directly connect the on-site pedestrian paths with neighboring properties, particularly those in residential use. The site plan (Attachment C.76, Sheet C2.1) illustrates how the internal pedestrian circulation plan, and the site in general, will connect to external circulation systems. This standard is met.

Compatibility Details (ADC 8.390). Commercial development shall be designed to comply with the following applicable details and any other details warranted by the local conditions:

- (1) On-site lighting is arranged so that light is reflected away from adjoining properties and/or streets.

- (2) *Any undesirable impacts produced on the site, such as noise, glare, odors, dust or vibrations have been adequately screened from adjacent properties.*
- (3) *The site is protected from any undesirable impacts that are generated on abutting properties.*
- (4) *Unightly exterior improvements and items such as trash receptacles, exterior vents and mechanical devices have been adequately screened.*
- (5) *Storage areas, trash collection facilities and noise generating equipment are located away from public streets, abutting residential districts or development, or sight obscuring fencing has been provided.*
- (6) *Where needed, loading facilities are provided on-site and are of sufficient size and number to adequately handle the delivery or shipping of goods or people. Where possible, loading areas should be designed so that vehicles enter and exit the site in a forward motion.*

FINDINGS OF FACT: As depicted on Attachment C.91 (Sheet E1.1PH), lighting glare is minimized through the use of shielded light fixtures as illustrated on the photometrics plan. All overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors. Light pollution will be mitigated by utilizing appropriate shielding on light fixtures and planting trees along the borders of the parking lots.

Noise, dust, and vibration impacts will be similar to those from the existing fire station, primarily originating from emergency vehicle trips to and from the facility, plus infrequent use of the emergency generator. The facility is not anticipated to generate odors other than those from the trash enclosure, which is located over 13 feet from the property line and more than 15 feet from dwelling windows. The landscape plan (Attachment C.84 and C.85, Sheets L4.1 and L4.2) includes a variety of parking lot trees and shrubs lining the exterior of the parking lot and site with the intention of segregating the neighbors from the any noise, glare, odors, and dust associated with the fire station.

Based on the abutting commercial and residential uses, the likelihood of undesirable impacts from neighboring properties is limited. Nonetheless, the site has adequate landscaping and paved space between the building and neighboring properties to prevent undesirable effects of adjacent uses.

The trash collection facility is located near the northeast corner of the building and is fully screened from view by a fenced enclosure and landscaping. As shown in the site plan (Attachment C.76, Sheet C2.1), the trash receptacle will be screened from neighboring properties by landscaping (see Attachment C.84, Sheet L4.1) and a chain-link fence with slats.

The emergency generator has been sited north of the building as far from residences as possible to minimize noise impacts on neighboring properties. The generator will only operate during periods of power failure and during periodic test cycles (required to occur at least monthly for 30 minutes).

Building mechanical equipment will be located on the roof behind the parapets to decrease visibility from Lyon Street, and the electrical transformer and emergency generator are located within the fenced and screened yard north of the building.

The proposed fire station does not necessitate the use of a designated loading facility, as any deliveries can be accommodated via the paved area near the apparatus bay's east entrance. A portion of the apparatus bay itself has been designed to allow fire trucks to pull through from east to west. These criteria are met.

Supplemental Commercial Design Standards in Village Centers, ADC 8.405 – 8.475

8.415 Buildings Along Public Streets.

Standards.

- (1) *Buildings and plazas shall be located within the maximum setback area for at least 40 percent of one public street frontage.*
- (2) *For sites with frontage on more than one public street (i.e., corner lots), this standard applies to one frontage only.*
- (3) *The public street frontage is the length of the property as measured along the street right-of-way excluding*

the width of entrance driveways and/or streets.

- (4) *Building facades that face public streets shall be divided into human-scale proportions using at least two features such as windows, entrances, arcades, arbors, awnings, trellises with vines, or an equivalent element. A blank, uninterrupted wall shall not be longer than 30 feet.*
- (5) *No parking, loading or travel aisles shall be located between the public street and buildings within 50 feet of the street, except that a designated park-and-ride lot or one drive-through lane may be permitted. See Section 8.420(b).*
- (6) *To count toward this standard, a plaza shall:*
 - (a) *Be well defined at the street edge by a low decorative architectural wall (no higher than three feet), or a line of shrubs or trees of the same species, or similar landscaped or built feature;*
 - (b) *Use materials that are similar in quality to the principal materials of the primary building(s) and landscape. Landscaping with drought-resistant native species is strongly encouraged;*
 - (c) *Have direct access to the public street sidewalk and be located the shortest distance to the nearest building main entrance; and*
 - (d) *Extend at least the full depth of the maximum setback.*

FINDINGS OF FACT: Although the LE zone has no maximum front setback, the proposed building encompasses 45% of the Lyon Street frontage. To ensure a pedestrian-friendly development, the primary entrance and office portion of the building is located approximately 10 feet from the front property line. The remainder of the building is approximately 26 feet from the front property line. The office portion of the building is located as close as practical to the front property line, while the apparatus bay is set back to accommodate emergency vehicles entering and existing. The public street frontage along Lyon Street is 375 feet. The building is 170 feet in length, which encompasses 45% of the Lyon Street frontage.

The proposed plan includes a plaza that extends out to the public sidewalk on Lyon Street. The plaza is located on the south side of the building between the fire station and the parking lot. The plaza will be visually separated from the street by an enclosure of trees geometrically clustered in the southwest corner of the site, two rows of trees lining the south side of the building, and trees lining the parking area and islands.

No parking, loading, or travel aisles are proposed between Lyon Street and the building. The design of the fire station has been selected to complement other structures in Albany's historic downtown. As shown on Attachments C.87 and C.90 (Sheets A2.1 and A3.1), the building design incorporates significant windows, multiple glass overhead doors, and doorways. The uninterrupted wall at the north end of the Lyon Street elevation is approximately 26 feet long, which is less than the 30-foot maximum. These criteria are met.

N/A 8.420 Maximum Setback.

Standards.

- (1) *In zoning districts with maximum setbacks, buildings within 50 feet of a public street shall have 40% of the building located within the maximum setback except that:*
 - (a) *If a previously recorded easement precludes meeting the maximum setback, the applicant shall demonstrate that an alternative layout best addresses the intent of this standard and the character of the village center.*
 - (b) *A building with drive-through service may have one drive-through lane between the building and the street provided that the building is set back no more than 25 feet and the drive-through lane is screened according to standards for perimeter parking area landscaping in ADC 8.470.*
- (2) *Any building more than 50 feet from a public street is exempt from this standard.*

FINDINGS OF FACT: The LE zone has no maximum front setback; therefore, this criterion is not applicable.

N/A 8.430 Size Limitations. See building size limitations in Articles 4 and 5, Table 1, Development Standards.

FINDINGS OF FACT: There is no maximum building size for the LE or HM zoning district; therefore, this criterion is not applicable.

8.440 Building Design.

Standards.

- (1) Building facades longer than 100 feet shall have relief such as recessed entries, offsets, jogs, bays, columns, ribs, pilasters, piers, cornices, bases, or other distinctive constructed changes. Changes in relief in the building façade shall occur at least every 100 feet for at least 20% of the exterior wall area. At least two colors or textures shall be used (not including stripes or bands).
- (2) Rooflines longer than 100 feet shall be relieved by elements such as parapets, gables, dormers, towers, steeples, etc.
- (3) No building wall shall be longer than 300 feet unless the building façade has one or more major offsets in wall plane. A major offset in wall plane shall have a depth of at least 10% of the length of the longest abutting wall and shall continue for at least 20% of the building façade. Minor changes in wall plane such as entries, jogs, bays, columns, ribs, pilasters, piers, or cornices do not count toward meeting this standard.
- (4) In developments with multiple buildings, each individual building shall include predominant characteristics shared by all buildings in the development, so that the development forms a cohesive place within the district. A standardized prototype design shall be modified if necessary to meet the provisions of this Code and character of this district.
- (5) Corrugated metal siding is prohibited on any building. Corrugated metal roofing is allowed.

FINDINGS OF FACT: As shown on Attachment C.87 and C.90 (Sheets A2.1 and A3.1), the building has an overall length of 172 feet and incorporates significant windows, multiple glass overhead doors, doorways, and a decorative cornice on the parapet. The vestibule and lobby project out toward the Lyon Street sidewalk while the remainder of the front façade is offset in two different planes. The overhead doors serve to break up the significant massing of the 110-foot portion of the façade north of the lobby. The east face of the building is comprised of three materials including brick, glass, and fiber cement accent panels. This criterion is met.

8.445 Pedestrian Network.

Standards.

- (1) Sidewalks must be located to provide the shortest direct connection from the public street sidewalk(s) to all customer entrances.
- (2) Sidewalks must be located to provide the shortest direct connection between all on-site customer entrances.
- (3) Sidewalks must be located along every public street frontage and both sides of on-site private streets. These sidewalks must be separated from the street by a tree-lined landscape strip.
- (4) Extra-wide sidewalks are encouraged to provide space for tables and chairs and other pedestrian amenities, creating a concentration of activity to serve as the neighborhood center.
- (5) Sites larger than eight acres shall create an open space or plaza with amenities such as benches, monuments, kiosks or public art. Amenities shall be in prominent locations, interconnected with the uses and walkways on the site, and be landscaped.

FINDINGS OF FACT: The primary entrance on the southwest corner of the building will be only ten feet from the front property line. This is the shortest and most direct location for connection to the sidewalk within the right-of-way. Other public entrances are located on the south side of the building within the plaza area, adjacent to the public parking lot. Both entrances are surrounded by hardscape plaza spaces that connect directly to public sidewalks.

Public sidewalks are located on Lyon Street and 7th Avenue. Street trees will be planted in accordance with applicable City standards. The sidewalk along Lyon Street is 10 feet wide, which allows for significant pedestrian volume. Although the site area is less than eight acres (1.63 acres), the proposed plaza will serve as a publicly accessible focal point for the neighborhood. These criteria are met.

8.450 Privacy Considerations.

Standard.

(1) *Non-residential uses and parking areas shall be arranged to minimize infringement on the privacy of adjoining residents.*

FINDINGS OF FACT: As depicted on Attachment C.76 (Sheet C2.1), the building and parking areas are located as far away from abutting residences north and east as practical, with a 10-foot wide or greater vegetated buffer. The landscaping plan on Attachment C.84 and C.85 (Sheets L4.1 and L4.2), illustrates the proposed plantings intended to maximize privacy of nearby properties. This criterion is met.

8.460 Parking Areas.

Standards.

(1) *On-street parking spaces within 100 feet of a commercial or office development may count towards meeting the parking requirement.*

(2) *Shared parking is encouraged for all uses.*

(3) *Trees intended for parking area landscaping shall provide a canopy cover of at least 20% of the parking area at maturity. Existing trees may be included to meet the canopy requirement, provided the site plan identifies such trees and the trees meet the standards of size, health, and placement. The extent of canopy at maturity shall be based on published reference texts generally accepted by landscape architects, nurserymen, and arborists.*

(4) *Bioswales shall be considered as the initial stormwater collection system.*

FINDINGS OF FACT: There are several on-street parking spaces proposed along Lyon Street for use by site visitors, in addition to the 47 on-site spaces. The parking lot trees illustrated on Attachment C.84 and C.85 (Sheets L4.1 and L4.2), have been selected to provide a mature canopy of 23% of the north parking area and 25% of the south parking area. The parking facility is designed to provide an inviting experience for both pedestrians and drivers alike. The public parking area has integrated trees, a plaza, landscaped islands, and other landscaping features to mitigate the effects of an open paved lot. Multiple vegetated planters will provide stormwater quality for parking lot runoff (see Attachment C.77, Sheet C2.2). These criteria are met.

8.470 Perimeter Parking Area Landscaping.

Standards.

1) *All parking areas (excluding entranceways) adjacent to a public street shall be screened with:*

(a) *A low continuous hedge of evergreen shrubs, trees and plantings that are at least 3 feet tall within 2 years and grow to provide an evergreen screen of at least 70%; OR*

(b) *A berm 3 feet tall with a maximum slope of 3:1, in combination with coniferous and deciduous trees and shrubs; OR*

(c) *A low decorative masonry wall at least 3 feet tall in combination with landscaping; OR*

(d) *A combination of any of these methods.*

(2) *The landscape plan shall be prepared by a licensed landscape architect.*

FINDINGS OF FACT: The site has been laid out to ensure that no parking areas are adjacent to a public street; all parking lots are more than 20 feet from the right-of-way line. Attachment C.84 and C.85 (Sheets L4.1 and L4.2) depicts the parking lot perimeter landscaping. The parking areas are surrounded by evergreen shrubs with larger parking lot trees just to the interior. These criteria are met.

N/A 8.475 Signs.

Standards.

For integrated centers, an overall sign and graphics program shall be provided as part of the development application to ensure that stand-alone signs are consolidated and that signs complement the character of the neighborhood. Monument signs are preferred rather than freestanding signs.

FINDINGS OF FACT: The proposal is not an integrated center; therefore, this criterion does not apply.

DESIGN STANDARDS CONCLUSION

The proposal satisfies the applicable design review criteria without conditions.

SITE PLAN REVIEW CRITERIA (ADC 2.450)

Criterion (1) Public utilities can accommodate the proposed development.

FINDINGS OF FACT

1.1 The findings of fact for ADC 2.250 conditional use review criterion number four above are incorporated here by reference. In summary, these findings conclude that public utilities are available to the site and are adequate to serve the proposed development if conditions of approval are met.

CONCLUSION

1.1 Public utilities can accommodate the proposed development if conditions of approval are met.

1.2 This criterion is met with the conditions listed under conditional use review criterion number four above.

Criterion (2) The proposed post-construction stormwater quality facilities (private and/or public) can accommodate the proposed development, consistent with Title 12 of the Albany Municipal Code.

FINDINGS OF FACT

2.1 The findings of fact for ADC 2.250 conditional use review criterion number four above are incorporated here by reference. In summary, these findings conclude that stormwater quality facilities are adequate to serve the proposed development if conditions of approval are met.

CONCLUSION

2.1 Post-construction storm water quality facilities can accommodate the proposed development if conditions of approval are met.

2.2 This criterion is met with the conditions listed under conditional use review criterion number four above.

Criterion (3) The transportation system can safely and adequately accommodate the proposed development.

FINDINGS OF FACT

3.1 The findings of fact for ADC 2.250 conditional use review criterion number three above are incorporated here by reference. In summary, these findings conclude that the transportation system can accommodate the proposed development if conditions of approval are met.

CONCLUSION

3.1 The transportation system can safely and adequately accommodate the proposed development if conditions of approval are met.

3.2 This criterion is met with the conditions listed under conditional use review criterion number three above.

Criterion (4) *Parking areas and entrance-exit points are designed to facilitate traffic and pedestrian safety and avoid congestion.*

FINDINGS OF FACT

4.1 The findings of fact for ADC 2.250 conditional use review criterion number three above are incorporated here by reference. In summary, these findings conclude that the access and parking areas can accommodate the proposed development if conditions of approval are met.

CONCLUSION

4.1 The parking areas and entrance-exit points are designed to facilitate traffic and pedestrian safety and avoid congestion if conditions of approval are met.

4.2 This criterion is met with the conditions listed under conditional use review criterion number three above.

Criterion (5) *The design and operating characteristics of the proposed development are reasonably compatible with surrounding development and land uses, and any negative impacts have been sufficiently minimized.*

FINDINGS OF FACT

5.1 The findings of fact for ADC 2.250 conditional use review criterion number two above are incorporated here by reference. In summary, these findings conclude that the proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, and setbacks. Differences in appearance or scale of this relatively large building and site are sufficiently mitigated through such means as setbacks, screening, landscaping, and building design. As such, the proposal is consistent with criterion two of ADC 2.250 if conditions of approval are met.

CONCLUSION

5.1 The design and operating characteristics of the proposed development are reasonably compatible with surrounding development and land uses, and any negative impacts have been sufficiently minimized if conditions of approval are met.

5.2 This criterion is met with the conditions listed under conditional use review criterion number two above.

Criterion (6) *Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.*

FINDINGS OF FACT

6.1 The findings of fact for ADC 2.250 conditional use review criterion number six above are incorporated here by reference. In summary, these findings indicate that the site does not have special purpose districts that will be impacted by the proposed development.

CONCLUSION

6.1 The site has no special purpose districts that require conditions of approval to mitigate.

6.2 This criterion is met without conditions.

Criterion (7) *The site is in compliance with prior land use approvals.*

FINDINGS OF FACT

7.1 Prior land use approvals on record include: 1) Site plan review for a change of use from a residential use to an office use with joint-use parking (SP-26-92); 2) Site plan review to expand a paved parking area with concurrent variance request to reduce the required buffer/screening area to five feet where 10 feet is required (SP-35-92); and 3) Replat to combine three parcels into two parcels (RL-02-15). There are no code compliance issues related to these land use approvals.

CONCLUSION

7.1 The site is in compliance with prior land use approvals.

7.2 This criterion is met without conditions.

Criterion (8) Sites that have lost their nonconforming status must be brought into compliance, and may be brought into compliance incrementally in accordance with Section 2.370.

FINDINGS OF FACT

8.1 The entire site is proposed to be redeveloped and it is not considered nonconforming.

CONCLUSION

8.1 This criterion is not applicable.

TENTATIVE PLAT REVIEW CRITERIA (ADC 11.180)

Criterion (1) Development of any remainder of property under the same ownership can be accomplished in accordance with this Code.

FINDINGS OF FACT

1.1 As illustrated on Attachment C.79 (Sheet C2.4), the replat would combine six parcels, plus the vacated portion of 6th Avenue, into one parcel under the same ownership. The proposed development would utilize the entire site.

CONCLUSION

1.1 The proposed replat does not create additional parcels nor leave any remaining amount of land.

1.2 This criterion is met without conditions.

Criterion (2) Adjoining land can be developed or is provided access that will allow its development in accordance with this Code.

FINDINGS OF FACT

2.1 This review criterion has been interpreted by the City Council to require only that adjoining land either have access, or be provided access, to public streets.

2.2 ADC 12.060 requires that development must have frontage on or approved access to a public street currently open to traffic.

2.3 As illustrated on Attachment C.79 (Sheet C2.4), the replat would combine six parcels, plus the vacated portion of 6th Avenue, into one parcel.

2.4 The proposed replat and street vacation will not impact the ability of adjoining land to develop.

2.5 Adjoining lands have direct access to public streets and will continue to have direct access after the proposed replat and street vacation.

CONCLUSIONS

2.1 The subject properties and surrounding properties have access to public streets, and the proposed replat with street vacation will not interfere with existing access or the public street right-of-way; nor will it impact the ability to develop adjoin land.

2.2 This criterion is met without conditions.

Criterion (3) The proposed street plan affords the best economic, safe, and efficient circulation of traffic possible under the circumstances.

FINDINGS OF FACT

3.1 The subject properties are presently served by public streets and will remain so with the replat and street vacation.

3.2 No new streets are proposed with the replat.

CONCLUSIONS

- 3.1 The proposed replat does not create additional parcels nor leave any remaining amount of land.
- 3.2 The proposed replat will not result in an increase to the potential intensity of development allowed on the site.
- 3.3 This criterion is met without conditions.

Criterion (4) The location and design allows development to be conveniently served by various public utilities.

FINDING OF FACT

- 4.1 The findings of fact for ADC 2.250 conditional use review criterion number four above are incorporated here by reference. In summary, these findings conclude that public utilities are available to the site and are adequate to serve the proposed development if conditions of approval are met.

CONCLUSION

- 4.1 Public utilities (sanitary sewer, water, and storm drainage) are in place and are adequate to serve the proposed development.
- 4.2 This criterion is met with the conditions listed under conditional use review criterion number four above.

Criterion (5) Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.

FINDINGS OF FACT

- 5.1 The findings of fact for ADC 2.250 conditional use review criterion number six above are incorporated here by reference. In summary, these findings indicate that the site does not have special purpose districts that will be impacted by the proposed development.

CONCLUSIONS

- 5.1 The site has no special purpose districts that require conditions of approval to mitigate.
- 5.2 This criterion is met without conditions.

REPLAT CONCLUSION

The application for a replat to combine six parcels, plus the vacated portion of 6th Avenue, into one parcel satisfies the applicable review criteria without conditions.

OVERALL CONCLUSION

As proposed and conditioned, the application for a street vacation, conditional use review and replat to develop a new fire station satisfies all of the applicable review criteria as outlined in this report.

OPTIONS FOR THE PLANNING COMMISSION

The Planning Commission has three options with respect to the proposal:

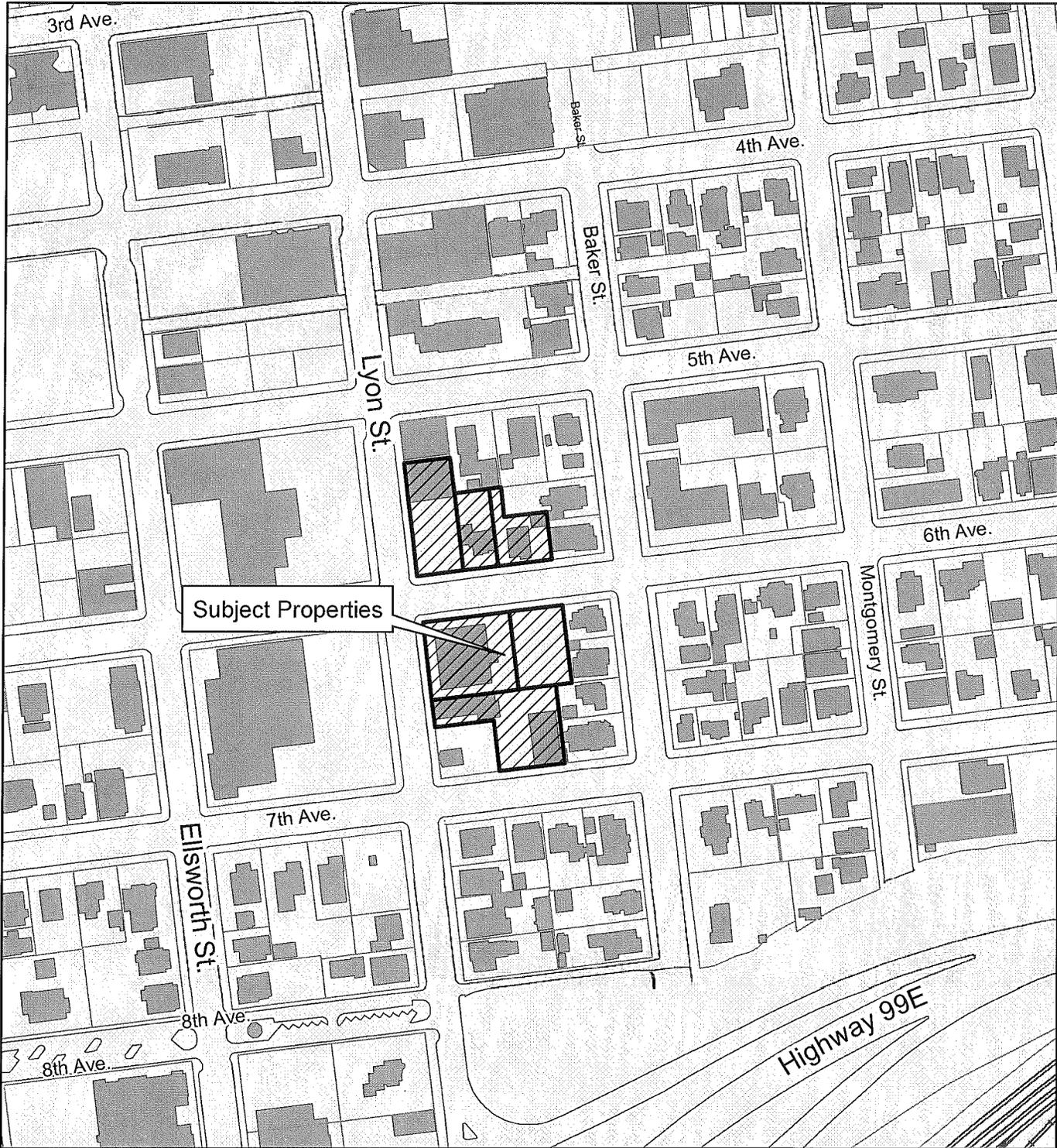
- Option 1: Recommend that the City Council approve request as proposed;
- Option 2: Recommend that the City Council approve the request with conditions of approval; or
- Option 3: Recommend that the City Council deny the request.

STAFF RECOMMENDATION

Based on analysis in this report, staff suggests that the Planning Commission recommend that the City Council approve the requested street vacation, conditional use review, and replat, as conditioned in this staff report. Motions to this effect will be provided for consideration during the public hearing on this application.

ATTACHMENTS

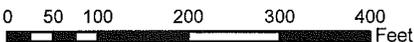
- A. Location Map
- B. Comments from Duane James Liner, ODOT Development Review Coordinator (dated Oct. 29, 2015)
- C. Applicant's Submittal:
 - C.1 – C.6 Street Vacation Findings and Exhibits
 - C.7 – C.58 Conditional Use and Replat Findings Narrative
 - C.59 – C.63 Summary of Neighborhood Meeting
 - C.64 – C.65 Trip Generation Calculations
 - C.66 Zoning Map
 - C.67 Street View 1
 - C.68 Aerial View 2
 - C.69 Building Elevations 3
 - C.70 Building Elevations 4
 - C.71 Materials Board 5
 - C.72 Title Sheet and Drawing Index (Sheet T1.1)
 - C.73 Code Analysis (Sheet T1.2)
 - C.74 Existing Conditions Plan (Sheet C1.1)
 - C.75 Demolition Plan (Sheet C2.0)
 - C.76 Site Plan (Sheet C2.1)
 - C.77 Grading Plan (Sheet C2.2)
 - C.78 Utility Plan (Sheet C2.3)
 - C.79 Tentative Replat (Sheet C2.4)
 - C.80 Details (Sheet C8.0)
 - C.81 Landscape Materials and Layout Plan - North (Sheet L2.1)
 - C.82 Landscape Materials and Layout Plan - South (Sheet L2.2)
 - C.83 Irrigation Plan (Sheet L3.1)
 - C.84 Planting Plan - North (Sheet L4.1)
 - C.85 Planting Plan – South (Sheet L4.2)
 - C.86 Irrigation and Planting Details (Sheet L8.1)
 - C.87 First Floor Plan (Sheet A2.1)
 - C.88 Second Floor Plan (Sheet A2.2)
 - C.89 Roof Plan (Sheet A2.3)
 - C.90 Building Elevations (Sheet A3.1)
 - C.91 Site Photometrics (E1.1PH)



Location Map: 110 - 623 Lyon St. SW, 129 - 133 6th Ave. SW and 123 - 177 7th Ave. SW



The City of Albany's Infrastructure records, drawings, and other documents have been gathered over many decades, using differing standards for quality control, documentation, and verification. All of the data provided represents current information in a readily available format. While the data provided is generally believed to be accurate, occasionally it proves to be incorrect; thus its accuracy is not warranted. Prior to making any property purchases or other investments based in full or in part upon the material provided, it is specifically advised that you independently field

June 2, 2015
 Planning Division
 City of Albany - 333 Broadalbin St. SW, Albany, Oregon 97321 (541) 917- 7550



Oregon

Kate Brown, Governor

**Department of Transportation
Region 2**

455 Airport Road SE, Bldg. B
Salem, Oregon 97301-5395

FILE CODE: 031 MP 10.85 vic.
DRS Case No. 6570 CHAMPS # N/A

Submitted to ePlans & cc by email

October 29, 2015

Melissa Anderson
Albany Community Development Department
333 Broadalbin Street SW
PO Box 490
Albany, OR 97321

SUBJECT: ODOT Comments for City Land Use File No. VC-03-15, CU-08-15, & RL-11-5
Project Review - Development of a New Albany Fire Station
Applicant: Fire Chief John Bradner Agent: Brian Varrichione, Mackenzie
Assessor's Map Number, T11S, R03W, Section 07BA, Tax Lot 1000, 1001,
1600, 2000, 2100, & 2701

Dear Melisa,

Thank you for notifying the Oregon Department of Transportation (ODOT) of the Project Review. This letter is submitted for inclusion in the public hearing record and ODOT should be considered a party to the land use action. Please provide a copy of the land use decision, notice of any time extensions or continuances, to ODOT at the address provided below, or you may provide notice to ODOT via e-mail. Electronic format is preferred.

Planning and Development Manager
Oregon Department of Transportation
Region 2 Headquarters
455 Airport Road SE, Building B
Salem, OR 97301-5395

Electronic documents can be directed to:

ODOTR2PLANMGR@ODOT.STATE.OR.US

ODOT staff has completed a review of the submitted materials for this proposed development of a new Albany Fire Station, and has the following comments.

The property abuts US 20 (Albany - Corvallis Highway, ODOT Hwy #031, also known as Lyon Street) and is subject to state laws administered by the Oregon Department of Transportation. These laws may require the applicant to obtain one or more state permits to carry out the intended use of the property, or to otherwise comply with state law without need for a permit.

ACCESS TO STATE HIGHWAY

The project review application materials show one proposed approach to US 20 (Lyon Street). This one approach is to serve the fire station apparatus bay. The applicant will need to submit an *Application for State Highway Approach* (access permit application) for this proposed approach to the highway. Applications are subject to review and approval consistent with Oregon Administrative Rule Chapter 734, Division 51.

Application for State Highway Approach can be found at:

<http://www.oregon.gov/ODOT/HWY/ACCESSMGT/Pages/Application-Forms.aspx>

Apparatus Bay Approach:

With the proposed new fire station, the existing apparatus bay approach is removed and replaced with a new one at the location of the proposed vacated SE 6th Avenue. With submittal of the *Application for State Highway Approach* for this approach please:

- Clearly indicate if the City is requesting relocating or removing the existing fire signal on Lyon Street. The fire signal on Ellsworth Street is also associated with this fire station and a clear indication of what is being proposed for this fire signal needs to be included as part of the application.
- Clearly indicate if the City is requesting emergency vehicle warning signs. If so, indicate the proposed signs and locations on the site plan.
- Provide an explanation on how fire apparatus returning to the fire station will maneuver into position for the next emergency call. In other words, ODOT does not recommend, encourage, or approve backing of emergency vehicles from the highway to the fire station bays. Any backing or maneuvering of the fire apparatus into the fire station bays should happen off of the highway.

ASSOCIATED ADDITIONAL HIGHWAY WORK

Bulb-outs, Curbing, and On Street Parking:

The site plan shows on street parking, curbing, and bulb-outs. The designs of these features will need to consider vehicle turning radius (to help determine curb placement location and curb radii) as well as sight distance. The design will also need to verify that parking does not impact/limit sight distance from the proposed approach or the line of sight to any associated emergency vehicle warning signs. Plans should clearly call out the proposed curb radii, travel lane widths, and bulb-out dimensions. Plans should show the design vehicle used and include turning templates. ODOT will need to review plans for this proposed work.

Existing drainage will be impacted with this proposal. Plans should be prepared that clearly show the drainage which is impacted and the proposed improvements that will be made as part of this project.

Crosswalk:

The site plan shows a marked crosswalk on the south leg of the US20 (Lyon St) / 6th Avenue. ODOT's approval must be obtained for any new marked crosswalk on the highway.

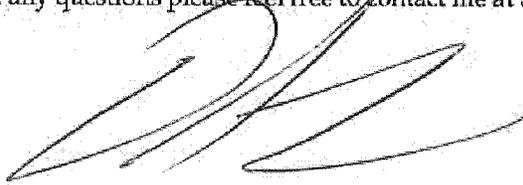
ODOT DRS#6570, CHAMPS# N/A
City of Albany Land Use File # VC-03-15, CU-08-15, & RL-11-5
Page 3 of 3

CONSTRUCTION ACTIVITIES IN ODOT RIGHT-OF-WAY

Please note the applicant will be required to contact the District 4 Maintenance office (541-757-4211) to obtain permits if construction activities are to occur within state highway right-of-way.

If you have any questions please feel free to contact me at 541-757-4140.

Sincerely,



Duane James Liner, P.E.
ODOT Development Review Coordinator

cc: Electronic copies provided to: Valerie Grigg Devis, ODOT / Kendall Weeks, ODOT / Scott Nelson, PE, ODOT / Dorothy Upton, ODOT / Julie Infante, ODOT / Ed Cantrell, ODOT / Ron Irish, City of Albany / Jeff Humphries, Mackenzie / Brian Varricchione, Mackenzie

Albany Fire Department - 6th Avenue Vacation



Sixth Avenue - Vacation Review Criteria

- (1) The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan city transportation or public facility plan.***

Fact: This section of 6th Avenue is not listed as a major street in the Albany Transportation System Plan.

Conclusion: Vacating this right-of-way is not inconsistent with the Albany Transportation System Plan.

- (2) The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.***

Fact: All existing properties will still have access to a public right-of-way if the City properties are combined. The City properties are being combined into a single property as part of the Fire Department project.

Conclusion: When the City properties are combined all properties will have access to a public street.

- (3) The requested vacation will not have a negative effect on traffic circulation or emergency service protection.***

Fact: While 6th Avenue between Lyon Street and Baker Street will be vacated, all properties will be able to access all the remaining streets in the area.

Conclusion: There will be no negative effect on traffic circulation or emergency service protection due to vacating this right-of-way.

- (4) The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.***

Fact: The right-of-way will be converted into a portion of the new City of Albany Fire Station. The Fire Station will be required to meet Albany Development Code standards.

Conclusion: The vacated right-of-way will be brought into Development Code compliance.

- (5) The public interest, present and future, will be best served by approval of the proposed vacation.***

Fact: The vacated right-of-way will be converted into a portion of the new City of Albany Fire Station.

Conclusion: It is in the public interest to have a new Fire Station. Approving this right-of-way vacation will allow the Fire Station to be completed.

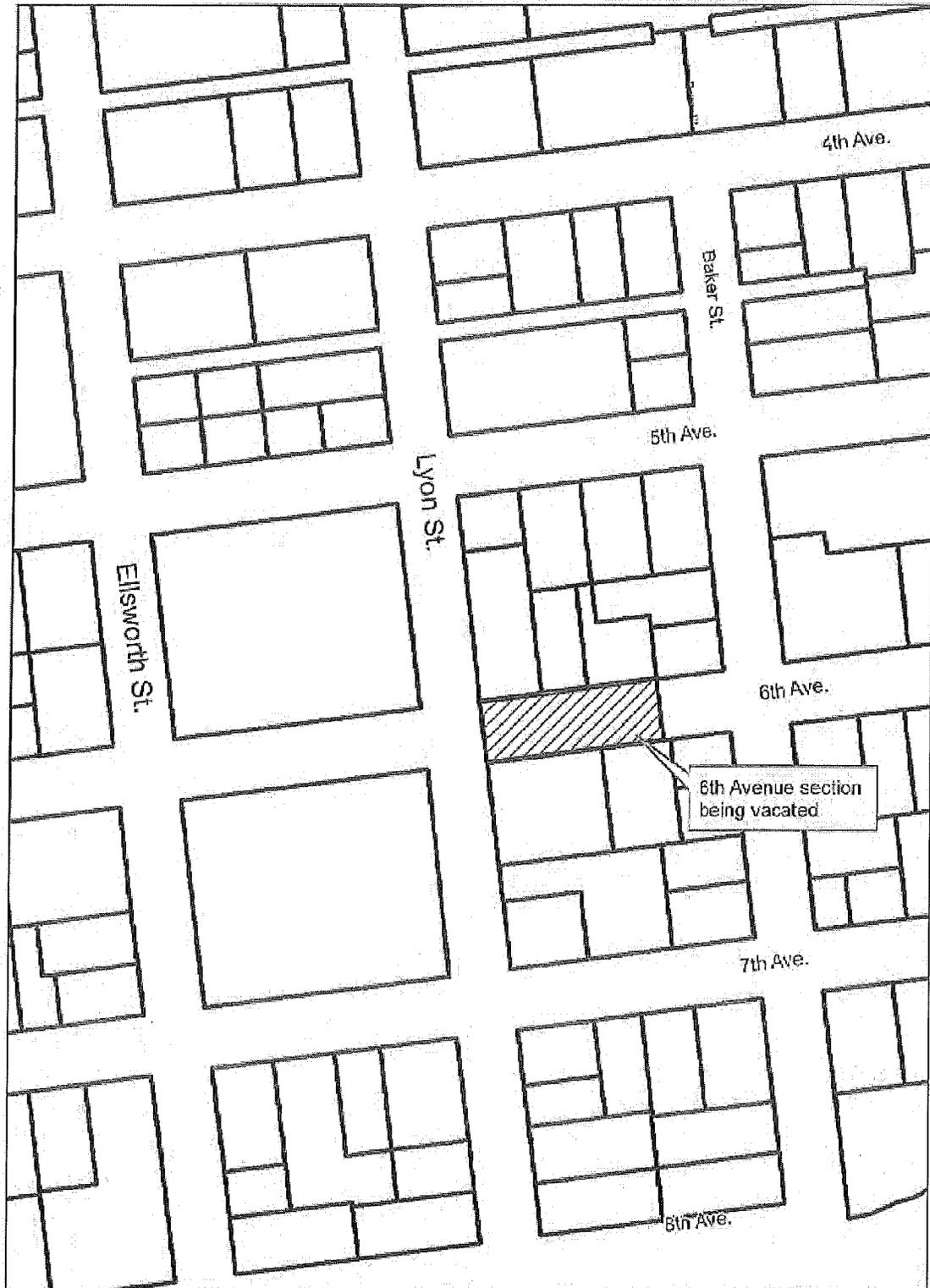
Legal Description of proposed Vacation of 6th Avenue.

The western 194.12 feet of 6th Avenue SE right-of-way between Blocks 6 and 7 of Eastern Addition to Albany subdivision, lying immediately east of the Lyon Street right-of-way, as shown on attached Exhibit A.

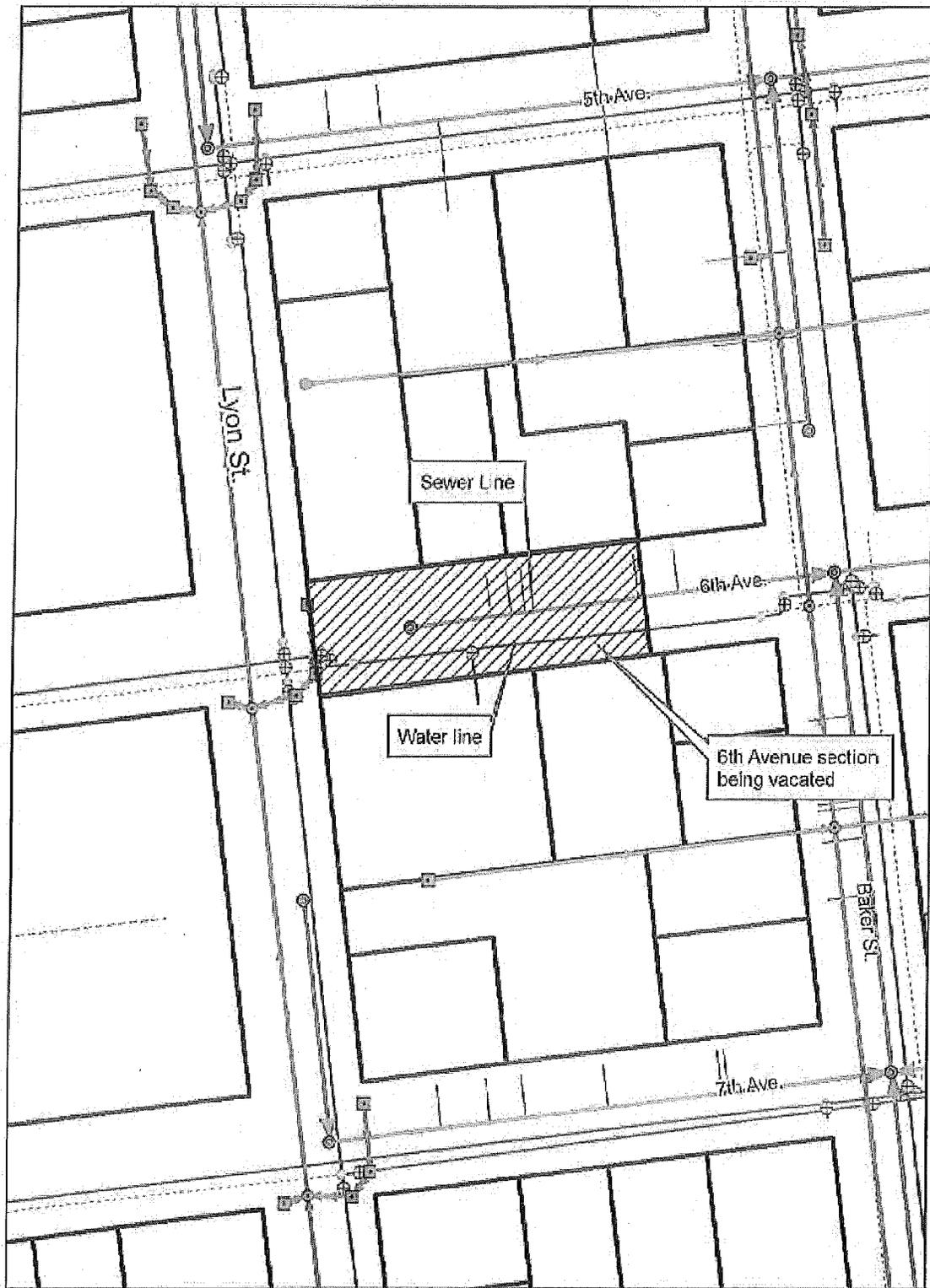
Properties to which the vacated area will be transferred

The properties that the vacated area will revert to are the properties immediately north and south of the right-of-way. The northern half will go to the northern properties and the southern half will go to the southern properties. All of the properties are owned by the City of Albany and are to be combined, along with the vacated right-of-way, into a single property.

Exhibit A - 6th Avenue Vacation



Site Plan - 6th Avenue Vacation



**CONDITIONAL USE
PERMIT, STREET
VACATION, TENTATIVE
REPLAT, & SITE PLAN
REVIEW-TREE FELLING**

To
City of Albany

For
Fire Station 11 Replacement
Lyon Street SE at 6th Avenue

Submitted
September 14, 2015
Revised October 9, 2015

Project Number
2140087.02

M.

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EXHIBITS

1. Zoning Map
2. Building Perspectives, Elevations, and Materials
3. Plans
4. Neighborhood Meeting Summary
5. Trip Generation Calculations
6. Street Vacation Findings
7. Preliminary Stormwater Report

I. PROJECT SUMMARY

Applicant: Albany Fire Department
Att: John Bradner, Fire Chief
333 Broadalbin Street SW
Albany, OR 97321
john.bradner@cityofalbany.net
(541) 917-7700

Owner: City of Albany
PO Box 490
Albany, OR 97321
(541) 917-7500

Contact: Mackenzie
Att: Brian Varricchione
1515 SE Water Avenue, Suite 100
Portland, OR 97214
bvarricchione@mcknze.com
(503) 224-9560

Location: Lyon Street SE between 5th Avenue SE and 7th Avenue SE
Tax Map 11S03W07BA, Tax Lots 1000, 1001, 1600, 2000, 2100, &
2701

Site Address: 500, 517, 519, 525, 527, 531, & 623 Lyon Street SE; 110, 129, 131, &
133 6th Avenue SE; and 123 & 177 7th Avenue SE

Site Size: 1.64 Acres

Zoning: LE – Lyon-Ellsworth District (western portion of site)
HM – Hackleman-Monteith District (eastern portion of site)

Comprehensive Plan: Village Center (western portion of site)
Residential – Low Density (eastern portion of site)

Adjacent Zoning: HM to the northeast, east, and southeast of site; LE to northwest,
west, and southwest of site.

Existing Structures: There are six existing buildings that will be demolished during the
development of the site.

Request: Conditional Use Permit, Tentative Replat, Site Plan Review for Tree
Felling, and Street Vacation for replacement of Albany Fire Station 11

II. INTRODUCTION

Description of Request

On May 19, 2015, the citizens of Albany passed a bond measure which allowed for the reconstruction of existing police and fire facilities which have become inadequate to serve the needs of the Police Department and Fire Department. Therefore, the Albany Fire Department is now requesting the review and approval of land use applications to allow for the reconstruction of Fire Station 11 on Lyon Street SE (US Highway 20). Required applications include a conditional use permit, a tentative replat to combine six parcels into one, site plan review for tree felling, and vacation of a portion of 6th Street between Lyon Street SE and Baker Street SE.

Existing Site & Surrounding Land Use

The proposed site for the new Fire Station 11 is comprised of six tax lots east of Lyon Street SE, including the location of the existing Fire Station 11. The site is within the area bounded by Lyon Street SE to the west, 5th Avenue SE to the north, Baker Street SE to the east, and 7th Avenue SE to the south (see Figure 1). The site is separated from Baker Street SE and 5th Avenue SE by intervening developed tax lots. 6th Avenue bisects the site but the majority of the street is proposed to be vacated to accommodate the development plan.

There are six existing buildings that will be demolished as a part of this development, including a laundromat, the current Fire Station 11, an investment company, an accountant, and two outbuildings.

The site is located partially within two different zoning districts. The western portion of the site is the mixed-use Lyon-Ellsworth District (LE) zone and the eastern portion of the site is the residential Hackleman-Monteith District (HM) zone. A fire station is categorized as a Community Service use and is a permitted use in the LE zone and a conditional use in the HM zone. The site is adjacent to but west of and outside the Hackleman Historic District.

Nearby land uses include the Hasty Freez southwest of the site, the Democrat-Herald and Mark Thomas GMC Buick across Lyon Street, and multi-family and single-family residences to the north, east, and south.



Figure 1: Site Boundaries

Description of Proposed Development

The applicant is proposing the development of a 24,265 square foot fire station to serve the City of Albany, consisting of the apparatus bay, living quarters, offices, and classroom space. The total site area is 70,925 SF. Of that, 25% will be building area, 44% will be paved, and 31% will be landscaped.

Development will include a total of 47 on-site parking spaces, 19 of which will be dedicated to official fire station use. Of the 28 parking spaces open to the public, 2 will be accessible spaces. There will also be on-street parking spaces on Lyon Street and in the 6th Street right-of-way. The applicant proposes extensive landscaping and a plaza fronting on Lyon Street and a vegetated buffer along the east site boundary to separate the development from adjoining residences. The building itself is proposed to have a brick façade with a generous amount of windows to be an inviting and attractive addition to the downtown Albany streetscape. The structure will be oriented so that the apparatus bay has front and rear doors to both open directly onto Lyon Street and to allow access to and from the east in the 6th Avenue corridor.

III. NARRATIVE & COMPLIANCE

Oregon Revised Statutes

Chapter 271 - Use and Disposition of Public Lands Generally; Easements

Response: The development proposal is contingent upon the vacation of a portion of 6th Street SE between Lyon Street and Baker Street. ORS Chapter 271 (271.080-271.230) governs the process for vacating streets within incorporated cities. Findings in support of the vacation request are included in Exhibit 6. This standard is met.

City of Albany Development Code

Article 1 – Administration and Procedures

Application Procedures

1.200 Land Use Application Procedures.

- (3) *When a proposal involves more than one application for the same property, the applicant(s) may submit concurrent applications that shall be processed simultaneously in accordance with the highest numbered procedure specified. When concurrent applications are received and accepted as complete, the 120-day requirement of Section 1.220(2) shall apply as if a single application had been made.*

Response: The proposed development will require numerous land use permits and processes. It will involve a street vacation for a portion of 6th Avenue; a re-plat to remove property lines and combine parcels; a Conditional Use permit review for a Community Services land use; and site plan review for tree felling. All of these applications will be submitted and processed simultaneously. Since the street vacation requires hearings in front of the Planning Commission and the City Council, the City Council will be the decision-making body for all associated applications. This standard is met.

1.204 Neighborhood Meeting Standards.

- (1) *The applicant shall consult with City staff to determine an appropriate meeting date, time, and place given the location of the proposed development and availability of staff to attend.*
- (2) *The applicant shall send mailed notice of the public meeting to the Community Development Department Director and all property owners within a minimum distance of 300 feet of the boundaries of the subject property with the specific area to be determined by the Director based on the project scale, land use and transportation patterns or anticipated public interest in the project. If any part of the subject property is within the boundaries of a neighborhood association recognized by the City of Albany or within 300 feet of any other neighborhood association recognized by the City, notice shall be sent to the designated representative(s) of such neighborhood association(s). The property owner list shall be compiled from county tax assessor's property owner list from the most recent property tax assessment roll. The address for the designated representative(s) of the affected neighborhood association(s) shall be obtained from the City. The notice shall be sent a minimum of 10 days and no more than 30 days before the meeting, and shall include:*
- (a) *Date, time and location of the public meeting.*
- (b) *A brief written description of the development proposal and proposed use(s) with enough specificity so that the project is easily discernable.*

- (c) *The location of the subject property(ies), including address (if applicable), nearest cross streets and any other easily understood geographical reference, and a map (such as a tax assessors map) that depicts the subject property.*
- (3) *The applicant's presentation at the neighborhood meeting shall include:*
 - (a) *A map depicting the location of the subject property(ies) proposed for development.*
 - (b) *A visual description of the project including a site plan, tentative subdivision plan and elevation drawings of any proposed structures, when applicable.*
 - (c) *A description of the nature of the proposed use(s) including but not limited to, sizes and heights of structures, proposed lot sizes, density, etc.*
 - (d) *The expected or anticipated impacts from the proposed development (e.g. traffic, storm drainage, tree removal, etc.).*
 - (e) *Mitigation proposed by the applicant to alleviate the expected/anticipated impacts.*
 - (f) *An opportunity for the public to provide comments.*

Response: The applicant coordinated with Planning Division staff to arrange for and publicize the required neighborhood meeting, which was held on August 12, 2015. The notice and materials followed the above procedures. A summary of the meeting is attached as Exhibit 4. This standard is met.

Article 2 – Review Criteria

Conditional Uses

2.250 Review Criteria.

Requests for conditional uses will be approved if the review body finds that the application meets all of the following criteria, either outright or with conditions that bring the proposal into compliance:

- (1) *The proposed use is consistent with the intended character of the base zone and the operating characteristics of the neighborhood.*

Response: There is currently a fire station on the site; this project aims to improve the appearance, effectiveness, and reach of the existing use. The new station acts as a public safety service and will protect and enable healthy growth of the surrounding community. This standard is met.

- (2) *The proposed use will be compatible with existing or anticipated uses in terms of size, building scale and style, intensity, setbacks, and landscaping or the proposal mitigates difference in appearance or scale through such means as setbacks, screening, landscaping or other design features.*

Response: While the site itself will be larger than the majority of other developments in the area, the building and site features will be developed to a human scale that is responsive to its surroundings, with the structure located on Lyon Street and parking and landscaping to the rear away from existing residences. The site will integrate well into this transition area between mixed-use and residential zones. The site will be landscaped and situated in a manner that will mitigate the size of the site. This standard is met.

- (3) *The transportation system can support the proposed use in addition to the existing uses in the area. Evaluation factors include street capacity and level of service, on-street parking impacts, access requirements, neighborhood impacts and pedestrian safety.*

Response: A transportation impact study was not required for this development since the new fire station would be an outgrowth of the existing facility and would also eliminate multiple existing buildings and land uses. Based on the size and scope of the proposed fire station, Mackenzie transportation engineers estimate that the project would generate 87 average daily trips, of which 9 would be during the evening peak hour. These figures are lower than the trips from the existing

structures that will be demolished, so there will be a net reduction in traffic from the site (see Exhibit 5). To minimize impacts on Highway 20, the only driveway proposed directly onto Lyon Street is the apparatus bay driveway, which would be used primarily for emergency response. Driveways for employees and visitors would connect to Baker Street and 7th Avenue. Limited parking is proposed on Lyon Street in areas bounded by curb bump-outs to separate the parking spaces from the travel lanes. Pedestrian connections are proposed to 7th Avenue and Lyon Street, and bicycle parking will be located on site for use by employees and visitors. It is not anticipated that the vehicle traffic associated with the station would cause any of the existing transportation network to fall below mobility standards for either the City of Albany or the Oregon Department of Transportation (ODOT), which has jurisdictional control over Lyon Street and Ellsworth Street (Highway 20). This standard is met.

- (4) *Public services for water, sanitary and storm sewer, water management, and for fire and police protection, can serve the proposed use.*

Response: As shown in the attached plans, public services such as water, sanitary, storm sewer, and water management are currently serving the existing fire station on site (as well as other uses) and can continue to serve the new facility without requiring infrastructure upgrades. The site is within a well-connected and highly developed central location that allows for acceptable response time for fire and police protection in the event outside assistance is needed. This standard is met.

- (5) *The proposal will not have significant adverse impacts on the livability of nearby residentially zoned lands due to:*
- (a) *Noise, glare, odor, litter, or hours of operation.*
 - (b) *Privacy and safety issues.*

Response: The Development Code classifies fire stations as Community Service uses, and the proposed fire station will replace an existing fire station already on-site. As such, the new building itself will not result in any significant changes in off-site impacts. Noise, dust, and vibration impacts will be similar to those from the existing fire station, primarily originating from emergency vehicle trips to and from the facility, plus infrequent use of the emergency generator during power failures and occasional test cycles. Lighting glare is minimized through the use of shielded light fixtures as illustrated on the photometrics plan (Exhibit 3 Sheet E1.1PH). The facility is not anticipated to generate odors other than those from the trash enclosure, which is located over 13 feet from the property line and more than 15 feet from dwelling windows. The landscape plan (Exhibit 3 Sheets L4.1 and L4.2) includes a variety of parking lot trees and shrubs lining the exterior of the parking lot and site with the intention of segregating the neighbors from any noise, glare, odors, and dust associated with the fire station. Overall, the improvements proposed for the replacement fire station will increase the livability of neighboring uses because of enhanced screening and landscaping and use of appropriate shielding on lighting fixtures, and the apparatus bay would be farther away from adjoining residences than the existing fire station. This standard is met.

- (6) *Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.*

Response: The proposal is not sited within any special purpose districts. This standard does not apply.

Vacations

2.630 Review Criteria.

A vacation request may be approved if the review body finds that the applicant has shown that all of the following review criteria are met:

- (1) *The requested vacation is consistent with relevant Comprehensive Plan policies and with any street plan, city transportation or public facility plan.*

- (2) *The requested vacation will not have a negative effect on access between public rights-of-way or to existing properties, potential lots, public facilities or utilities.*
- (3) *The requested vacation will not have a negative effect on traffic circulation or emergency service protection.*
- (4) *The portion of the right-of-way that is to be vacated will be brought into compliance with Code requirements, such as landscaping, driveway access, and reconstruction of access for fire safety.*
- (5) *The public interest, present and future, will be best served by approval of the proposed vacation.*

Response: Findings in support of the proposed street vacation request are attached as Exhibit 6. This standard is met.

Article 3 – Residential Zoning Districts

Zoning Districts

3.020 Establishment of Residential Zoning Districts.

In order to implement the mixed-use and livability concepts in the Town Center and Albany Comprehensive Plans, the following zoning districts are created:

- (7) *HM—HACKLEMAN-MONTEITH DISTRICT. The HM district is intended primarily to preserve the existing single-family residential character of the Hackleman and Monteith National Register Historic Districts. Conversion of single-family residential structures to other uses, including multi-family residential, is not allowed.*

Response: The eastern portion of the proposed site, as shown in Exhibit 1, is within the Hackleman-Monteith District and is subject to its development standards. There are no existing single-family residences in the HM zone proposed for conversion to other uses as part of this application. This standard is met.

Schedule of Permitted Uses

3.050 Schedule of Permitted Uses.

[...]

The abbreviations used in the schedule have the following meanings:

[...]

CU Use permitted conditionally under the provisions of Sections 2.230-2.260 through a Type III procedure.

USES ALLOWED IN RESIDENTIAL ZONING DISTRICTS	
Use Categories	HM
Institutional	
Community Services	CU

Response: Community services are permitted conditionally under the provisions of Sections 2.230-2.260 through a Type III procedure. This application includes a request for a Type III Conditional Use approval. This standard is met.

Development Standards*3.190 Purpose.*

Development standards are intended to promote site planning and design that consider the natural environment, site intensity, building mass, and open space. The standards also promote energy conservation, needed privacy, safe and efficient parking areas for new development, and improve the general living environment and economic life of a development. Table 1, on the following page, summarizes the basic development standards. It should be used in conjunction with the sections immediately succeeding the table, which address special circumstances and exceptions. See Article 8 for design standards for single-family and multiple-family developments.

TABLE 3-1 EXCERPT: RESIDENTIAL DISTRICT DEVELOPMENT STANDARDS (EXCERPTS FOR HM ZONE AS APPLICABLE TO NON-RESIDENTIAL PROJECTS)		
Standard	Allowance	Proposed
Minimum Lot Depth	65'	76' (portion within HM zone)
Minimum Front Setback ¹	15'	N/A (front is on Lyon Street in LE zone)
Maximum Front Setback	None	N/A (front is on Lyon Street in LE zone)
Minimum Interior: Two or More Stories ¹	6'	41'
Minimum Building Separation	N/A	N/A
Maximum Height ²	30'	30'
Maximum Lot Coverage	60%	58% (portion within HM zone)
Minimum Open Space	N/A	N/A
Minimum Landscaped Area	All yards adjacent to streets	All yards adjacent to streets

(1) Additional setbacks may be required, see Sections 3.230-3.330 and the buffer matrix at 9.210; exceptions to Setbacks for Accessibility Retrofits are in Section 3.263; Zero-Lot Line standards are in Sections 2.365 and 2.370.

(2) See exceptions to height restrictions, Section 3.340.

Response: As illustrated on Exhibit 3 and noted in the table above, the proposed fire station complies with all applicable development standards for the portion within the HM zone. This standard is met.

Setbacks*3.330 Special Setbacks for Education and Religious Institutions, Public and Semi-Public Buildings.*

Any new construction of an educational institution, religious institution, or public or semi-public building shall be set back at least 25 feet from any property line adjoining or directly across public right-of-way from any residential district. No required front or interior yard of the lot on which such building or use is located shall be used for stockpiling or storage of materials or equipment. All other setbacks of the district where the property is located continue to apply.

Response: As illustrated on Exhibit 3 Sheet C2.1, the proposed building would be located upwards of 40 feet from any property line within the HM zone (on the rear of the site). No materials or equipment will be stockpiled or stored in the front or interior yard of the lot. This standard is met.

Building Height*3.340 Height Exceptions.*

Height limitations are shown in Table 1, Development Standards. See also Table 2, Accessory Structure Standards.

- (1) *Roof Structures and Architectural Features.* Roof structures for the housing of elevators, stairways, tanks, ventilating fans and similar equipment required to operate and maintain the building, fire walls, skylights, towers, flagpoles, chimneys, smokestacks, wireless masts, antennas, steeples, and similar structures may be erected above the height limits prescribed in this Article provided that no roof structure, feature, or any other device above the prescribed height limit shall be allowed or used for the purpose of providing additional floor space.
- (2) *Religious Institutions and Public and Semi-Public Buildings.* In zoning districts where religious institutions and certain public and semi-public buildings require Conditional Use approval, the height restrictions may be waived as a part of the Conditional Use proceedings, provided that a request for such has been noted in the public hearing notice.

Response: As illustrated in Exhibit 3 Sheet A3.1, the 30-foot building height complies with the applicable zoning standard, so no height exceptions have been requested. This standard does not apply.

Article 5 – Mixed Use Zoning Districts

Zoning Districts

5.030 Establishment of Mixed Use Zoning Districts.

In order to implement the mixed-use and livability concepts in the Town Center and Albany Comprehensive Plans, the following zoning districts are created:

- (5) *LE – LYON-ELLSWORTH DISTRICT.* The LE district is intended primarily as a location for development that serves the Historic Downtown district and Downtown Central Business district. This district is the most desirable location in the Central Albany area for parking structures with ground-floor commercial uses.

Response: The western portion of the proposed site, as shown in Exhibit 1, is within the Lyon-Ellsworth District and is subject to its development standards. This standard is met.

Schedule of Permitted Uses

5.060 Schedule of Permitted Uses.

[...]

The abbreviations used in the schedule have the following meanings:

[...]

S Use permitted that requires a site plan approval prior to the development or occupancy of the site or building.

TABLE 5-1 SCHEDULE OF PERMITTED USES	
Use Categories	LE
Institutional	
Community Services ¹	S

(1) Community Service Uses. Community Service uses that may have significant off-site impacts, such as public swimming pools, public safety facilities and homeless shelters, may be considered through the Conditional Use process.

Response: Community Services are permitted but require City approval of the site plan prior to development or occupancy. The proposed fire station is classified as a Community Service Use but since it is also a public safety facility the City is reviewing it under the provisions of the Conditional Use process. This standard is met.

Development Standards

5.090 Purpose.

Development standards are intended to promote site planning and design that consider the natural environment; site intensity, building mass and open space. The standards also promote energy conservation, needed privacy, and safe and efficient parking areas for new development; and improve the general living environment and economic life of a development. Table 5-2 summarizes the basic development standards. It should be used with the sections immediately following the table, which address special circumstances and exceptions. Additional design standards for commercial and multi-family developments are located in Article 8.

TABLE 5-2 EXCERPT: LE – MIXED-USE VILLAGE CENTER DEVELOPMENT STANDARDS		
Standard	Allowance	Proposal
Minimum Lot Size	2,000 SF	46,520 SF (portion within LE zone)
Maximum Building Footprint	None	15,400 SF (portion within LE zone)
Lot Width, minimum	20'	450'
Lot Depth, minimum	50'	67'
Landscaped Area	All yards adjacent to streets	All yards adjacent to streets
Minimum Open Space	None	25.6% (portion within LE zone)
Maximum Front Setbacks	None	10'
Minimum Front Setback	0'	10'
Minimum Interior Setback	0'	74'
Minimum Setback to Garage Entrance	10'	26'
Height, maximum	60'	30'
Lot Coverage, maximum	100%	74.4% (portion within LE zone)

Response: As illustrated on Exhibit 3 and noted in the table above, the proposed fire station complies with all applicable development standards for the portion within the LE zone. This standard is met.

Setbacks

5.210 Special Setbacks for Schools, Churches, Public and Semi-Public Buildings.

Any new construction of a school, church, or public or semi-public building must be set back at least 25 feet from any property line abutting any residential district. No required front or interior setback of the lot on which such building or use is located may be used for stockpiling or storing materials or equipment. All other setbacks of the district where the property is located continue to apply.

Response: As illustrated on Exhibit 3 Sheet C2.1, the proposed building would be located upwards of 40 feet from any property line within the abutting residential HM zone (on the rear of the site). This standard is met.

Height

5.250 Height Exceptions.

- (1) Roof structures for the housing of elevators, stairways, tanks, ventilating fans and similar equipment required to operate and maintain the building, fire walls, skylights, towers, flagpoles, chimneys, smokestacks, wireless masts, antennas, steeples, and similar structures may be erected above the height limits prescribed in this article, provided that no roof structure, feature,

or any other device above the prescribed height limit may be allowed or used for the purpose of providing additional floor space. Antennas may exceed the minimum building height but must meet the standards outlined in 5.070(20). Towers must meet the standards in 8.500.

- (2) *Religious Institutions and Public and Semi-Public Buildings.* In zoning districts where religious institutions and certain public and semi-public buildings require Conditional Use approval, the height restrictions may be waived as a part of the Conditional Use proceedings, provided that a request for such has been noted in the public hearing notice.

Response: As illustrated in Exhibit 3 Sheet A3.1, the 30-foot building height complies with the applicable zoning standard, so no height exceptions have been requested. This standard does not apply.

Outside Storage

5.370 Screening of Refuse Containers.

The following standards apply to all development, except for one and two family dwellings. Any refuse container or disposal area that would otherwise be visible from a public street, customer or resident parking area, any public facility, or any residential area, must be screened from view by placement of a sight obscuring fence, wall, or hedge at least 6 feet tall. Refuse disposal areas may not be located in required setback areas or buffer yards and must be contained within the screened area. No refuse container shall be placed within 15 feet of a dwelling window.

Response: As shown in the attached plans (Exhibit 3 Sheet C2.1) and drawings (Exhibit 2 page 3), a fenced trash enclosure will be located on site, northeast of the building. The enclosure will be screened from neighboring properties by a 6-foot-tall chain link fence (with slats), landscape buffering, and screening as illustrated on Exhibit 3 Sheet L4.1. The enclosure will be located over 13 feet from the property line and more than 15 feet from dwelling windows. This standard is met.

Article 7 – Historic Overlay District

7.010 Applicability.

This article is applied:

- (1) *To properties in the Downtown, Hackleman, Monteith or Albany Municipal Airport National Register Historic Districts as identified in Figure 7-1 and 7-2.*
- (2) *To all other structures and sites that appear on the City's adopted Local Historic Inventory, including individually designated National Register Historic Landmarks.*

Response: The proposed development is adjacent to the Hackleman Historic District but does not fall within its boundaries in any location. This standard does not apply.

Article 8 – Design Standards

Commercial and Institutional Site Design

8.310 Purpose.

These sections are intended to set threshold standards for quality design in new commercial, mixed-use, and institutional development. Good design results in buildings that are visually compatible with one another and adjacent neighborhoods and contribute to a commercial district that is attractive, active and safe. These qualities in turn contribute to the creation of commercial districts that facilitate easy pedestrian movement and a rich mixture of land uses.

Response: Care has been taken to design a community service building that contributes aesthetically to the urban fabric of the neighborhood in which it is placed. The design focuses on maximizing efficacy as a fire station while also creating a landmark that will invigorate the community. This standard is met.

8.315 Applicability.

These standards apply to the design of new commercial, mixed-use, or institutional development and to the expansion of existing developments in any district.

Response: Fire stations are classified as a Community Service, thus falling within the “institutional development” designation that triggers compliance with the commercial and institutional site design standards. The following section of the narrative will discuss how the proposal satisfies the required design standards. This standard is met.

8.330 Building Orientation.

Building orientation and maximum setback standards are established to help create an attractive streetscape and pleasant pedestrian environment.

- (1) *New buildings shall be oriented to existing or new public streets. Building orientation is demonstrated by placing buildings and their public entrances close to streets so pedestrians have a direct and convenient route from the street sidewalk to building entrances.*
 - (a) *On sites smaller than three acres, new buildings shall be oriented to the public street/sidewalk and off-street parking shall be located to the side or rear of the building(s), except where it is not feasible due to limited or no street frontage, the site is an infill site less than one acre, conservation of natural resources, or where there are access restrictions.*
 - (b) *Buildings on sites larger than three acres may be setback from the public street and oriented to traffic aisles on private property, if the on-site circulation system is developed like a public street with pedestrian access, landscape strips and street trees.*
- (2) *Customer entrances should be clearly defined, highly visible, using features such as canopies, porticos, arcades, arches, wing walls, and planters.*

Response: The primary frontage of the building will be on Lyon Street SE. This orientation will provide emergency vehicle access to Lyon Street SE directly from the apparatus bay within the fire station. Furthermore, the front of the public portion of the building has an entrance approximately ten feet behind the front property line. Landscaping between the right-of-way and the building will be established on both sides of the driveway and will extend the width of the property.

There are two dedicated parking lots for this fire station, both of which are on the side or rear of the building, not between the front of the building and the sidewalk. The parking area on the south side of the building is composed of 28 standard stalls that will be available for the public. The parking lot on the north and east side of the building includes 19 standard parking stalls that are dedicated as official use areas by the fire station, including employee parking. In addition, there will be on street parking available on Lyon Street SE on both sides of the apparatus bay driveway.

The proposed orientation will maximize exposure to Lyon Street SE to maintain and enhance a safe and inviting section of the street for pedestrian and motor vehicles. As illustrated on Exhibit 3 Sheets C2.1 and A3.1, the main public entrance is clearly defined by a canopy, multiple windows, and an entry plaza. This standard is met.

8.340 General Building Design.

New commercial buildings shall provide architectural relief and interest, with emphasis at building entrances and along sidewalks, to promote and enhance a comfortable pedestrian scale and orientation. Blank walls shall be avoided except when not feasible.

Response: As illustrated on Exhibit 3 Sheet A3.1 and Exhibit 2, blank walls are minimized on façades which are publicly visible. The building elevations make use of a brick façade with fiber cement accent panels, a cornice, substantial windows, glass rollup doors in the apparatus bay, and sunshades and canopies over selected windows and doors. This standard is met.

- (1) *Ground floor windows shall be provided along frontages adjacent to sidewalks. The main front elevation(s) of buildings shall provide windows or transparency at the pedestrian level in the following minimum proportions:*

TABLE 8-1: REQUIRED WINDOW TRANSPARENCY PERCENTAGES BY DISTRICT	
District	% Transparency
RC, CC, NC, OP, MUC	25%
MS, LE, PB, ES, MUR, WF	50%
HB, CB	75%

The minimum window and door requirements are measured between 2 and 8 feet from the ground. Only the glass portion of doors may be used in the calculation.

If there are upper floor windows, they shall continue the vertical and horizontal character of the ground level windows.

Response: Due to the site’s location within the Lyon-Ellsworth (LE) zone, fifty percent of the Lyon Street elevation is required to be transparent between a height of 2 and 8 feet above ground. This elevation has a length of 172 feet, so a minimum of 516 square feet of windows are required in this height range. As illustrated on Exhibit 3 Sheets A2.1 and A3.1, the proposed building design provides 617 square feet (60%) transparency from the glass overhead doors, storefront window system, and vestibule. The upper floor windows mirror the style and locations of those on the first floor to the extent possible. This standard is met.

- (2) *Walls that are visible from a public street shall include a combination of architectural elements and features such as offsets, windows, entry treatments, wood siding, brick, stucco, synthetic stucco, textured concrete block, textured concrete), and landscaping.*

Response: As illustrated on Exhibit 3 Sheet A3.1, the proposed building design incorporates brick, fiber cement accent panels, multiple windows and glass overhead doors, a canopy above the public entrance, and a decorative cornice. Exhibit 3 Sheets L4.1 and L4.2 illustrate the proposed landscaping that is intended to complement the building design. This standard is met.

8.350 Street Connectivity and Internal Circulation.

The following standards emphasize the importance of connections and circulation between uses and properties. The standards apply to both public and private streets.

- (1) *New commercial buildings may be required to provide street or driveway stubs and reciprocal access easements to promote efficient circulation between uses and properties, and to promote connectivity and dispersal of traffic.*

Response: The Fire Department investigated opportunities to interconnect the site with the adjoining Hasty Freez property in order to minimize the number of driveways on the abutting streets. However, due to the orientation and circulation pattern of the existing Hasty Freez business it was not possible to connect the sites. The design team coordinated with Albany Engineering and ODOT staff and

determined to restrict parking area access to Baker Street and 7th Avenue rather than connecting the parking lots to Lyon Street. This standard is met.

- (2) *The internal vehicle circulation system of a commercial development shall continue the adjacent public street pattern wherever possible and promote street connectivity. The vehicle circulation system shall mimic a traditional local street network and break the development into numerous smaller blocks.*

Response: The location of the apparatus bay driveways on Lyon Street and Baker Street is midway between 5th Avenue and 7th Avenue, thereby mimicking the location of 6th Avenue, which is proposed to be vacated to accommodate the proposed facility. This standard is met.

- (3) *Traffic aisles shall not be located between the building(s) and the sidewalk(s), except as provided in (4) below, or where drive-through windows are permitted, sites are constrained by natural resources, or are infill sites less than one acre.*

Response: No traffic aisles are proposed to be located between the building and the sidewalk. The apparatus bay driveway is perpendicular rather than parallel to Lyon Street and will function more similarly to an intersecting street than to a drive aisle. This standard is met.

- (4) *Where drop off facilities are provided, they shall be designed to meet the requirements of the American with Disabilities Act but still provide for direct pedestrian circulation.*

Response: No drop-off facilities are proposed for this development. This standard does not apply.

- (5) *Internal roadways shall be designed to slow traffic speeds. This can be achieved by keeping road widths to a minimum, allowing parallel parking, and planting street trees to visually narrow the road.*

Response: Traffic will likely be generally slow due to the relatively small parking lots. Internal circulation within parking areas is designed to discourage aggressive driving and ensure a safe environment for pedestrian and motor vehicle operators. The use of landscaping and islands will create a pedestrian-focused environment and an aesthetically pleasing and environmentally friendly design. This standard is met.

8.360 Pedestrian Amenities.

Amenities such as awnings, seating, special paving and planters can have a dramatic affect on the pedestrian environment. Commercial developers should give as much thought to the pedestrian environment as they give to vehicle access, circulation and parking. The standards for pedestrian amenities are related to the scale of the development and also provide the flexibility for the developer to select the most appropriate amenities for the particular site and use.

- (1) *All new commercial structures and improvements to existing sites shall provide pedestrian amenities. The number of pedestrian amenities shall comply with the following sliding scale.*

REQUIRED PEDESTRIAN AMENITIES	
Size of Structure or Improvement	Number of Amenities
Less than 5,000 sf	1
5,000 – 10,000 sf	2
10,001 – 50,000 sf	3
More than 50,000 sf	4

- (2) *Acceptable pedestrian amenities include the following improvements. No more than two of any item may be used to fulfill the requirement:*

- (a) Sidewalks at least 10 feet wide with ornamental treatments (e.g., brick pavers), or sidewalks that are 50 percent wider than required by the Code.
 - (b) Benches or outdoor public seating for at least four people.
 - (c) Sidewalk planter(s) enclosing a total of 8 square feet.
 - (d) Pocket parks or decorative gardens (minimum usable area of 300 square feet).
 - (e) Plazas (minimum usable area of 300 square feet).
 - (f) Street trees 50 percent larger than required by the Code.
 - (g) Weather protection (awnings, etc.).
 - (h) Other pedestrian amenities that are not listed but are similar in scale and benefit.
- (3) Pervious pedestrian amenities can include approved vegetated post-construction stormwater quality improvements.
- (4) Pedestrian amenities shall meet the following standards:
- (a) Amenities shall be located outside the building main entrance, along pedestrian corridors, or near transit stops. Amenities shall be visible and accessible to the general public from an improved public or private street. Access to pocket parks, plazas, and sidewalks must be provided by a public right-of-way or a public access easement.
 - (b) Amenities are not subject to setback requirements.
 - (c) Amenities are consistent with the character and scale of surrounding developments. For example, similarity in awning height, bench style, planter materials, street trees, and pavers is recommended to foster continuity in the design of pedestrian areas. Materials should be suitable for outdoor use, easily maintained, and have a reasonably long life cycle (e.g., 10 years before replacement).

Response: The proposed building size of 24,265 square feet requires the inclusion of three pedestrian amenities. As illustrated in Exhibit 3, proposed amenities include a plaza with seating walls, weather protection over the main entryway, footbridges over the stormwater facility, 30-square-foot sidewalk planters, and street trees. The plaza, weather protection, and footbridges are all near the main building entrance. A consistent theme has been utilized for the amenities to ensure harmony with the building design. This standard is met.

8.370 Pedestrian Connections.

- (1) New retail, office and institutional buildings at or near existing or planned transit stops shall provide convenient pedestrian access to transit.
- (2) Walkways shall be provided that connect building entrances and streets adjoining the site.
- (3) Pedestrian connections to adjoining properties shall be provided except where impractical. Pedestrian connections shall connect the on site circulation system to existing or proposed streets, walkways, and driveways that abut the property. When adjacent properties are undeveloped or have the potential to be redeveloped, streets, accessways and walkways on site shall be laid out or stubbed to allow for extension to the adjoining property. For the purposes of this section, "impractical" means where one or more of the following conditions exists:
 - (a) Physical or topographic conditions make a connection impracticable. Such conditions include but are not limited to freeways, railroads, steep slopes, wetlands or other bodies of water where a connection could not reasonably be provided;
 - (b) Buildings or other existing development on adjacent land physically preclude a connection now or in the future considering the potential for redevelopment; or
 - (c) Where streets or accessways would violate provisions of leases, easement, covenants, restrictions or other agreements existing as of May 1, 1995, which preclude a required street or accessway connection.

Response: The fire station site is 0.2 miles from the transit stop at the Albany Transit Station (112 10th Avenue SW) and can be reached via the abutting public sidewalk on Lyon Street. An accessible walkway is proposed between the main building entrance and the public sidewalk, and sidewalks will also connect to 7th Avenue and Baker Street. The Fire Department investigated opportunities to interconnect the site with the adjoining Hasty Freez property in order to minimize the number of driveways on the abutting streets. However, due to the orientation and circulation pattern of the existing Hasty Freez business it was not possible to connect the sites. Due to the location, orientation, and usage of existing developed neighboring properties it was deemed impractical to directly connect the on-site pedestrian paths with neighboring properties, particularly those in residential use. The site plan (Exhibit 3 Sheet C2.1) illustrates how the internal pedestrian circulation plan, and the site in general, will connect to external circulation systems. This standard is met.

- (4) *On sites at major transit stops provide the following:*
- (a) *Either locate buildings within 20 feet of the transit stop, a transit street or an intersecting street, or provide a pedestrian plaza at the transit stop or a street intersection;*
 - (b) *A reasonably direct pedestrian connection between the transit stop and building entrances on the site;*
 - (c) *A transit passenger landing pad accessible to disabled persons;*
 - (d) *An easement or dedication for a passenger shelter, if requested by the transit provider; and*
 - (e) *Lighting at the transit stop.*

Response: The site is not located at a major transit stop. This standard does not apply.

8.390 Compatibility Details.

Attention to detail can significantly increase the compatibility of commercial development with adjacent uses. Commercial development shall be designed to comply with the following applicable details and any other details warranted by the local conditions:

- (1) *On-site lighting is arranged so that light is reflected away from adjoining properties and/or streets.*

Response: As depicted on Exhibit 3 Sheet E1.1PH, all overhead lighting will be directed downward to illuminate the parking areas and exterior of the building with the primary purpose of increasing safety for employees and visitors. Light pollution will be mitigated by utilizing appropriate shielding on light fixtures and planting trees along the borders of the parking lots. This standard is met.

- (2) *Any undesirable impacts produced on the site, such as noise, glare, odors, dust, or vibrations have been adequately screened from adjacent properties.*

Response: Noise, dust, and vibration impacts will be similar to those from the existing fire station, primarily originating from emergency vehicle trips to and from the facility, plus infrequent use of the emergency generator. Lighting glare is minimized through the use of shielded light fixtures as illustrated on the photometrics plan (Exhibit 3 Sheet E1.1PH). The facility is not anticipated to generate odors other than those from the trash enclosure, which is located over 13 feet from the property line and more than 15 feet from dwelling windows. The landscape plan (Exhibit 3 Sheets L4.1 and L4.2) includes a variety of parking lot trees and shrubs lining the exterior of the parking lot and site with the intention of segregating the neighbors from the any noise, glare, odors, and dust associated with the fire station. This standard is met.

- (3) *The site is protected from any undesirable impacts that are generated on abutting properties.*

Response: Based on the abutting commercial and residential uses, the likelihood of undesirable impacts from neighboring properties is limited. Nonetheless, the site has adequate landscaping and paved space

between the building and neighboring properties to prevent undesirable effects of adjacent uses. This standard is met.

(4) *Unightly exterior improvements and items such as trash receptacles, exterior vents and mechanical devices have been adequately screened.*

Response: As shown in the attached plans, trash receptacles will be stored in a trash enclosure northeast of the building, which is screened from neighboring properties by landscaping (see Exhibit 3 Sheet L4.1) and a chain-link fence with slats (Exhibit 2 page 3). Building mechanical equipment will be located on the roof behind the parapets to decrease visibility from Lyon Street, and the electrical transformer and emergency generator are located within the fenced and screened yard north of the building. This standard is met.

(5) *Storage areas, trash collection facilities and noise generating equipment are located away from public streets, abutting residential districts or development, or sight obscuring fencing has been provided.*

Response: The trash collection facility is located near the northeast corner of the building and is fully screened from view by a fenced enclosure and landscaping. The emergency generator has been sited north of the building as far from residences as possible to minimize noise impacts on neighboring properties. The generator will only operate during periods of power failure and during periodic test cycles (required to occur at least monthly for 30 minutes). This standard is met.

(6) *Where needed, loading facilities are provided on-site and are of sufficient size and number to adequately handle the delivery or shipping of goods or people. Where possible, loading areas should be designed so that vehicles enter and exit the site in a forward motion.*

Response: The proposed fire station does not necessitate the use of a designated loading facility, as any deliveries can be accommodated via the paved area near the apparatus bay's east entrance. A portion of the apparatus bay itself has been designed to allow fire trucks to pull through from east to west. This standard is met.

Supplemental Design Standards in Village Centers

8.405 Village Center Character.

The purpose of these standards is to contribute to the desired character of Albany's village centers. They are intended to promote the design of an urban environment that is built to human scale with attractive street fronts and interconnected walkways that promote pedestrian usage and accommodate vehicles. Development in the village center must contribute to a cohesive, visually compatible and functionally linked pattern through street and sidewalk layout, building siting and character, and site design. Details count.

Response: The design of the fire station has been selected to complement other structures in Albany's historic downtown. This standard is met.

8.410 Applicability.

These standards apply to commercial, office, mixed-use and institutional development within the Village Center Comprehensive Plan designation. They are in addition to the Commercial and Institutional Design Standards in this article. Taken together, these design standards are intended to foster a mixed-use character for village centers.

Response: The western portion of the site is within the Village Center Comprehensive Plan designation area. These standards apply and have been integrated into the proposed plan. This standard is met.

8.415 Buildings Along Public Streets.

Purpose. Buildings along the public street define the street edge and frames the streetscape. In larger development, the locations of pad site buildings also provide opportunities to frame entries into the shopping centers, and contribute to the visual interest of the site. These provisions are intended to avoid deep building setbacks behind large expanses of parking areas or vacant land.

Standards.

- (1) *Buildings and plazas shall be located within the maximum setback area for at least 40 percent of one public street frontage.*

Response: As outlined in Section 5.090, the LE zone has no maximum front setback. To ensure a pedestrian-friendly development, the front wall of the office portion of the fire station is located approximately 10 feet from the front property line, and the remainder of the building is approximately 26 feet from the front property line. The plaza extends clear out to the public sidewalk on Lyon Street. This standard is met.

- (2) *For sites with frontage on more than one public street (i.e., corner lots), this standard applies to one frontage only.*

Response: This site is not a corner lot due to the location of the Hasty Freez parcel at the southwest corner of the site. The building has been designed to meet these criteria for the Lyon Street frontage. This standard is met.

- (3) *The public street frontage is the length of the property as measured along the street right-of-way excluding the width of entrance driveways and/or streets.*

Response: The site has a public street frontage of 375 feet along Lyon Street. This standard is met.

- (4) *Building facades that face public streets shall be divided into human-scale proportions using at least two features such as windows, entrances, arcades, arbors, awnings, trellises with vines, or an equivalent element. A blank, uninterrupted wall shall not be longer than 30 feet.*

Response: As shown on Exhibit 3 Sheets A2.1 and A3.1, the building design incorporates significant windows, multiple glass overhead doors, and doorways. The uninterrupted wall at the north end of the Lyon Street elevation is approximately 26 feet long, which is less than the 30-foot maximum. This standard is met.

- (5) *No parking, loading or travel aisles shall be located between the public street and buildings within 50 feet of the street, except that a designated park-and-ride lot or one drive-through lane may be permitted. See Section 8.420(b).*

Response: No such aisle is proposed. This standard does not apply.

- (6) *To count toward this standard, a plaza shall:*

- (a) *Be well defined at the street edge by a low decorative architectural wall (no higher than three feet), or a line of shrubs or trees of the same species, or similar landscaped or built feature;*
- (b) *Use materials that are similar in quality to the principal materials of the primary building(s) and landscape. Landscaping with drought-resistant native species is strongly encouraged;*
- (c) *Have direct access to the public street sidewalk and be located the shortest distance to the nearest building main entrance; and*
- (d) *Extend at least the full depth of the maximum setback.*

Response: The proposed plan includes a plaza on the south side of the building between the fire station and the parking lot, but the plaza is not proposed to count toward the requirement of subsection 1. The plaza will be visually separated from the street by an enclosure of trees geometrically clustered in the southwest corner of the site, two rows of trees lining the south side of the building, and trees lining the parking area and islands. However, as the LE zone has no maximum setback, this standard does not apply.

8.420 Maximum Setback.

Purpose. Customer entrances should be readily accessible from the public street sidewalk as well as from the parking lot. Build-to lines form visually continuous, pedestrian-oriented street fronts with no vehicle use area between building fronts and the street.

Standards.

- (1) *In zoning districts with maximum setbacks, buildings within 50 feet of a public street shall have 40% of the building located within the maximum setback except that:*
 - (a) *If a previously recorded easement precludes meeting the maximum setback, the applicant shall demonstrate that an alternative layout best addresses the intent of this standard and the character of the village center.*
 - (b) *A building with drive-through service may have one drive-through lane between the building and the street provided that the building is set back no more than 25 feet and the drive-through lane is screened according to standards for perimeter parking area landscaping in ADC 8.470.*
- (2) *Any building more than 50 feet from a public street is exempt from this standard.*

Response: The primary building entrance is located approximately 10 feet from the front property line. The office portion of the building is located as close as practical to the front property line, while the apparatus bay is set back to accommodate emergency vehicles entering and existing. However, as the LE zone has no maximum setback, this standard does not apply.

8.430 Size Limitations.

See building size limitations in Articles 4 and 5, Table 1, Development Standards.

Response: These criteria have been addressed in the responses to Article 5. This standard is met.

8.440 Building Design.

Purpose. These provisions are intended to reduce the visual appearance of larger scale buildings to a smaller, pedestrian-level scale that is appropriate for a village center. They are not intended to limit the size of the building.

Standards.

- (1) *Building facades longer than 100 feet shall have relief such as recessed entries, offsets, jogs, bays, columns, ribs, pilasters, piers, cornices, bases, or other distinctive constructed changes. Changes in relief in the building façade shall occur at least every 100 feet for at least 20% of the exterior wall area. At least two colors or textures shall be used (not including stripes or bands).*
- (2) *Rooflines longer than 100 feet shall be relieved by elements such as parapets, gables, dormers, towers, steeples, etc.*
- (3) *No building wall shall be longer than 300 feet unless the building façade has one or more major offsets in wall plane. A major offset in wall plane shall have a depth of at least 10% of the length*

of the longest abutting wall and shall continue for at least 20% of the building facade. Minor changes in wall plane such as entries, jogs, bays, columns, ribs, pilasters, piers, or cornices do not count toward meeting this standard.

- (4) *In developments with multiple buildings, each individual building shall include predominant characteristics shared by all buildings in the development, so that the development forms a cohesive place within the district. A standardized prototype design shall be modified if necessary to meet the provisions of this Code and character of this district.*
- (5) *Corrugated metal siding is prohibited on any building. Corrugated metal roofing is allowed.*

Response: As shown on Exhibit 3 Sheets A2.1 and A3.1, the building has an overall length of 172 feet and incorporates significant windows, multiple glass overhead doors, doorways, and a decorative cornice on the parapet. The vestibule and lobby project out toward the Lyon Street sidewalk while the remainder of the front façade is offset in two different planes. The overhead doors serve to break up the significant massing of the 110-foot portion of the façade north of the lobby. The east face of the building is comprised of three materials including brick, glass, and fiber cement accent panels. This standard is met.

8.445 Pedestrian Network.

Purpose. By creating a safe, continuous network of sidewalks within and between developments, pedestrians will feel more inclined to walk (rather than drive). A pedestrian network that offers clear circulation corridors from the parking areas to building entries creates a friendlier, more inviting image. A detailed pedestrian circulation plan must demonstrate that the layout of sidewalks contributes to the overall pedestrian connectivity of the village center.

Standards.

- (1) *Sidewalks must be located to provide the shortest direct connection from the public street sidewalk(s) to all customer entrances.*

Response: The primary entrance on the southwest corner of the building will be only ten feet from the front property line. This is the shortest and most direct location for connection to the right-of-way. Other public entrances are located on the south side of the building within the plaza area, adjacent to the public parking lot. This standard is met.

- (2) *Sidewalks must be located to provide the shortest direct connection between all on-site customer entrances.*

Response: Both entrances are surrounded by hardscape plaza spaces that connect directly to public sidewalks. This standard is met.

- (3) *Sidewalks must be located along every public street frontage and both sides of on-site private streets. These sidewalks must be separated from the street by a tree-lined landscape strip. Approved vegetated post-construction stormwater quality facilities are allowed in the landscape strip.*

Response: Public sidewalks are located on Lyon Street and 7th Avenue. Street trees will be planted in accordance with applicable City standards. This standard is met.

- (4) *Extra-wide sidewalks are encouraged to provide space for tables and chairs and other pedestrian amenities, creating a concentration of activity to serve as the neighborhood center.*

Response: The proposed sidewalk along Lyon Street allows for significant pedestrian volume, and the proposed plaza will serve as a publicly-accessible focal point for the neighborhood. This standard is met.

- (5) *Sites larger than eight acres shall create an open space or plaza with amenities such as benches, monuments, kiosks or public art. Amenities shall be in prominent locations, interconnected with the uses and walkways on the site, and be landscaped. Approved vegetated post-construction stormwater quality facilities are allowed in these areas.*

Response: This site's size is 1.63 acres. This standard does not apply.

8.450 Privacy Considerations.

Purpose. Village centers are mixed-use areas where special attention is given to resolving potentially incompatible situations. General standards provide the flexibility to adjust the design and operating characteristics to given circumstances.

Standard.

- (1) *Non-residential uses and parking areas shall be arranged to minimize infringement on the privacy of adjoining residents.*

Response: As depicted on Exhibit 3 Sheet C2.1, the building and parking areas are located as far away from abutting residences north and east as practical. Sheets L4.1 and L4.2 illustrate the proposed plantings intended to maximize privacy of nearby properties. This standard is met.

8.460 Parking Areas.

Purpose. While recognizing the paramount role of cars in everyday life and the need to provide adequate and convenient space for them, these standards move away from the typical suburban pattern of predominant and highly-visible parking areas in commercial developments. They are intended to reduce the scale of parking areas by siting a portion of the parking lot out of view, and using increased landscaping to screen spaces and reduce the overall visual impact of large parking areas.

Standards.

- (1) *On-street parking spaces within 100 feet of a commercial or office development may count towards meeting the parking requirement.*
- (2) *Shared parking is encouraged for all uses.*
- (3) *Trees intended for parking area landscaping shall provide a canopy cover of at least 20% of the parking area at maturity. Existing trees may be included to meet the canopy requirement, provided the site plan identifies such trees and the trees meet the standards of size, health, and placement. The extent of canopy at maturity shall be based on published reference texts generally accepted by landscape architects, nurserymen, and arborists.*
- (4) *Vegetated post-construction stormwater quality facilities shall be considered as the initial stormwater collection system.*

Response: There are several on-street parking spaces proposed along Lyon Street for use by site visitors, in addition to the 47 on-site spaces. The parking lot trees illustrated on Exhibit 3 Sheets L4.1 and L4.2 have been selected to provide a mature canopy of 23% of the north parking area and 25% of the south parking area. The parking facility is designed to provide an inviting experience for both pedestrians and drivers alike. The public parking area has integrated trees, a plaza, landscaped islands, and other landscaping features to mitigate the effects of an open paved lot. Multiple vegetated planters will provide stormwater quality for parking lot runoff (see Exhibit 3 Sheet C2.2). This standard is met.

8.470 Perimeter Parking Area Landscaping.

Purpose. These provisions are intended to give parking a low profile in order to improve the pedestrian experience and the overall aesthetic quality of the street. They will minimize the expansive appearance of parking lots, increase the sense of neighborhood scale, and improve the character of a village center. They will also create an attractive, shaded environment along streets that gives visual relief from continuous hard street edges; buffer automobile traffic, and focus views for both pedestrians and motorists.

Standards.

- (1) All parking areas (excluding entranceways) adjacent to a public street shall be screened with:
 - (a) A low continuous hedge of evergreen shrubs, trees and plantings that are at least 3 feet tall within 2 years and grow to provide an evergreen screen of at least 70%; OR
 - (b) A berm 3 feet tall with a maximum slope of 3:1, in combination with coniferous and deciduous trees and shrubs; OR
 - (c) A low decorative masonry wall at least 3 feet tall in combination with landscaping; OR
 - (d) A combination of any of these methods.
- (2) The landscape plan shall be prepared by a licensed landscape architect.

Response: The site has been laid out to ensure that no parking areas are adjacent to a public street; all parking lots are more than 20 feet from the right-of-way line. Exhibit 3 Sheets L4.1 and L4.2 depict the parking lot perimeter landscaping. The parking areas are surrounded by evergreen shrubs with larger parking lot trees just to the interior. This standard is met.

8.475 Signs.

Purpose. Signs must be scaled appropriately to appeal to both pedestrians walking on the adjacent sidewalks and to nearby motorists. The following standards are intended to create aesthetically pleasing and cohesive sign standards while reinforcing the context of the village center.

Standards.

- (1) For integrated centers, an overall sign and graphics program shall be provided as part of the development application to ensure that stand-alone signs are consolidated and that signs complement the character of the neighborhood.
- (2) Monument signs are preferred rather than freestanding signs.

Response: Any proposed on-site signage will be designed in accordance with all applicable City requirements. Exhibit 3 Sheet L2.2 illustrates the proposed location of a concrete sign wall next to plaza. This standard is met.

Article 9 – On-Site Development and Environmental Standards**Off-Street Parking****9.020 Space Requirements.**

Off-street parking and loading must be provided for all development in the amounts indicated in the table below subject to any applicable reductions permitted in this Article. All required parking must be developed in accordance with the standards in this Article.

- (1) *Calculating Floor Area for Parking.* The area measured is the combined floor area of each level of a building exclusive of vent shafts, court yards, stairwells, elevator shafts, restrooms, storage rooms and rooms designed and used for the purpose of storage and operation of maintenance equipment, and covered or enclosed parking areas.
- (2) *Employees.* The number of employees shall include those working on the premises, plus proprietors, during the largest shift at peak season.
- (3) *Fractional Space Requirements* shall be counted to the nearest whole space; half spaces will be rounded up.
- (4) *Unspecified Uses and Alternative Standards.* When a use is not specifically listed in the Table 9-1: Parking Requirements, the Director will determine if the use is similar to a listed use in terms of parking needs. When a use is not similar to a use listed in Table 9-1 or the applicant has documentation that demonstrates a different parking demand, the Director may approve alternative parking standards. Acceptable documentation may include parking standards from other cities of similar size, company data on parking demand, parking demand studies, or the ITE Parking Generation Manual.

Response: Table 9-1 (Parking Requirements) does not identify the minimum number of spaces required for fire stations. No similar uses are listed. Therefore, the Planning Director may approve alternative parking standards. The applicant proposes to utilize information from the existing Fire Station 11 (company data on parking demand) to determine the required parking standard. The existing facility is approximately 14,500 square feet and has 16 on-site parking spaces, or approximately one space per 900 square feet. However, the existing facility provides inadequate parking, so the applicant proposes a parking standard requiring one space per 600 square feet (an increase of 50% over the existing amount). Based on the proposed building size of 24,265 square feet, this would equate to a minimum of 41 spaces. As illustrated on Exhibit 3 Sheet C2.1, the proposed site plan would provide 28 parking spaces for public/visitor use and 19 secure spaces for fire department staff, totaling 47 spaces, which exceeds the proposed requirement. Additionally, the proposed on-street spaces on Lyon Street could accommodate approximately 9 vehicles. This standard is met.

9.025 Parking in the Public Right-of-Way.

Parking spaces in a public right-of-way may not be counted as fulfilling any part of the parking requirements except when permitted below. Any parallel parking spaces in the right-of-way that are counted toward fulfilling the parking requirements must be at least 25 feet long.

- (4) *Mixed Use Zones.* On street parking spaces abutting the property in the MUR, Mixed Use Residential District; CB, Central Business District; and LE, Lyon Ellsworth District, may be counted towards meeting parking requirements.
- (5) *Non-Residential Development.* The amount of off-street parking required may be reduced by one space for every on-street space abutting the development for up to 25 percent of the minimum parking requirement, except when the development is proposed in a residential zoning district in Article 3 or in the ES (Elm Street) zone.
 - (a) *The on-street parking spaces must be at least 100 feet from a residential zoning district or the ES zoning district.*
 - (b) *On-street parking credits can only be granted for developments with frontage on streets that allow parking on both sides and with approval from the Director of Public Works.*

Response: The site is located within the LE zone so on-street parking may count toward the minimum parking standard. The proposed on-street spaces on Lyon Street illustrated on Exhibit 3 Sheet C2.1 could accommodate approximately 9 vehicles. This standard is met.

9.120 Parking Area Improvement Standards.

All public or private parking areas, loading areas and outdoor vehicle sales areas must be improved based on the following standards:

- (1) General. All parking spaces must be improved in accordance with these standards and available for use at the time of project completion.*
- (2) Other Requirements. All parking areas shall conform to the setback, clear vision, landscaping, and buffering/screening provisions of this Code.*

Response: The proposed parking lot has been designed in compliance with applicable provisions of the Albany Development Code and shall be completed prior to the opening of the new fire station. This standard is met.

- (3) Surfacing. All required parking, including travel aisles and access, shall have a durable, dust-free surface of asphalt, cement concrete, or other materials approved by the Director. Parking lot surfacing shall not encroach upon the public right-of-way except when it abuts a concrete public sidewalk, or has been otherwise approved by the Director of Public Works. Pervious pavements, such as pervious asphalt or pervious concrete, may be allowed by the Director of Public Works.*

Response: The proposed parking lot and aisles are proposed to be concrete or asphalt in accordance with these requirements. This standard is met.

- (4) Drainage. All parking lots must provide a drainage system to dispose of the runoff generated by the impervious surface. Post-construction stormwater quality facilities are required per Title 12 of the Albany Municipal Code when applicable. Provisions shall be made for the on-site collection of drainage water to eliminate sheet flow of such water onto sidewalks, public rights-of-way, and abutting private property. All drainage systems must be approved by the Director of Public Works.*

Response: As illustrated in Exhibit 3 Sheets C2.1 and C2.2, the parking lot has been designed to convey stormwater to vegetated stormwater planters prior to discharge to the municipal storm drain system. This standard is met.

- (5) Perimeter Curb. Perimeter curbing is required for protection of landscaped areas and pedestrian walkways, and to prevent runoff onto adjacent properties. All parking areas except those required in conjunction with a single- or two-family dwelling or approved overflow parking areas shall provide a curb at least 6 inches high along the perimeter of all parking areas. Exceptions may be allowed for connections to approved vegetated post-construction stormwater quality facilities.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the parking lot is encircled by perimeter curbing, with the exception of areas discharging to stormwater planters and those parking areas which are integrated with the plaza area. This standard is met.

- (6) Wheel Bumper. All parking stalls fronting a sidewalk, alleyway, street or property line, except for those required in conjunction with a single- or two-family dwelling, shall provide a secured wheel bumper at least 6 inches high and at least 6 feet long, set back from the front of the stall at least 2-1/2 feet, but no more than 3 feet. If the sidewalk is widened to 7 feet 6 inches to allow for vehicle encroachment, no wheel bumpers are required.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the parking spaces abutting the sidewalk to 7th Avenue will have secured wheel bumpers. This standard is met.

- (7) *Turnaround. Except for single-family and duplex dwellings, groups of more than two parking spaces must be located and served by an aisle or turnaround so that their use will require no backing movements or other maneuvering in a street right-of-way other than an alley.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the proposed parking lot design allows for vehicles to back up and reverse direction on-site without requiring backing movements within a public street. This standard is met.

- (8) *Striping. Lots containing more than two parking spaces must have all required spaces permanently and clearly striped. Stripes must be at least four inches wide. When motorcycle parking, compact, or handicapped parking spaces are provided, they shall be designated within the stall.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the proposed parking spaces are separated by a 4-inch-wide stripe. Handicapped spaces are appropriately denoted with painted and signed designations. This standard is met.

- (9) *Connecting to Adjacent Parking Areas. Where an existing or proposed parking area is adjacent to a developed or undeveloped site within the same zoning district, any modifications to the parking areas must be designed to connect to the existing or future adjacent parking area. This requirement may be waived by the Director when it is deemed impractical or inappropriate due to the nature of the adjoining uses.*

Response: The Fire Department investigated opportunities to interconnect the site with the adjoining Hasty Freez property in order to minimize the number of driveways on the abutting streets. However, due to the orientation and circulation pattern of the existing Hasty Freez business, it was not possible to connect the sites. Due to the location, orientation, and usage of existing developed neighboring properties it was deemed impractical to directly connect the on-site pedestrian paths with neighboring properties, particularly those in residential use. This standard does not apply.

- (10) *Parking Lot Landscaping. Parking lots shall be landscaped according to the standards in Section 9.150.*

Response: Compliance with this requirement is outlined in the response to Section 9.150. This standard is met.

- (11) *Compact Car Parking. No more than 40% of parking spaces provided may be designated for compact cars. Compact spaces must be signed and/or the space painted with the words "Compact Car Only."*

Response: No compact spaces are proposed as part of this development. This standard does not apply.

- (12) *Parking Accessible to the Disabled. All parking areas must provide accessible parking spaces in conformance with the Oregon Structural Specialty Code.*

Response: For parking lots with 26 to 50 spaces, the Oregon Structural Specialty Code requires two accessible spaces, one of which must be van accessible. As illustrated on Exhibit 3 Sheet C2.1, one standard accessible space and one van-accessible space is proposed. This standard is met.

- (13) *Bicycle Parking. Bicycle parking space requirements are as follows:*

- (a) *For multiple-family dwellings (three or more units) – one space per four units.*
- (b) *For industrial development – one space for every 10 automobile spaces required.*
- (c) *For commercial or office development - at least two spaces, plus one space for every 10 automobile spaces required.*

- (d) *Exemptions -- the Director may allow exemptions to or reductions in required bicycle spaces in connection with temporary uses or uses that are not likely to need bicycle parking.*

Bicycle parking spaces shall meet the following standards:

- (e) *Required spaces should be visible and not hidden, and must be located as near as possible to building entrances used by automobile occupants.*
- (f) *Each required bicycle parking space must have a parking rack securely fastened to the ground. Parking racks must support each bicycle at a minimum of two points, including at least one point on the frame, and must allow the frame and at least one wheel to be locked with a U-type lock.*
- (g) *Bicycle parking areas must provide at least 3 feet of clearance around all 3 sides of a fully-loaded bicycle rack and have an overhead clearance of at least 7 feet.*
- (h) *At least one-half of required bicycle parking spaces must be sheltered. Spaces must be protected from precipitation by a roof overhang or a separate roof at least 7 feet tall. Bicycle parking spaces within roofed buildings and bike lockers are considered sheltered spaces.*

Response: The closest category for this use is commercial or office development, so this project would need to provide at least two bicycle parking spaces, plus one space for every 10 required vehicle spaces. Based on the required 41 parking spaces, a total of 7 bicycle spaces are required. As illustrated on Exhibit 3 Sheets C2.1 and L2.2, parking for 12 bicycles has been provided south of the building near the plaza. The racks are proposed to be located so that the canopy provides weather protection to four of the racks (8 spaces). This standard is met.

- (14) *Lighting. Any lights provided to illuminate any public or private parking area or vehicle sales area must be arranged to reflect the light away from any abutting or adjacent properties.*

Response: As shown in Exhibit 3 Sheet E1.1PH, parking lot lighting fixtures will be located to concentrate lighting around the parking areas and shielding will be provided as appropriate. This standard is met.

- (15) *Pedestrian Access. Walkways and accessways shall be provided in all new off-street parking lots and additions to connect sidewalks adjacent to new development to the entrances of new buildings. All new public walkways and handicapped accessible parking spaces must meet the minimum requirements of the Oregon Structural Specialty Code.*

Response: Sidewalks will fully extend along the west and south sides of the property lines abutting Lyon Street and 7th Avenue. The western sidewalk will connect directly to the entry plaza from Lyon Street. The parking area will have a sidewalk connecting 7th Avenue to the entry plaza by way of the western border of the parking area, along the eastern boundary of the Hasty Freeze site. This standard is met.

- (16) *When employee parking is designated in new developments, parking for carpools and vanpools shall be provided and located near the employee entrances to buildings.*

Response: As shown in Exhibit 3 Sheet C2.1, carpool parking will be located east of the building near the employee entrance. This standard is met.

9.130 Off-Street Parking Lot Design.

All off-street parking lots must be designed in accordance with City standards for stalls and aisles as set forth in Table 9-2: Parking Lot Design and supplemental drawings. Stall dimensions are measured from inside the stripes.

- (1) Compact spaces shall be at least 8 feet wide by 16 feet long.
- (2) Accessible spaces shall be a minimum of 9 feet wide and 17 feet long and designed in accordance with the Oregon Structural Specialty Code (OSSC). An adjacent access aisle must be provided that is at least 8 feet wide and 17 feet long for a van-accessible space, and 6 feet wide for a standard accessible space.
- (3) Stall Width. Long-term parking spaces must be at least 8.5 feet wide. Parking stalls for grocery stores or adjacent to planter islands must be at least 9.5 feet wide.
- (4) Minimum Aisle Widths. Aisles for two-way traffic and emergency vehicle operations must be at least 24 feet wide. One-way aisles and one-way emergency vehicle access must be at least 20 feet wide.
- (5) The design of driveways and on-site maneuvering and loading areas for commercial and industrial developments shall include 20 feet of storage length for entering and exiting vehicles, in order to prevent vehicles from backing into the flow of traffic on the public street or causing unsafe conflicts with on-site circulation.

TABLE 9-2: PARKING LOT DESIGN (IN FEET)

A Parking Angle	B Stall Width	C Curb Width	D Aisle Width	E Stall Depth	F Bumper Overhang	G Dead-end Back-up
(Parallel)	8.0 feet	8.0 feet	N/A	25.0 feet	N/A	N/a
45	8.5	12.0	13.0	17.5	2.0	5.0
	9.0	12.7	12.0	17.5	2.0	5.0
	9.5	13.4	11.0	17.5	2.0	5.0
	10.0	14.1	11.0	17.5	2.0	5.0
60	8.5	9.8	18.0	19.0	2.5	5.0
	9.0	10.4	16.0	19.0	2.5	5.0
	9.5	11.0	15.0	19.0	2.5	5.0
	10.0	11.6	14.0	19.0	2.5	5.0
Compact	8.0 C	8.0 C	26.0 C	16.0 C	3.0	5.0
90	8.5	8.5	26.0	18.5	3.0	5.0
	9.0	9.0	26.0	18.5	3.0	5.0
	9.5	9.5	26.0	18.5	3.0	5.0
	10.0	10.0	24.0	18.5	3.0	5.0

Response: As illustrated on Exhibit 3 Sheet C2.1, the proposed parking lots will not have any compact spaces and will provide accessible spaces exceeding the minimum dimensions specified in the Oregon Structural Specialty Code. All parking spaces in the public parking lot are proposed to have a width of 9 feet, while those within the secure parking area have a width of 10 feet. All aisle widths are a minimum of 24 feet, and a 20-foot storage area is provided for the driveway to 7th Avenue. All parking spaces have a minimum depth of 18.5 feet, with the exception of the secure parking area, in which some spaces have a depth of 17.5 feet due to the provided bumper overhang. The spaces with a width of 10 feet have a 24-foot aisle while those with narrower spaces have a 26-foot aisle. A dead-end backup of 5 feet is provided in the secure parking area. This standard is met.

Landscaping

9.140 General Requirements.

Landscaping requirements by type of use are listed below:

- (6) *Landscaping Required – Residential. All front setbacks (exclusive of accessways and other permitted intrusions) are required to be landscaped before an occupancy permit will be issued or final building permit approved. In all residential districts except Rural Residential (RR), the minimum landscaping acceptable for every 50 lineal feet of street frontage (or portion thereof, deducting the width of the driveway) is:*
- (a) *One tree at least 6 feet tall.*
 - (b) *Four 1-gallon shrubs or accent plants.*
 - (c) *The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, and evergreen shrubs).*

Response: No residential uses are proposed. This standard does not apply.

- (7) *Landscaping Required – Non-Residential. All required front and interior setbacks, exclusive of accessways and other permitted intrusions, must be landscaped before an occupancy permit will be issued. Minimum landscaping acceptable for every 1,000 square feet of required setbacks in all commercial industrial districts is as follows:*
- (a) *One tree at least 6 feet tall for every 30 feet of street frontage.*
 - (b) *Five 5-gallon or eight 1-gallon shrubs, trees or accent plants.*
 - (c) *The remaining area treated with suitable living ground cover, lawn, or decorative treatment of bark, rock, or other attractive ground cover.*
 - (d) *When the yard adjacent to a street of an industrially zoned property is across a right-of-way from other industrially or commercially zoned property, only 30 percent of such setback area must be landscaped.*

Response: Exhibit 3 Sheets L4.1 and L4.2 depict the proposed landscaping, which consists of street trees and on-site trees abutting the Lyon Street and 7th Avenue frontages, together with multiple shrubs and ground cover plantings. However, as noted in the response to Section 5.090, the LE zone has no required front or interior setbacks, so this standard does not apply.

- (3) *Alternate Plan – Non-Residential - The Director may approve placement of the required setback landscaping in public right-of-way when the following conditions are met:*
- (a) *The site contains existing development that includes substantial building(s), and is subject to ADC improvement requirements due to a change of use or vacancy; and*
 - (b) *The appropriate government agency grants written permission for use of the right-of-way; and*
 - (c) *The applicant provides written assurance that on-site setback landscaping will be installed within 90 days in the event permission to use the right-of-way is revoked; and*
 - (d) *The Director finds that the required setback landscaping can feasibly be installed on the property without creating other violations of this Code; and*
 - (e) *The Director finds that providing the landscaping in the public right-of-way in the interim fulfills the intent this Code for high quality development (9.010) and the minimum landscaping requirements in Section 9.140(2)(a) through (c).*

Response: The applicant is not requesting that landscaping in the right-of-way satisfy the Development Code requirements for setback plantings. This standard does not apply.

9.150 Parking Lot Landscaping.

The purpose of landscaping in parking lots is to provide shade, reduce stormwater runoff, and direct traffic. Incorporation of approved vegetated post-construction stormwater quality facilities in landscaped areas is encouraged. Parking lots must be landscaped in accordance with the following minimum standards:

- (1) *Planter Bays. Parking areas shall be divided into bays of not more than 12 parking spaces. At both ends of each parking bay there shall be curbed planters at least 5 feet wide, excluding the curb. Gaps in the curb may be allowed for connections to approved post-construction stormwater quality facilities. Each planter shall contain one canopy tree at least 10 feet high and decorative ground cover containing at least two shrubs for every 100 square feet of landscape area. Neither planter bays nor their contents may impede access on required public sidewalks or paths, or handicapped-accessible parking spaces.*

Response: As illustrated in Exhibit 3 Sheets C2.1, L4.1, and L4.2, no more than eight contiguous parking spaces comprise each bay. The proposed parking lot planters have widths of at least seven feet (excluding curbs), exceeding the five-foot minimum standard. Each planter bay would have one canopy tree and at least six shrubs, which exceeds the requirement for two shrubs per 100 square feet. This standard is met.

- (2) *Entryway Landscaping. Both sides of a parking lot entrance shall be bordered by a minimum 5-foot-wide landscape planter strip meeting the same landscaping provisions as planter bays, except that no sight-obscuring trees or shrubs are permitted.*

Response: As illustrated in Exhibit 3 Sheets C2.1, L4.1, and L4.2, landscape planters are proposed next to the parking lot entrances with plantings that provide canopy trees and multiple low-growing shrubs, well in excess of the requirement for two shrubs per 100 square feet. This standard is met.

- (3) *Parking Space Buffers. Parking areas shall be separated from the exterior wall of a structure by pedestrian walkways or loading areas or by a 5-foot strip of landscaping materials.*

Response: As illustrated in Exhibit 3 Sheet C2.1, the parking areas are separated from the building by at least five feet, with the exception of the north wall, which has a 4.5-foot separation. However, at this location, the curb stops are placed within the parking stalls to provide sufficient space for bumper overhang that vehicles do not interfere with the building. This standard is met.

- (4) *Alternate Plan. An alternate plan may be submitted that provides landscaping of at least five percent of the total parking area exclusive of required landscaped yard areas and that separates parking areas of more than 100 spaces into clusters divided by landscape strips. Each planter area shall contain 1 tree at least 10 feet tall and decorative ground cover containing at least 2 shrubs for every 100 square feet of landscape area. Landscaping may not impede access on required public sidewalks or paths, or handicapped-accessible parking spaces.*

Response: No alternate plan is proposed. This standard does not apply.

- (5) *Landscape Protection. Required landscaped areas adjacent to graveled areas must be protected, either by railroad ties secured by rebar driven 18 inches into the ground, by large boulders, or by another acceptable means of protection.*

Response: As illustrated in Exhibit 3 Sheets L2.1 and L2.2, the only area requiring such protection is the gravel mulch at the picnic area east of the building, which would be separated from the landscape by a border strip. This standard does not apply.

9.160 Irrigation of Required Landscaping.

All required landscaped areas must be provided with an irrigation system unless a licensed landscape architect, landscape construction professional or certified nurseryman submits written verification that the proposed plants do not require irrigation. Irrigation systems installed in the public right-of-way require an encroachment permit.

Response: Exhibit 3 Sheet L3.1 illustrates the irrigation requirements for all of the landscaped areas. The landscaped area and the stormwater treatment areas will be irrigated using rotary spray nozzles. Final design of the irrigation system would be performed at the time of permitting. This standard is met.

9.170 Identification of Existing Trees.

In all proposed developments, existing trees over 25 inches in circumference (8 inches in diameter) as measured 4.5 feet above mean ground level from the base of the trunk shall be noted on all development plans, with notations indicating whether they are to be removed or utilized in the development. To obtain the circumference of a tree with multiple trunks, add the individual trunk circumferences, which are greater than 6 inches in circumference. Clusters of trees in open space and floodplain areas may be noted in approximate locations.

Response: The required information is contained on Exhibit 3 Sheet C2.0. This standard is met.

Tree Protection**9.207 Applicability.**

Site Plan Review approval is required for the felling of 5 or more trees larger than 25 inches in circumference (approximately 8 inches in diameter) on a lot or property in contiguous single ownership in excess of 20,000 square feet in any zone.

The following activities are exempt from site plan review if they meet the applicable requirements of the Significant Natural Resource overlay districts in Article 6:

- (1) *The action of any City official or of any public utility necessary to remove or alleviate an immediate danger to life or property; to restore utility service or to reopen a public street to traffic.*
- (2) *Felling of any tree that is defined as a nuisance under the Albany Municipal Code.*
- (3) *Any felling necessary to maintain streets or public or private utilities within a public right-of-way or utility easement provided the Tree Commission or City Forester approved the proposed tree felling.*
- (4) *Felling of trees planted as Christmas trees.*
- (5) *Felling of trees on property under a Forest Stewardship Plan approved by the Oregon Department of Forestry.*

Response: As illustrated on Exhibit 3 Sheet C2.0, the proposed development would result in the removal of 11 trees on site in excess of 8-inch diameter. The proposed tree felling does not meet the exemptions outlined above. Consequently, the proposal is subject to review under Section 9.208. This standard is met.

9.208 Tree Felling Criteria.

The following review criteria replace the Site Plan Review criteria found elsewhere in this code for the purpose of reviewing tree felling. A Site Plan Review for tree felling will be processed as a Type I-L land use decision.

- (1) *The Community Development Director or his/her designee shall approve a Site Plan Review for tree felling when the applicant demonstrates that the felling of the tree(s) is warranted because of the condition of the tree(s) with respect to disease, hazardous or unsafe conditions, danger of falling, proximity to existing structures or proposed construction, or interference with utility services or pedestrian or vehicular safety. The Director, in consultation with the City Arborist, may also grant an exception to any of the tree cutting standards for industrial development on industrially zoned land. The Director may require the applicant to provide a Certified Arborist's report.*
- (2) *For property where a site plan review, conditional use or land division application has been approved or is currently under review, the Community Development Director, City Forester, or*

his/her designee shall approve site plan review when the applicant demonstrates that all of the following review criteria are met:

- (a) It is necessary to fell tree(s) in order to construct proposed improvements in accordance with an approved site plan review or conditional use review, or to otherwise utilize the applicant's property in a manner consistent with its zoning, this code, applicable plans adopted by the City Council, or a logging permit issued by the Oregon Department of Forestry.
- (b) The proposed felling is consistent with State standards, City ordinances, and the proposed felling does not negatively impact the environmental quality of the area, including but not limited to: the protection of nearby trees and windbreaks; wildlife; erosion; soil retention and stability; volume of surface runoff and water quality of streams; scenic quality, and geological sites.
- (c) The uniqueness, size, maturity, structure, and historic value of the trees have been considered and all other options for tree preservation have been exhausted. The Director may require that trees determined to be unique in species, size, maturity, structure, or historic values are preserved.
- (d) Tree felling in Significant Natural Resource Overlay Districts meets the applicable requirements in Article 6.

Response: As illustrated on Exhibit 3 Sheets C2.0 and C2.1, removal of multiple trees is required to construct the proposed fire station, parking areas, stormwater management facilities, and required landscaping. Due to the site's location within the heart of the City, it is not anticipated that tree removal would negatively impact environmental quality. The applicant has utilized an arborist to identify whether specific trees merited saving. The site is not within a Significant Natural Resource Overlay District. This standard is met.

- (4) The Director may attach conditions to the approval of the tree felling permit to ensure the replacement of trees and landscape or otherwise reduce the effects of the felling, and may require an improvement assurance to ensure all conditions are met.
- (5) Precautions shall be made to protect residual trees and tree roots from damaging agents during and after the removal process. The following tree protection specifications should be followed to the maximum extent feasible for all projects with protected existing trees.
 - (a) Within the drip line of any protected existing tree, there shall be no cut or fill over a four-inch depth unless a qualified arborist or forester has evaluated and approved the disturbance.
 - (b) Prior to and during construction, an orange fence shall be erected around all protected existing trees that is a minimum of 4 feet tall, secured with metal T-posts, no closer than 6 feet from the trunk or within the drip line, whichever is greater. There shall be no storage or movement of equipment, material, debris or fill within the fenced tree protection zone.
 - (c) During the construction stage of development, the applicant shall prevent the cleaning of equipment or material or the storage and disposal of waste material such as paints, oils, solvents, asphalt, concrete, motor oil or any other material harmful to the life of a tree within the drip line of any protected tree or group of trees.
 - (d) No damaging attachment, wires, signs or permits may be fastened to any protected tree.
 - (e) Large property areas containing protected trees and separated from construction or land clearing areas, road rights-of-way and utility easements may be "ribboned off," rather than erecting protective fencing around each tree as required in subsection (5)(b) above. This may be accomplished by placing metal t-post stakes a maximum of 50 feet apart and tying ribbon or rope from stake-to-stake along the outside perimeters of such areas being cleared.

- (f) *The installation of utilities, irrigation lines or any underground fixture requiring excavation deeper than 6 inches shall be accomplished by boring under the root system of protected existing trees at a minimum depth of 24 inches. The auger distance is established from the face of the tree (outer bark) and is scaled from tree diameter at breast height as described in Table 9-3 below.*

Tree Diameter at Breast Height (inches)	Auger Distance from Face of Tree (feet)
8-9	5
10-14	10
15-19	12
Over 19	15

Response: The proposed plantings illustrated on Exhibit 3 Sheets L4.1 and L4.2 more than compensate for the trees proposed for removal. As all trees are proposed to be removed, there is no need for protection measures such as those outlined above. This standard is met.

Buffering and Screening

9.210 General Requirements/Matrix.

In order to reduce the impacts on adjacent uses of a different type, buffering and screening is required in accordance with the matrix that follows Section 9.300. The property owner of each proposed development is responsible for the installation and maintenance of such buffers and screens. The Director may waive the buffering/screening requirements of this section where such has been provided on the adjoining property in conformance with this Code. Where a use would be abutting another use except for separation by right-of-way, buffering (but not screening) shall be required as specified in the matrix. Where a proposed use abuts undeveloped property, only one half of the buffer width shall be required.

Abutting Use or District	Proposed Use Commercial/professional or mixed-use
Dwellings in HM	10' S
Any arterial street	10'
Commercial or professional uses, or commercial and mixed use districts	0'
Any parking lot with at least 5 spaces	0'

"S" indicates screening required.

The buffer/screening standard does not apply along arterial streets where it conflicts with other provisions of this code.

Response: Albany Development Code Table 9-4 (Buffer and Screening Matrix) dictates that a 10-foot buffer with screening is required when abutting dwellings. This is applicable on the eastern and northern portions of the site that abut houses in the HM zone. A 10-foot buffer (but no screening) is required on Lyon Street since it is an arterial street and on the portion of 7th Avenue within the HM zone. No other buffering or screening is required in the LE zone. There is currently an encroachment onto City-owned property at the southeast corner of the site from the neighboring property's garage, but the applicant proposes to provide the full ten-foot buffer and screen on the City property in accordance with the provisions above. The standards of this section are addressed below.

9.240 Buffering.

The minimum improvements within a buffer area consist of the following:

- (1) *At least one row of trees. These trees will be not less than 10 feet high at time of planting for deciduous trees and spaced not more than 30 feet apart and 5 feet high at time of planting for evergreen trees and spaced not more than 15 feet apart. This requirement may be waived by the Director when it can be demonstrated that such trees would conflict with other purposes of this Code (e.g. solar access).*
- (2) *At least five 5-gallon shrubs or ten 1-gallon shrubs for each 1,000 square feet of required buffer area.*
- (3) *The remaining area treated with attractive ground cover (e.g., lawn, bark, rock, ivy, evergreen shrubs).*

Response: As illustrated in Exhibit 3 Sheets L4.1 and L4.2, landscaping meeting the buffering requirements is provided along the eastern site boundary adjacent to residences within the HM zone, consisting of a row of trees closer than 30 feet apart, shrubs, and groundcover (some of which will be within stormwater facilities). Similarly, trees, shrubs, and groundcover are proposed along Lyon Street. This standard is met.

9.250 Screening.

Where screening is required or provided, the following standards apply in addition to conditions (1) and (3) above:

- (1) *One row of evergreen shrubs that will grow to form a continuous hedge at least 4 feet tall within two years of planting, or*
- (2) *A fence or masonry wall at least 5 feet tall constructed to provide a uniform sight-obscuring screen, or*
- (3) *An earth berm combined with evergreen plantings or a fence that forms a sight and noise buffer at least 6 feet tall within two years of installation.*

Response: As illustrated in Exhibit 3 Sheets L2.1 and L2.2, a chain-link fence with slats is proposed to meet the screening requirement along the eastern site boundary adjacent to residences within the HM zone. This standard is met.

9.260 Clear Vision.

Buffering and screening provisions are superseded by the clear vision requirements of Section 12.180 and by the fence and wall height restrictions of the zone when applicable.

Response: Exhibit 3 Sheets L4.1 and L4.2, illustrate the required clear vision areas at the driveway entrances and the landscaping has been selected to comply with the applicable standards. This standard is met.

Fences*9.380 Standards.*

Fences and walls shall meet the following standards. If a fence or wall is used to meet required screening, it shall meet the provisions in Section 9.385.

Standards in Commercial, Industrial, HD, CB, ES, LE, MS, PB, WF, and TD zones:

- (4) *Fences in front setbacks. Fences shall be no taller than 6 feet in required front setbacks. 6-foot fences containing barbed wire on top or fences taller than 6 feet are not permitted in the front setback.*

Response: As illustrated on Exhibit 3 Sheets L2.1 and L2.2, a chain-link fence with slats is proposed along the eastern and northern site boundaries and north of the fire station. However, as noted in the response to Section 5.090, the LE zone has no required front setback, so this standard does not apply.

Article 11 – Land Divisions and Planned DevelopmentsSubdivisions and Partitions*11.180 Tentative Plat Review Criteria.*

Approval of a tentative subdivision or partition plat will be granted if the review body finds that the applicant has met all of the following criteria which apply to the development:

- (1) *Development of any remainder of property under the same ownership can be accomplished in accordance with this Code.*

Response: As illustrated on Exhibit 3 Sheet C2.4, the replat would combine six parcels, plus the vacated portion of 6th Avenue, into one parcel under the same ownership. The proposed development would utilize the entire site. This standard is met.

- (2) *Adjoining land can be developed or is provided access that will allow its development in accordance with this Code.*

Response: The adjoining land is currently developed and has access to existing rights-of-way so the proposed vacation area is unnecessary for development of adjoining lots. Combining multiple parcels into one lot allows for construction of the proposed fire station. This standard is met.

- (3) *The proposed street plan affords the best economic, safe, and efficient circulation of traffic possible under the circumstances.*

Response: No new streets are proposed, and the street grid system provides adequate circulation. Combining multiple parcels into one lot allows for construction of the proposed fire station. This standard is met.

- (4) *The location and design allows development to be conveniently served by various public utilities.*

Response: As shown in the attached plans, the subject site is already and will continue to be served by public utilities. This standard is met.

- (5) *Activities and developments within special purpose districts must comply with the regulations described in Articles 4 (Airport Approach), 6 (Natural Resources), and 7 (Historic), as applicable.*

Response: The subject site is not in a special purpose district. This standard does not apply.

Article 12 – Public ImprovementsStreets*12.060 General Provisions.*

No development may occur unless it has frontage on or approved access to a public street currently open to traffic. A currently non-open public right-of-way may be opened by improving it to City standards.

Response: The subject site has frontage on Lyon Street and 7th Avenue. This standard is met.

Streets shall be connected to reduce travel distance, provide multiple travel routes, and promote the use of alternative modes. Street patterns have a greater long-range effect on land use patterns, than do parcel patterns or building location.

Response: The site is located on Lyon Street and 7th Avenue and abuts existing developed parcels. The street network is already set at this location. This standard does not apply.

Streets (including alleys) within and adjacent to a development shall be improved in accordance with the standards in this Article. In addition, any new street or additional street width planned as a portion of an approved street plan shall be dedicated and improved in accordance with this Article.

Response: This proposal will include improvement of the abutting streets as required by the City and the Oregon Department of Transportation. This standard is met.

When the City Engineer determines that a required street improvement would not be timely, the City Engineer may accept a Petition for Improvement/Waiver of Remonstrance for a future assessment district.

Response: No remonstrance is requested. This standard does not apply.

The City Engineer may approve adjustments to the required street right-of-way and planter widths when necessary to accommodate approved street-side post-construction stormwater quality facilities.

Response: No adjustment is requested. This standard does not apply.

12.070 Creation of Streets.

Streets are usually created by approval of a subdivision or partition plat. However, the City Council may also approve creation of a street by acceptance of a deed. If creating a street unintentionally results in a land partition, the owner is not required to apply for partition approval as long as the resulting parcels comply with Code standards.

Response: No streets are proposed as part of this application. This standard does not apply.

12.100 Access to Public Streets.

With the exceptions noted in Section 1.070, the location and improvement of an access point onto a public street shall be included in the review of a development proposal. In addition, the following specific requirements shall apply to all access points, curb cuts, and driveways:

- (1) *Approaches and driveways to City streets and alleys must be paved and constructed in accordance with the Standard Construction Specifications. Driveways serving more than one property shall be paved the full length of the shared portion.*

Response: As shown in the attached plans, paved driveway access points are proposed on Lyon Street, the portion of 6th Avenue that would remain after the vacation, and 7th Avenue. This standard is met.

- (2) *Driveways for single- and two-family dwellings must have a minimum width of 10 feet and a maximum width of 24 feet (not to exceed the width of the driveway curb cut) and minimum separation of 5 feet.*

Up to four multiple-family units that front on a public street may have separate driveways. The driveways shall meet the same standards as for single- and two-family dwellings.

Driveways for all other uses must have widths of 12-16 feet for one-lane (one-way) driveways, 24-32 feet for two-lane driveways, and 36 feet for three-lane driveways. Three-lane driveways

must have designated lanes and turning movements. Industrial driveways shall have a width of 24-48 feet. There must be a minimum separation of 22 feet between all driveways except for single- and two-family dwellings. The width of a driveway will be determined by measuring at the curb line and will exclude the transitions which must conform to standards fixed by the City Engineer.

Response: This proposal is for a public building and use. As shown in the attached plans, the 6th Avenue and 7th Avenue driveways will be two-way and will be a minimum of 24 feet wide. The Lyon Street access has a width of approximately 78 feet and is intended solely for emergency vehicles exiting or entering the apparatus bay. This five-lane driveway is not subject to the width limitations for one-, two-, or three-lane driveways noted above, and likewise is not subject to the width limitations for industrial driveways since fire stations are classified as institutional uses. As Lyon Street is under the jurisdictional control of ODOT, the applicant will need to obtain an approach permit from the State of Oregon. This standard does not apply.

- (3) *All driveways must be located as far as practical from a street intersection, and in no instance shall the distance from an intersection be less than the following, as measured from the nearest curb return radius:*

Arterial Street 40 feet

Collector Street 20 feet

Local Street 10 feet

At intersections with bulbouts or post-construction stormwater quality curb extensions incorporated into the curb return the measurement will be made from the nearest curb return radius. When different classes of streets intersect, the distance required is between an access point and the intersection of the street type that requires the greater distance.

Response: The subject site is located on Lyon Street, a Principal Arterial street, and 6th and 7th Avenues, which are local streets. The proposed driveway onto Highway 20 will be over 225 feet from 5th and 7th Avenues, while the driveway on 7th Avenue will be over 135 feet from Lyon Street and Baker Street. This standard is met.

- (4) *The location, width, and number of accesses to a public street may be limited for developments that are subject to site plan review. All development that proposes access to an arterial street is subject to site plan review and the design requirements of 12.230.*

Response: This process is understood. Section 12.230 is addressed in this narrative. This standard is met.

- (5) *Access points to a public street shall be the minimum necessary to provide reasonable access while not inhibiting the safe circulation and carrying capacity of the street.*

Response: Only one access point is proposed to each public street. This standard is met.

- (6) *Properties with frontage on more than one street may be restricted to access on the street(s) of a lower classification through site plan, land division, or other review procedures.*

Response: This potential limitation is understood. The applicant requests an emergency vehicle access to Lyon Street. This standard is met.

- (7) *A common access point at a property line is encouraged and may be required in order to reduce the number of access points to streets. Construction of common access points must be preceded by recording of joint access and maintenance easements.*

Response: This potential requirement is understood.

- (8) *With the exception of single-family residential development, approach grades must not exceed 10 percent slope within 20 feet of a public street. Driveways for single-family residential development shall comply with applicable fire and building codes.*

Response: As shown in the attached grading plan (Exhibit 3 Sheet C2.2), the slopes within 20 feet of the public streets will be no more than 5.5%. This standard is met.

- (9) *Access to designated state highways is subject to the provisions of this Article in addition to requirements of the State Highway Division and State Department of Transportation. When regulations of the City and State conflict, the more restrictive requirements apply.*

Response: This relationship is understood. The applicant will apply for ODOT permits for the proposed emergency access driveway on Highway 20. This standard is met.

- (10) *For developments on property larger than five acres in contiguous ownership fronting on an arterial street or limited access highway, a frontage road may be required in order to provide a single access determined by the review body to be the most appropriate location for safety and convenience.*

Response: The subject site is less than five acres. This standard does not apply.

- (11) *When access is allowed on an arterial street, efforts shall be made to locate it adjacent to the interior property line where it could be shared by the adjacent property.*

Response: The proposed driveway onto Highway 20 cannot be shared with other properties as it is solely for emergency vehicles exiting the apparatus bay. This standard does not apply.

12.110 Street Location, Width and Grade.

The location, width, and grade of all streets must conform to any approved transportation master plan or recorded subdivision plat. When location of a street is not shown in an approved street plan, the arrangement of streets in a development shall either provide for the continuation or appropriate projection of existing principal streets in the surrounding areas or conform to a plan for the neighborhood approved or adopted by the City to meet a particular situation where topographical or other conditions made continuance of or conformance to existing streets impractical or where no plan has been previously adopted.

In addition, new streets may be required to be located where the City Engineer determines that additional access is needed to relieve or avoid access deficiencies on adjacent or nearby properties. In determining the location of new streets in a development or street plan, consideration shall be given to maximizing available solar access for adjoining development sites.

Street grades must be approved by the City Engineer, who will consider drainage and traffic safety.

Response: No changes are proposed to the existing street locations or grades. This standard does not apply.

12.120 Rights-of-Way and Roadway Widths.

Unless otherwise indicated on an approved street plan or in Section 12.130, the street right-of-way and roadway widths shall not be less than the minimum shown below in Table 12-1. Where a range is indicated, the width shall be determined by the City Engineer. Reductions in roadway width may be allowed to facilitate curb extensions required for approved street side post-construction stormwater quality facilities in the landscape strip/planter area. Reductions require approval of the City Engineer.

TABLE 8-1: STREET RIGHT-OF-WAY AND ROADWAY WIDTH REQUIREMENTS		
Type of Street	Minimum Rights-of-Way Width	Minimum Roadway Width
Arterial	70-120 feet	40-70 feet
Collector	60-80 feet	36-48 feet
Local*	42-56 feet	22-32 feet
Radius for turnaround at end of cul-de-sac	43 feet	36 feet
Alley	14-20 feet	12-20 feet

*When street rights-of-way are less than 60 feet wide, a parallel public utility easement 7-feet-wide shall be dedicated on both sides of the right-of-way unless waived by the City Engineer.

Response: No changes are proposed to the existing right-of-way widths. This standard does not apply.

12.140 Additional Rights-of-Way.

A development project requiring land use approval is required to dedicate additional right-of-way if an existing street abutting or within the development does not meet the widths designated in Section 12.120. This provision does not apply to property line adjustments or historic review. While not required to dedicate additional right-of-way, single- and two-family dwellings (and related accessory buildings) are subject to setbacks from future street rights-of-way as provided in Section 3.190.

Response: The existing rights-of-way meet the designated standards. This standard does not apply.

12.150 Future Extensions of Streets.

When it is necessary to give access to or permit a future division of adjoining land, streets shall be extended to the adjoining tract. A barricade at the end of the street shall be installed and paid for by the property owners. It shall not be removed until authorized by the City Engineer.

Response: No access to or division of adjoining land is required. This standard does not apply.

12.160 Street Alignment.

As far as practical, streets shall be dedicated and constructed in alignment with existing streets. Arterial and collector streets shall have continuous alignments without offset or staggered intersections. In no case shall streets be designed so that jogs of less than 300 feet are created as measured from the centerline of any intersection involving an arterial or collector street.

Response: The street network is already set at this location. This standard does not apply.

12.170 Intersections.

Streets must intersect as nearly as possible at right angles. Proposed intersection of two streets at an acute angle of less than 75 degrees is not allowed. An oblique street should be curved approaching an intersection to provide at least 100 feet of street at right angles with the intersection. Not more than two streets shall intersect at any one point.

Response: The street network is already set at this location. This standard does not apply.

12.180 Clear Vision Area.

A clear vision area must be maintained at each access to a public street and on each corner of property at the intersection of two streets or a street and a railroad. No fence, wall, hedge, sign, or other planting or structure that would impede visibility between the heights of 2 and 8 feet shall be established in the clear vision area. Visibility is not considered impeded by a fence where materials are 35 percent or less of

the surface area of that portion of the fence above 2 feet. Fence posts spaced at 8 feet or more apart are not counted as part of the fence surface area. Height measurements shall be made from the top of the curb or, when no curb exists, from the established street center line grade.

- (1) The clear vision area provisions do not apply to the following:
 - (a) a public utility pole,
 - (b) a tree trimmed (to the trunk) to a line at least eight feet above the level of the intersection,
 - (c) another plant species of open growth habit that is not planted in the form of a hedge and that is planted and trimmed to leave at all seasons a clear and unobstructed cross-view,
 - (d) a supporting member or appurtenance to a permanent building lawfully existing on the date this standard becomes effective,
 - (e) an official warning sign or signal,
 - (f) the post section of a pole sign when there are no more than two posts and any post is less than eight inches in diameter, and
 - (g) existing or new buildings that meet the minimum setbacks.

Response: Only low shrubs are proposed within the clear vision area. This standard is met.

- (2) A clear vision area consists of a triangular area, two sides of which are lot lines or a driveway and a lot line for a distance specified in this section, or, where the lot lines have rounded corners, the lot lines extended in a straight line to a point of intersection and so measured, and the third side of which is a line across the corner of the lot joining the non-intersecting ends of the other two sides (See illustration below, Figure 6). The measurements in Table 12-4 below shall establish the clear vision areas:

TABLE 12-4: MEASUREMENTS FOR ESTABLISHING CLEAR VISION AREAS	
Type of Intersection	Measurement Along Each Lot Line or Drive Edge*
Controlled Intersection (stop sign or signal)	20 feet
Uncontrolled Intersection	30 feet
Commercial and Industrial District driveways	20 feet
Residential District driveways	15 feet
Alley (less than 25 feet)	20 feet

*When two or more streets of different right-of-way width intersect, the distance to be measured along the lot lines shall be the distance specified for each type street.

Response: Exhibit 3 Sheets L4.1 and L4.2 illustrate the required clear vision areas at the driveway entrances. No obstacles are proposed within the regulated heights (2-8 feet above grade) and the landscaping has been selected to comply with the applicable standards. This standard is met.

12.200 Street Abutting New Development.

Sections of existing streets that directly abut a new development and do not meet City standards shall be constructed to City standards. The City Engineer may approve construction of a partial-width street, provided the design is determined to be adequate to accommodate needed public facilities, storm drainage runoff, traffic volumes, and traffic loadings. The design of the improvement shall consider the ultimate design of the fully widened street. For purposes of this section, "development" means a land division, new commercial or industrial development, construction of multi-family residential units, or a manufactured home or recreational vehicle park.

A future improvement assurance, as described in Section 12.600, may be accepted by the City when the City Engineer determines that the street improvement would not be timely.

Response: Frontage improvements will be performed as required by the City Engineer. This standard is met.

12.210 Slope and Curves.

Slope shall not exceed 6 percent on arterials, 10 percent on collector streets, or 12 percent on other streets. Center line radii or curves shall be not less than 600 feet on arterials, 400 feet on collectors, or 200 feet on other streets. When existing conditions, particularly the topography, make it otherwise impractical to provide buildable sites, steeper grades and sharper curves may be approved by the review body. In flat areas, allowance shall be made for finished street grades having a minimum slope of at least 0.5 percent, when possible.

Response: No changes to the existing street system are proposed. This standard does not apply.

12.220 Street Adjacent to Railroad.

Whenever a proposed development contains or is adjacent to a railroad right-of-way, provision may be required for a street approximately parallel to and on each side of the railroad right-of-way at a distance suitable for appropriate use of the land between each street and the railroad. The distance shall be determined with consideration at each cross street of the minimum distance required for approach grades to a future grade separation and to provide sufficient depth to allow screen planting along the railroad right-of-way in non-industrial areas.

Response: The subject site is not adjacent to a railroad. This standard does not apply.

12.230 Access to Arterials.

When a development abuts or contains an existing or proposed arterial street, the development design shall provide adequate protection for residential properties and shall separate residential access and through traffic; or if separation is not feasible, the design shall minimize the traffic conflicts. The design requirements may include any of the following:

- (1) *A parallel access street along the arterial;*
- (2) *Lots abutting the arterial of suitable depth to provide adequate buffering and having frontage along another street;*
- (3) *Screen planting at the rear or side property line to be contained in a non-access reservation along the arterial; or*
- (4) *Other treatment, as determined by the Director, suitable to meet the objectives of this subsection.*

Response: This standard is primarily intended to protect new residences from the traffic effects from an arterial. Since no residences are proposed for this development, this standard does not apply.

12.260 Traffic Signals.

Where a single development or concurrent group of developments will create a need for a traffic signal at an intersection, such installation may be a condition of development approval.

Response: Feedback from the City Engineer and ODOT staff has indicated that no traffic signal is required. This standard does not apply.

Sidewalks**12.290 Requirement.**

All development for which land use applications are required by Section 1.060 must include sidewalks adjacent to public streets. This requirement also applies to new single-family houses and duplexes if they are located on arterial or collector streets or on curbed local streets, if there is an existing sidewalk within 500 feet on the same side of the street.

Sidewalks shall be built when arterial and collector streets are constructed and at the discretion of the City Engineer during their reconstruction. This provision shall also apply to local streets that serve commercial and multi-family development. Sidewalks are required on both sides of all streets. If an interim street standard is being constructed which does not include bike lanes or sidewalks, interim bikeways or walkways for pedestrians shall be provided by paved roadway shoulders at least 8 feet wide on arterials and 6 feet on other streets. Provision of sidewalks may be waived when the street serves a use or combination of uses that generate fewer than 50 trips a day (based on ITE standards) and cannot be continued or extended to other properties.

Response: As shown in the attached plans, sidewalk improvements will be made on Lyon Street and 7th Avenue. This standard is met.

12.300 Design, Width, and Location.

All sidewalks must be constructed, replaced or repaired in accordance with the Standard Construction Specifications. The required width and location of sidewalks is as follows:

- (1) *The required width for a sidewalk on an arterial or collector street is 7 feet. This may be reduced to 6 feet if the sidewalk is separated from the curb by a landscaped planter strip at least 5 feet wide. When there is inadequate right-of-way for additional width and no additional right-of-way can be obtained as a condition of development approval, the sidewalk width may be reduced to 5 feet. In all cases, any right-of-way remaining outside the sidewalk is to be landscaped and maintained by the adjoining property owner.*

Response: The proposed sidewalk replacement along Lyon Street would have a minimum width of 8 or more feet. This standard is met.

- (2) *Sidewalks along residential and other local streets must be at least 5 feet wide. A planter strip at least 6 feet wide shall separate the sidewalk from the street. Street trees shall be selected from the list of approved street trees established by the City. The planter strip shall be of permeable materials. Locating approved street-side post-construction stormwater quality facilities in the planter strip is encouraged.*

Response: The subject site does abut a local street, 7th Avenue. The sidewalk improvements will be constructed to City of Albany standards. This standard is met.

- (3) *In the Historic Downtown and Central Business districts, as defined on the zoning map, sidewalks must be at least 10 feet wide and be installed adjacent to the curb.*

Response: The subject site is in the Lyon-Ellsworth District, not in the Historic Downtown or Central Business District. This standard does not apply.

- (4) *Regardless of other provisions contained in this article, any sidewalk project that is less than 200 feet in length and connects on either end to an existing sidewalk may be designed to match the existing pattern with the approval of the City Engineer.*

Response: This option is not proposed.

- (5) *When obstructions exist or are proposed (including, but not limited to, mailboxes, utility poles, trees, planters, fire hydrants, signs, benches, bus stops, post-construction stormwater quality facilities, etc.), provisions must be made to maintain a minimum of 4 feet of unobstructed sidewalk width on local streets, 5 feet on collector and arterial streets, and 6 feet in the Historic Downtown (HD) and Central Business (CB) districts.*

Response: Any obstructions in the area of the replacement sidewalks will be designed in accordance with Americans with Disabilities Act standards. This standard is met.

- (6) *Clustered mailboxes shall be on an accessible path and shall include a 72" minimum accessible turning space. Adjacent sidewalks may be incorporated in the measurement of the turning space. Where constructed in the public right-of-way, construction shall be in accordance with the Standard Construction Specifications.*

Response: No clustered mailboxes are proposed. This standard does not apply.

- (7) *Maintenance of sidewalks and planter strips shall be the continuing obligation of the adjacent property owner except for approved post-construction stormwater quality facilities located in planter strips or when the lot backs onto an arterial. Other than approved post-construction stormwater quality facilities, planter strips shall be landscaped and maintained in like manner to the front yard setback requirements of Article 9.*

Response: Maintenance of the sidewalk and planting strip will be the obligation of the owner. This standard is met.

- (8) *Sidewalks shall be designed to parallel streets in line and grade and shall avoid unnecessary meandering from the curb line and elevation changes except as necessary to avoid significant trees or traverse topographic barriers.*

Response: As shown in the attached plans, the sidewalk replacement will be parallel with Lyon Street and 7th Avenue. This standard is met.

- (9) *Public paths not adjacent to a public street shall be a minimum of 10 feet wide.*

Response: No public paths not adjacent to a public street are proposed. This standard does not apply.

Street Trees

12.321 General Requirements.

When a new public street is created in conjunction with development, street trees are required in accordance with the standards provided in the Standard Construction Specifications and the Urban Forestry Management Plan.

Response: No new public streets are proposed. This standard does not apply.

12.324 Street Tree Planting Options.

Options available to meet this requirement are identified below. Where street side post-construction stormwater quality facilities are proposed on a new public street, the option selected must match the planting option selected for the post-construction stormwater quality facilities as identified in Title 12 of the Albany Municipal Code:

- (1) *Submit a street tree plan to the City for planting and establishing trees within the public rights-of-way that meets the tree planting standards in the Urban Forestry Management Plan. The City Forester shall either approve or deny the plan based on the plan's compliance with these requirements.*

- (2) *Pay a fee to the City based upon a requirement for one tree per thirty linear feet of street frontage. This fee shall be deposited into the City's Urban Forestry Program Fees Fund. The City shall thereafter assume responsibility for the purchase, planting, and establishment of street trees within the public right-of-way or on public lands maintained by the City within or abutting the specified development.*

Response: No new public streets are proposed; nonetheless, street trees are proposed in accordance with City requirements (see Exhibit 3 Sheets L4.1 and L4.2). This standard is met.

Bikeways

12.330 Master Bikeways Plan.

The City's adopted Master Bikeways Plan is in the Comprehensive Plan.

12.340 Provisions for Bikeways.

Developments adjoining or containing proposed bikeways identified on the adopted Master Bikeways Plan shall include provisions for the future extension of such bikeways. Land use approvals issued for planned developments, greenway conditional use permits, subdivisions and other developments that will principally benefit from such bikeways may be conditioned to include bikeway improvements.

In the case of arterial or collector streets, bike lanes shall be built during their construction, and considered during their reconstruction. This provision shall also apply to local streets in other than single-family residential developments.

Response: The subject site adjoins Highway 20, which is a proposed bikeway, but no new construction of arterial or collector streets is proposed. This standard does not apply.

12.350 Bikeway Design.

Where possible, bikeways should be separated from other modes of travel, including pedestrian. Minimum width for bikeways shall be 6 feet per travel lane when adjacent to a curb (one-way) and 10 feet when not on a roadway (two-way). A reduction in standards may be allowed when the City Engineer finds that no safety hazard will be created and other special circumstances (such as physical constraints) exist.

Response: No new bikeways are required. This standard does not apply.

Utilities – General

12.360 Utility Easements.

The developer shall make arrangements with the City of Albany and each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development. All utility easements must be public easements.

Response: Easements will be provided for public utilities as necessary. This standard is met.

12.370 Utility Easement Width.

The standard width for public utility easements adjacent to street rights-of-way is 7 feet. The minimum width for all other public utility easements shall be 15 feet for water, 20 feet for sewer, and 15 feet for piped storm drainage unless otherwise specified by the utility company or City Engineer. When feasible, utility easements shall be centered on a lot line.

Response: Any required easements will provide for the required easement widths noted above. This standard is met.

12.380 Information on Development Plans.

The developer must show easements for all utilities. Plans showing the location of all utilities shall be submitted to the City as part of the site plan review or land division process.

Response: No public utilities requiring easements are necessary to serve the development. The existing utilities in the 6th Avenue corridor will be abandoned in the proposed vacation area. This standard does not apply.

12.390 Requirement for Underground Utilities.

Except as exempted in Section 12.400, all utility lines, cables, or wires (including but not limited to those used for electricity, communication, street lighting, and cable television) constructed upon, adjacent to, or within land subdivided or prepared for development after the effective date of this Code, must be placed underground. The intent of the City is that no poles, towers, or other structures associated with utility facilities shall be permitted on any street or lot within such a subdivision or development.

Response: As described in the response to 12.400 below, the proposal is exempt from this standard.

12.400 Exceptions.

Overhead facilities are only permitted in the following instances:

- (1) *Emergency installations, electric transmission lines, or through feeders operating at distribution voltages which act as a main source of supply to primary laterals and to direct connected distribution transformers and primary loads.
Should it be necessary to increase the capacity of major power transmission facilities for service to the area, new or revised installations shall be made only on rights-of-way or easements on which overhead facilities exist at the time of the capacity increase.*
- (2) *Appurtenances and associated equipment such as surface-mounted transformers, pedestal-mounted terminal boxes, meter cabinets, telephone cable closures, connection boxes, and the like.*
- (3) *Structures without overhead wires, used exclusively for fire alarm boxes, street lights, or municipal equipment installed under the supervision and with the approval of the City Engineer.*
- (4) *Power substations, pumping plants, and similar facilities necessary for transmission or distribution of utility services.*
- (5) *Television antennas and satellite dishes [See Section 3.080 (12)].*
- (6) *Industrial developments, except for utility lines, cables, and/or wires providing service to an individual lot. Such lines must be placed underground from the nearest power pole to the facility ultimately being operated on the individual lot. Certain industries requiring exceptionally large power supplies may request direct overhead power as a condition of site plan approval. Underground utilities may be required in Industrial Park developments and planned developments in the Industrial Districts.*
- (7) *New development on existing individual lots of record in areas where service is currently by overhead utilities.*

Response: Overhead facilities exist on the western property line and will be allowed to remain per criterion (7) above. The proposal meets this exception.

12.405 Property Monuments.

Upon completion of a utility project and before acceptance by the City, all property corners and other monuments disturbed or removed by the project shall be reestablished and protected by an Oregon-licensed surveyor retained by the developer.

Response: This process is understood.

Water*12.410 When Public Water is Available.*

All new development, including a single-family residence, must extend and connect to the public water system when service is available within 150 feet of the property. Fire hydrants, mains, and related appurtenances shall be installed as required by the City Fire Marshal.

Response: As shown in the attached plans, the proposed development will connect to the public water system located in Lyon Street. This standard is met.

12.420 When Public Water is Not Available.

No new development is allowed on private well systems, except for construction of one single-family dwelling on an existing lot of record. Residential lots created by a land partition may be served by private wells if approved by the City, and provided the new lots are subject to a Petition for Improvement/Waiver of Remonstrance for a future assessment district for public water. If a second partition plat is filed on the same parcel, the application will be subject to the subdivision requirement that the development be served by public water.

Response: Public water is available to the subject site. This standard does not apply.

12.430 Extension Along Property Frontage and Within Interior.

Water distribution mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion. Main extensions may be required through the interior of properties when necessary to provide service to other properties or to provide looping for fire flows.

Response: The public water system currently meets this standard. This standard does not apply.

12.440 Water Plan Approval.

Preliminary water plans and systems must be submitted to the City Engineer as part of the tentative plat or Site Plan Review application. These plans must provide enough information to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The utility plans provided with this submittal show the information required above. This standard is met.

12.450 Design Requirements for New Development.

All new development within the City must, when appropriate, make provisions for the continuation or appropriate projection of existing principal water lines serving surrounding areas.

Response: The existing water system provides the services required to surrounding areas. This standard does not apply.

12.460 Restriction of Development.

The review body may restrict development approvals when a deficiency exists in the water system or portion thereof which cannot be corrected as a part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

Sanitary Sewers*12.470 When Public Sewer is Available.*

All new development must extend and connect to the public sewer system when service is available within 300 feet of the property.

Response: As shown in the attached plans, the proposed development will connect to the public sewer system in 6th Avenue. This standard is met.

12.480 When Public Sewer is Not Available.

Where sewer is not available within 300 feet of the property, no development is allowed on private septic systems, except for construction of one single-family dwelling on an existing lot of record or on a parcel no smaller than five acres created through the land division process. Any private on-site system allowed by this section must be approved by the county.

Response: Public sewer is available to the subject site. This standard does not apply.

12.490 Extension Along Property Frontage and Within Interior.

Sewer collection mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion. When private sanitary sewer services will exceed 100 feet long, as measured from the public main to the structure, the City Engineer may require extension of public sewers into the interior of the property.

Response: The public sewer system currently meets this standard. This standard does not apply.

12.500 Sewer Plan Approval.

Preliminary sewer plans and systems must be submitted to the City Engineer as part of the tentative plat or Site Plan Review application. These plans must provide enough information to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The utility plans provided with this submittal show the information required above. This standard is met.

12.510 Design Requirements for New Developments.

All new development within the City must, where appropriate, make provisions for the continuation or appropriate projection of existing sewer lines serving surrounding areas. Line extensions may be required through the interior of a property to the developed when the City Engineer determines that the extension is needed to provide service to upstream properties.

Response: The existing sewer system provides the services required to surrounding areas. This standard does not apply.

12.520 Restriction of Development.

The review body may restrict development approvals where a deficiency exists in the sewer system or portion thereof that cannot be corrected as a part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

Storm Drainage*12.530 General Provisions.*

The review body will approve a development request only when adequate provisions for storm and flood water run-off have been made as determined by the City Engineer. The stormwater drainage system must be separate from and independent of any sanitary sewer system. When possible, inlets should be provided so surface water is not carried across any intersection or allowed to flood any street. Surface water drainage patterns and proposed storm drainage must be shown on every development proposal plan. All proposed storm drainage management plans and systems must be approved by the City Engineer as part of the tentative plat or site plan review process.

Response: The proposed development will provide a storm drainage system meeting the requirements listed above (see Exhibit 3 Sheets C2.2 and C2.3 and Exhibit 7). This standard is met.

12.535 Storm Drainage Plan Approval.

Preliminary storm drainage management plans and systems must be submitted to the City Engineer as part of the Tentative Plat or Site Plan Review application. These plans must provide enough information to enable the City Engineer to determine that the proposed development is feasible, but are not required to be detailed construction level documents. The City's Engineering Standards, while not land use criteria, may be used, in whole or in part, by the City Engineer to determine the feasibility of a proposed plan.

Response: The proposed development will provide a storm drainage plan meeting the requirements listed above (see Exhibit 3 Sheets C2.2, C2.3, L4.1, and L4.2 and Exhibit 7). This standard is met.

12.540 Easements.

When a subdivision is traversed by a watercourse, drainageway, channel or stream, a public stormwater easement conforming substantially to the lines of the watercourse and further width as the City Engineer determines will be adequate for conveyance and maintenance shall be provided. Improvements to the drainage way, streets, or parkways parallel to watercourses may be required.

Response: There are no watercourses or similar features crossing the site. This standard does not apply.

12.550 Accommodation of Upstream Drainage.

A culvert or other drainage facility shall be large enough to accommodate potential run-off from its entire upstream drainage area, whether inside or outside of the development. The City Engineer must review and approve the necessary size of the facility, based on the provisions of the Storm Drainage Master Plans, and sound engineering principles, and assuming conditions of maximum potential watershed development permitted by the Comprehensive Plan.

Response: No culverts or other public facilities are proposed; the stormwater facilities will manage runoff from the proposed project. This standard does not apply.

12.560 Effect on Downstream Drainage Facilities.

When the City Engineer anticipates that the run-off resulting from the development will overload or cause damage to an existing drainage facility, the review body will withhold approval of the

development until provisions have been made for improvement, or prevention, of said potential condition.

Response: This condition does not exist to the knowledge of the applicant. This policy is understood but does not apply.

12.570 Storm Drainage Management Practices.

Development must employ storm drainage management practices approved by the City Engineer that minimize the amount and rate of surface water run-off into receiving streams or drainage facilities or onto adjoining properties. As required by Title 12 of the Albany Municipal Code, the development must also employ post-construction stormwater quality management practices approved by the City Engineer that regulate the quality of the stormwater leaving the site. Drainage management practices must include, but are not limited to, one or more of the following practices:

- (1) Temporary and permanent ponding or detention of water;*
- (2) Post-construction stormwater quality facilities;*
- (3) Minimization of impervious surfaces;*
- (4) Emphasis on natural drainageways;*
- (5) Prevention of uncontrolled water flow from the development;*
- (6) Stabilization of natural drainageways as necessary below drainage and culvert discharge points for a distance sufficient to convey the discharge without channel erosion;*
- (7) Collection of runoff from impervious surfaces and transportation to a natural drainage facility with sufficient capacity to accept the discharge; and*
- (8) Other practices and facilities designed to transport stormwater and improve water quality.*

Response: As shown in the attached plans, the proposed on-site storm drainage system meets the requirements listed above. This standard is met.

12.575 Extension Along Property Frontage and Within Interior.

Storm mains must be extended along the full length of the property's frontage along the right-of-way or to a point identified by the City Engineer as necessary to accommodate likely system expansion.

Response: As shown in the attached plans, a public storm line is proposed within 6th Avenue from the site to Baker Street to meet this requirement. This standard is met.

12.580 Design Requirements for New Development.

All new development within the City must, when appropriate, provide for the continuation or appropriate projection of existing storm drain lines or drainageways serving surrounding areas. Extensions may be required through the interior of a property to be developed when the City Engineer determines that the extension is needed to provide service to upstream properties.

Response: As shown in the attached plans, the proposed public storm line is designed to provide drainage for the site itself. No further extension of the public storm drainage system is required. This standard is met.

12.581 Restriction of Development.

The review body may restrict development approvals where a deficiency exists in the stormwater system or portion thereof that cannot be corrected as part of the development improvements.

Response: No deficiencies are known. This policy is understood but does not apply.

12.585 NPDES Permit Required.

A National Pollutant Discharge Elimination System (NPDES) permit must be obtained from the Department of Environmental Quality (DEQ) for construction activities (including clearing, grading, and excavation) that disturb one or more acre of land, or whatever the current standard is at the time the application is submitted.

Response: An NPDES permit from DEQ will be acquired prior to any commencement of construction. This standard is met.

Article 13 – Signs

Review Procedures.

13.210 Permit Needed.

Except as specifically excluded herein, no property owner, lessee, contractor, or other person shall display or cause to be displayed any sign requiring a permit as set forth in this Code, except for maintenance of signs that conform with this ordinance, without first obtaining from the Building Official a written permit to do so, paying the fees prescribed therefore, and otherwise complying with all of the applicable provisions of this ordinance. If a governmental agency requires the relocation of a sign, the fee described above shall be waived.

Response: Exhibit 3 Sheet L2.2 illustrates the proposed location of a concrete sign wall next to plaza. The applicant will obtain necessary permits prior to installation of any signage. This standard is met.

IV. CONCLUSION

Based on the information presented and discussed in this narrative and the attached supporting plans and documentation, this application meets all applicable standards necessary for land use approval. The proposed development complies with all applicable standards of the Albany Development Code and furthers the City's objectives of promoting downtown and providing a facility that meets the needs of the Fire Department. The applicant respectfully requests approval by the City.



City of Albany
Fire Station 11 Neighborhood Meeting

August 12, 2015

Fire Station 11, 110 6th Avenue SE, Albany, OR

6:00 p.m.

METHOD OF PUBLICIZING MEETING

For the neighborhood meeting, 206 public notices were mailed through the United States Postal Service to all property owners and residents within 1,000 feet of the boundaries of the subject property. The public notice included the date, time, and location of the public meeting and a brief description of the development plans.

INTRODUCTIONS

John Bradner, Fire Chief
Wes Hare, City Manager
Stacy Belcastro, Acting City Engineer
Jeff Woodward (David Evans & Associates), Owner's Representative
Jeff Humphreys & Lorraine Jack (Mackenzie), Architects

ATTENDEES

The neighborhood meeting was attended by 25 people. Two people were from Mackenzie, one was from David Evans & Associates, and 13 people were from the City. The remaining nine attendees were neighbors or other interested parties. See attached attendance lists.

OVERVIEW

Chief Bradner identified the purpose of the meeting was to inform neighbors of the planned development for the site; identify anticipated impacts to neighboring properties; try to mitigate potential impacts; and provide opportunity for public comments/questions.

The new station site will include the current station location and adjacent properties to the north and south that have been acquired by the City. The new site includes closure of 6th Avenue at Lyon Street.

Chief Bradner identified the project's main timelines:

- Currently in Phase II of the design process.
- Land use application submission in September 2015.
- Planning Commission in November 2015.
- Site prep in November 2015 through February 2016.
- Council approval in January 2016.
- Construction to start in April 2016.
- Completion in March 2017.

Jeff Humphreys identified that the current site is being redeveloped for the new station. The building will be just under 25,000 square feet in size and measure 30 feet tall at the highest point.

There will be secured employee parking on the north and northeast portions of the redeveloped site and public parking in the south corner. Emergency response vehicles will exit the station onto Lyon Street. He

City of Albany
 Fire Station 11 Neighborhood Meeting
 Page 2 of 3
 August 12, 2015

shared a slideshow presentation that included an aerial perspective of the new station, showing solar panels on the roof, which is one of several features to meet the 1.5 percent green technology requirement. There will be skylights to bring in natural light to deeper portions of the building, and the exterior will be brick for durability and to provide a traditional fire station appearance. The lobby will house an antique fire apparatus and there will be a public meeting/training room on the first floor.

PUBLIC COMMENTS/QUESTIONS

Question: Why don't the windows on the two floors line up on the east side of the building?

Response: The windows are placed due to the indoor use of the spaces.

Question: Will the City be requesting LEED certification?

Response: No, the City is not requesting certification; however, there will be aspects of the building that qualify, e.g., energy efficient lighting and windows, solar panels, water efficient toilets.

Question: What level of earthquake will the new station sustain?

Response: It's not measured by the seismic number. A fire station is considered an essential facility and is required by the state to be designed for immediate occupancy in the event of a major seismic event, with no impact to service from the station.

Question: What is being done in the station to address the one percent requirement for art?

Response: The intent is to refurbish a 1907 steamer engine to display in the first floor lobby. The concept is being presented to the City Art Commission in September for their review and recommendation to the City Council for approval.

Question: What type of security fence material will be used?

Response: Security fencing will be powder-coated, black chain link fence with privacy slats, and include landscape screening along Lyon Street.

Question: What type of site lighting will be used?

Response: There will be higher and lower lighting incorporated into the site, but the actual lighting has not been identified yet.

Question: Will there be fencing behind the public parking area?

Response: Dense landscaping is identified for that area at this time, but no fencing.

Question: Will you be removing the large trees that have ivy on them near the public parking area?

Response: Have not identified which trees will remain and which will need to be removed yet.

Question: Has the traffic been considered for crossing Lyon Street on 5th and 7th?

Response: No other street changes have been noted yet.

Question: At what point in the timeline will 6th Avenue be blocked off?

Response: When construction begins in April 2016.

Question: Where would the fencing be located at the northeast corner next to existing neighbor's home? Concern that the fences will be touching and not allow property owner to access the back side of his home for painting or repairs.

City of Albany
Fire Station 11 Neighborhood Meeting
Page 3 of 3
August 12, 2015

Response: There can be flexibility on the fence location to provide access behind neighbor's existing fence for maintenance of that area.

Question: What happens when 6th Avenue closes; where will that traffic divert? Traffic during certain times of the day can make it difficult to cross Lyon Street. Will there be an analysis to determine if additional safety measures are required?

Response: The City will be submitting an application to ODOT for the closure of 6th Avenue. Vacation of that street is part of the land use application. ODOT will likely require a high visibility crosswalk at the south side of 6th on Lyon Street. ODOT will likely require a gap analysis.

Question: Will there be any parking on Lyon Street?

Response: Yes, parking will be allowed on Lyon Street in front of the station and will be set back farther from the traffic to increase safety.

Comment: A person identifying himself as a neighbor commented that he appreciates the design of the building with the cornices and single window style and feels it is something Albany should be proud of.

SUMMARY

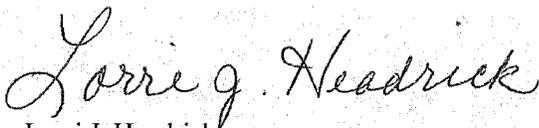
The City and designers will take into account all noted concerns and suggestions. Most notably: redirecting vehicular traffic, existing site trees, and the location of fencing along adjacent properties. The team will be discussing options for handling concerns with the two adjacent properties; the City will have an arborist evaluate the trees on site; and future discussions with ODOT and traffic analysis will be contingent upon any necessary revisions to current designs.

ADJOURNMENT

Wes Hare identified that this meeting was one step in a larger process and that there will be other opportunities for public comment; the next being at the November 16 Planning Commission meeting.

The meeting adjourned at 6:30 p.m.

Submitted by,



Lorri J. Headrick
Sr. Administrative Supervisor

Albany Fire Station Neighborhood Meeting
 August 12, 2015; 6:00 p.m.
 Fire Station 11

Sign-in Sheet

Name	Address
Staci Belcastro	City of Albany PW PO Box 369 Halsey OR 97348
KATE HENNESSY	ALBANY FIRE DEPT 333 BROADALBIN ST SW ALBANY, OR 97321
JEFF HUMPHREYS	MACKENZIE 1515 SE WATER AVE #100 PORTLAND, OR 97204
LORRAINE JACK	MACKENZIE 1515 SE WATER AVE #100 PORTLAND, OR 97214
Herb Yamamoto	705 Lyon SE Albany, OR 97321
Charlene Dunten	1230 NW. Thornton LK Dr Albany OR 97321
SHANE WOOTON	COA PO BOX 490
Shane Castle	AFD 72 N. Main Falls City OR 97346
Mary Hargrove	624 Baker St
James Furlo	Location 505 Lyon mail: 1934 1st Ave E Albany OR 97321
Roger Jory	226 SW 6 th AVE
Bryan/Natalie Bauer	524 Baker St SE Albany OR 97321
Sandra Harris	COA PLANNING STAFF 333 BROADALBIN
David Martinson	Albany Morning Division 333 Broadalbin St. SW Albany, OR 97321
Jennifer Moody	Albany Democrat-Herald 541-812-6113

Albany Fire Station Neighborhood Meeting
 August 12, 2015; 6:00 p.m.
 Fire Station 11

Sign-in Sheet

Name	Address
JEF WOODWARD	DAVID EWNS AND ASSOCIATES
Tina Vaughn	532 Baker Street ALBANY OR 97321
Floyd Collins	Albany City Council
John Bradner	Fire Chief, Albany Fire
Lorri Headrick	Albany Fire
Wes Hare	City Manager, City of Albany
Ryan Bond	Albany Fire
Ron Wackford	Albany Fire
Kyle Romey	Albany Fire

MACKENZIE.

DESIGN DRIVEN | CLIENT FOCUSED

September 9, 2015

City of Albany Community Development Department
 Attention: Melissa Anderson, AICP, PMP, CFM
 PO Box 490
 Albany, OR 97321

Re: **Albany Fire Station 11 Replacement**
Trip Generation Estimates
 Project Number 2140087.00

Dear Ms. Anderson:

This letter presents trip generation estimates for the proposed Fire Station 11 Replacement in Albany, Oregon. The new fire station will replace and enhance the functions of the existing station at 110 6th Avenue SE by redeveloping the existing site and several neighboring properties, including the 6th Avenue SE right-of-way to be vacated between Lyon and Baker Streets. Collectively, the redevelopment site totals approximately 1.6 acres. The new fire station will serve as a hub for expanded and modernized fire protection coverage in the surrounding area; it also will provide a community meeting room for public use. A reduced copy of the site plan is enclosed for reference.

Vehicle trip generation characteristics for a fire station land use are not well documented due, in part, to the wide variety of amenities they offer and the diverse communities they support. The Institute of Transportation Engineers' (ITE) *Trip Generation Manual* (currently in its 9th Edition), the industry standard reference, does not supply trip data for fire stations. The limited trip surveys with which we are familiar suggest a daily trip rate of 3.5 trips per 1,000 square feet of gross floor area and a PM peak hour trip rate approximately 10% of the daily rate may be appropriate. These rates are applied to the proposed 24,838 SF fire station.

As noted above, the proposed fire station replacement encompasses several neighboring properties; some of these are vacant, some are developed only with outbuildings, and some are developed with occupied buildings. The occupied buildings are understood to generate vehicle trips in the existing condition: the existing 14,500 SF fire station, a 3,672 SF accountant services business¹ (north of the existing fire station on Lyon Street), a 1,345 SF investment services business² (north of the existing fire station on 6th Avenue SE), and a 2,505 SF dry cleaner³ with drive-through lane (south of the existing fire station on Lyon Street). The fire station trips are calculated using the survey rates noted above. The ITE *Trip Generation Manual* offers compatible land use models for estimating vehicle trips at the accountant and investment services uses: General Office Building (ITE Land Use Code 710). ITE supplies no data for dry cleaner uses, so instead the business is estimated to process 15 transactions during a peak hour, representing 30 trips, and the daily trips may be approximately 10 times the PM peak hour trips.

¹ The existing building appears to be a single-story structure, so the stated floor area represents the surveyed footprint.

² The existing building appears, at least partially, to be a two-story structure, but the stated floor area represents the surveyed footprint for simplicity.

³ The existing building appears to be a single-story structure, so the stated floor area represents the surveyed footprint.



P 503.224.9560 ▪ F 503.228.1285 ▪ W MCKNZE.COM ▪ RiverEast Center, 1515 SE Water Avenue, #100, Portland, OR 97214
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The following table presents the trip generation estimates for the proposed fire station, the existing uses including the existing fire station, and the net changes in future trip generation anticipated with the proposed redevelopment.

TRIP GENERATION CALCULATIONS						
Building	ITE Land Use Code (or other model)	Floor Area (SF)	Weekday		Weekday PM Peak Hour	
			Trip Rate (per 1,000 SF)	Trips	Trip Rate (per 1,000 SF)	Trips
Proposed Use						
Fire Station	Surveys	24,838	3.5	87	0.35	9
Subtotal			n/a	87	n/a	9
Existing Uses						
Fire Station	Surveys	14,500	3.5	51	0.35	5
Accountant	710	3,672	11.03	41	1.49	5
Investment	710	1,345	11.03	15	1.49	2
Dry Cleaner	Estimates	2,505	n/a	300	n/a	30
Subtotal			n/a	407	n/a	42
Net Trips (Proposed Subtotal – Existing Subtotal)			n/a	(310)	n/a	(33)

The proposed redevelopment of the existing Fire Station 11 and neighboring properties is anticipated to generate net decreases of approximately 310 daily trips and 33 PM peak hour trips.

If you have any questions regarding these trip estimates, please contact either of us directly.

Sincerely,

Brent Ahrend, PE
Senior Associate | Traffic Engineer

David A. Holt, PE (WA)
Transportation Engineer

Enclosure: Reduced Site Plan

c: John Bradner – Albany Fire Department
Lorraine Jack, Brian Varricchione – Mackenzie

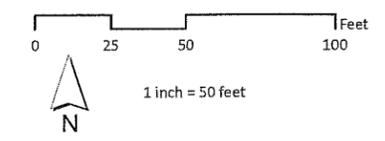
M.



Albany Fire Station #11
110 6th Avenue SW
Albany, Oregon

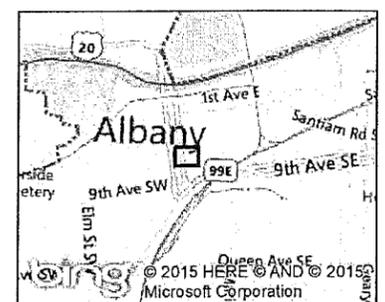
LEGEND

- Site Border
- Parcels
- Zoning**
- HM: Hackleman/Monteith (res.)
- LE: Lyon-Ellsworth (mixed use)



SOURCE DATA: Oregon Spatial Data Library, 2014
 GEOGRAPHIC PROJECTION: NAD 83 HARN, Oregon North Lambert Conformal Conic

Date: 8/20/2015 Map Created By: WGR
 File: site_21x17 Project No: 2140087.MK



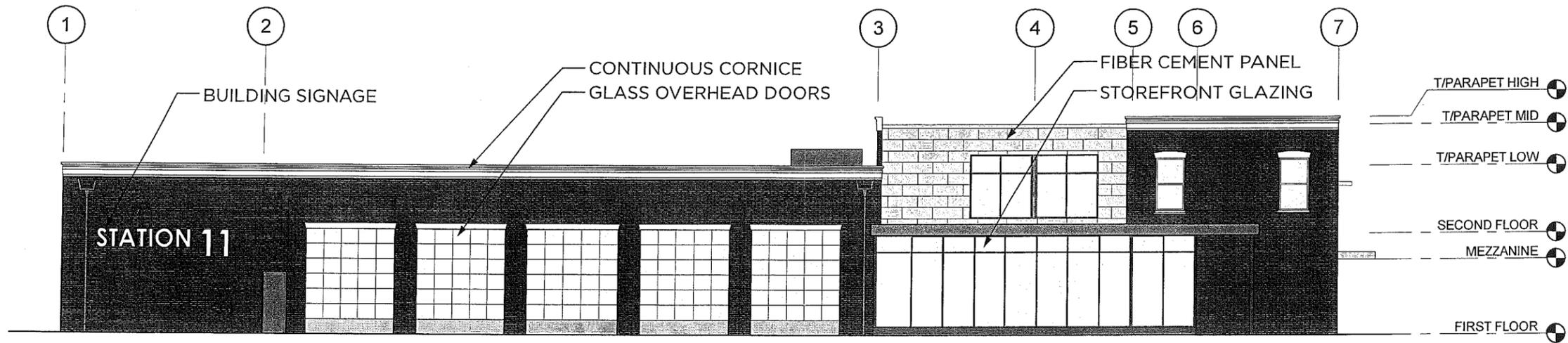
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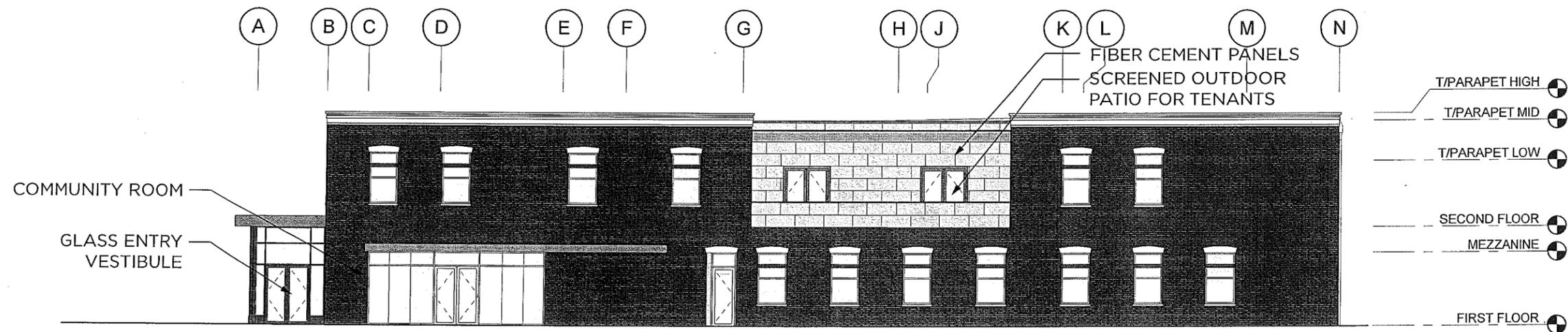
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SOUTH ELEVATION

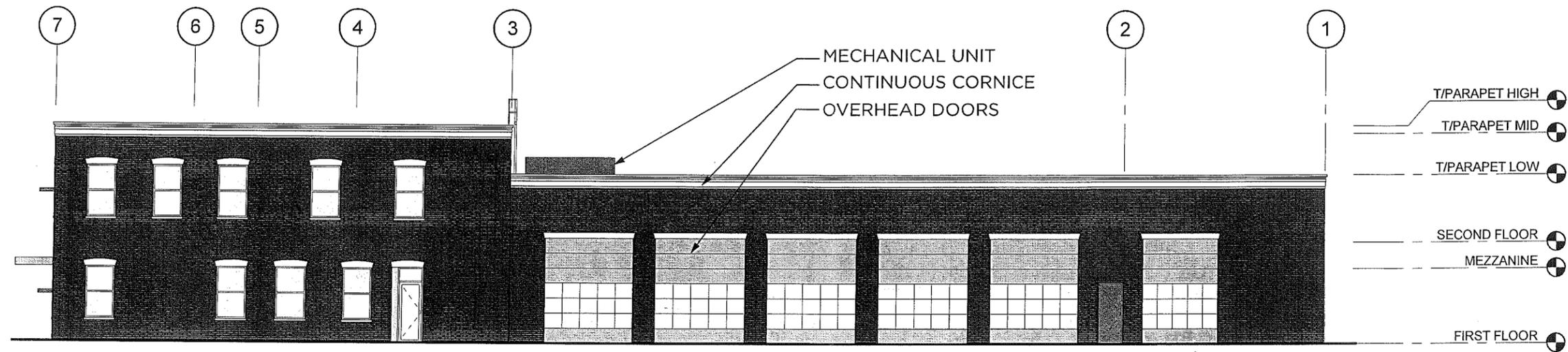


EAST ELEVATION

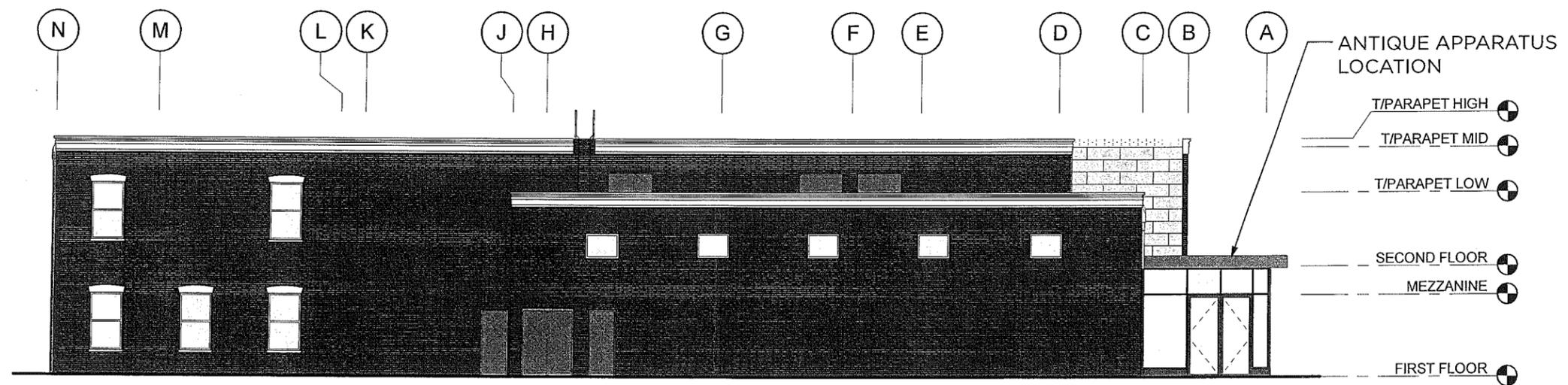


TRASH ENCLOSURE ELEVATIONS



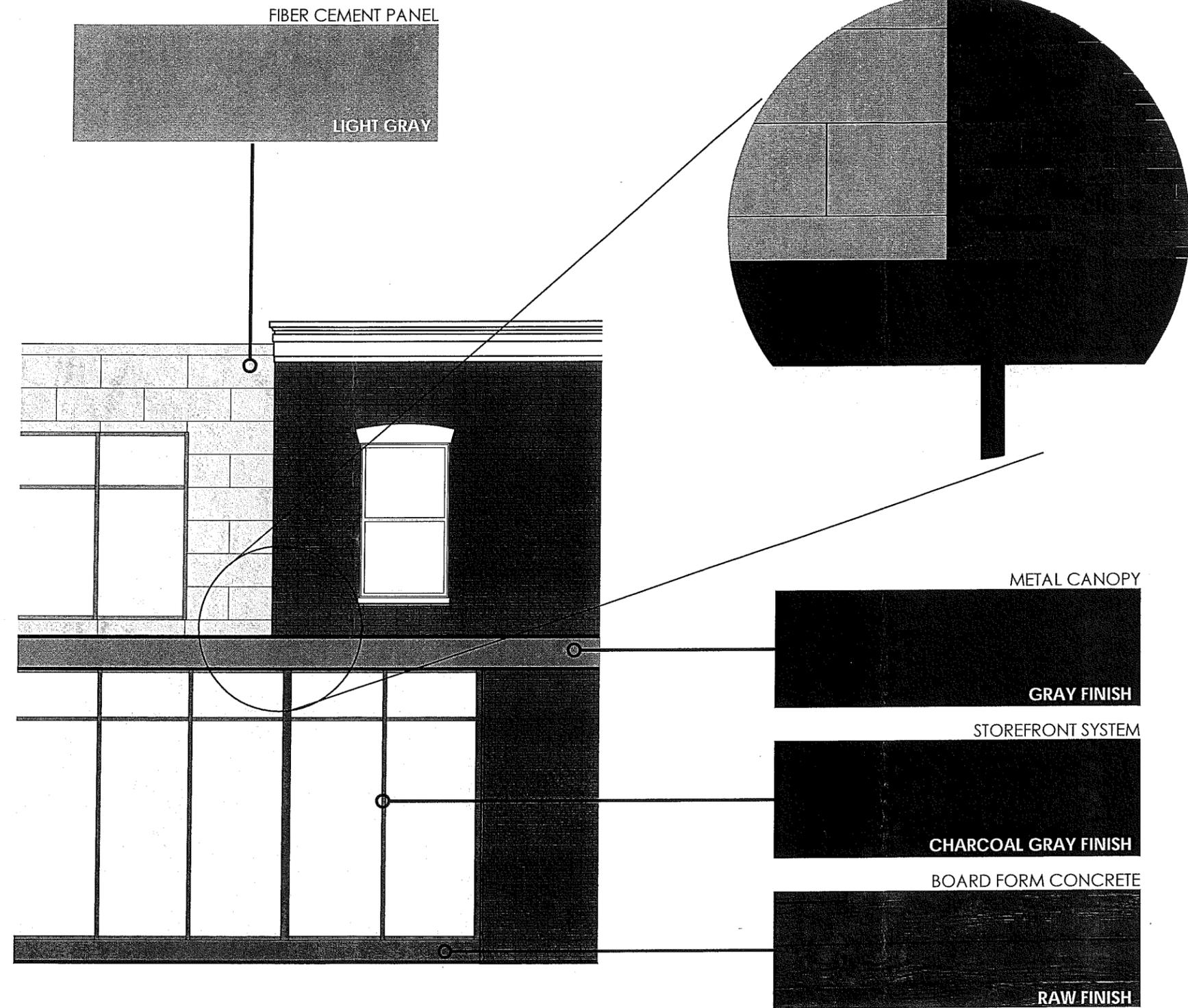
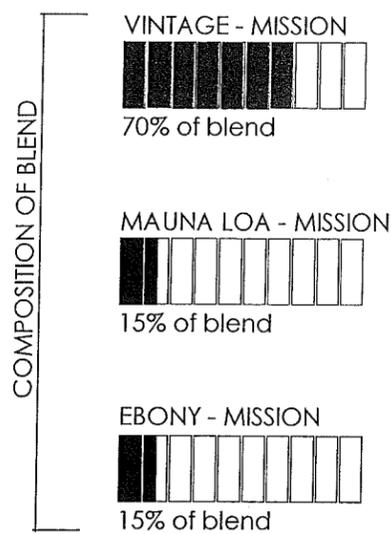
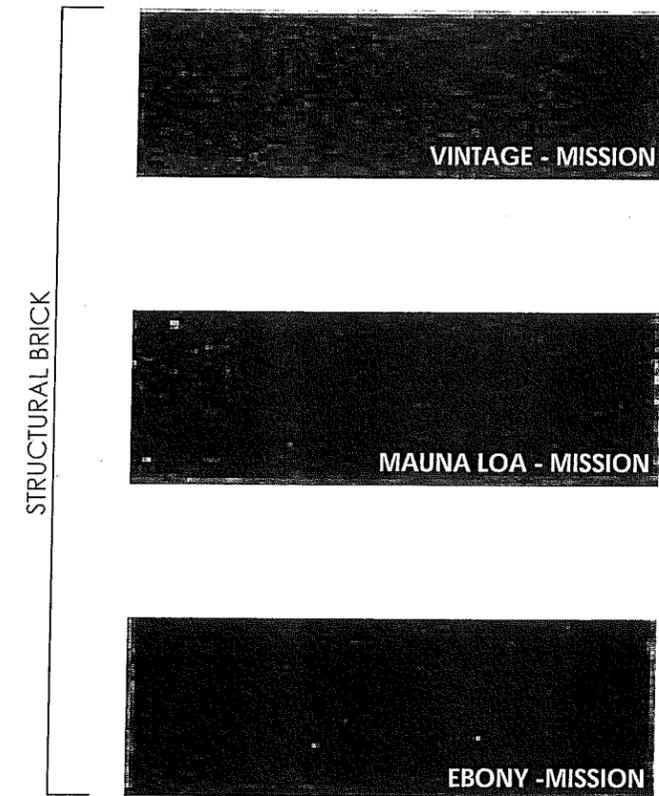


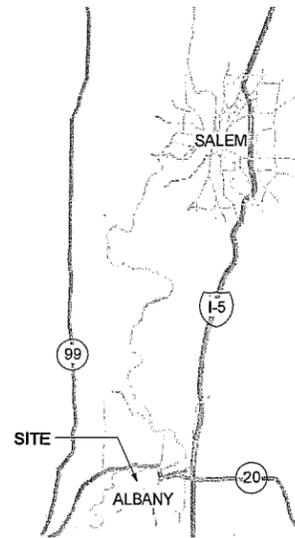
NORTH ELEVATION



WEST ELEVATION







SITE AREA MAP
NOT TO SCALE



VICINITY MAP
NOT TO SCALE



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Client

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FIRE DEPARTMENT

Project

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110 6th Avenue SE
Albany, OR 97321

Mechanical/Electrical
INTERFACE ENGINEERING, INC.
6542 SE Lake Road
Milwaukie, Oregon, 97222 -
2138

Phone: 503.659.6394
Fax: 503.659.9028

ALBANY FIRE DEPARTMENT - STATION 11

CONDITIONAL USE SUBMITTAL - 09/10/2015

CLIENT

CITY OF ALBANY FIRE DEPT.
333 BROADBLN ST SW
ALBANY, OR 97321
CITY PROJECT MANAGER: STACI BELCASTRO
OWNER REPRESENTATIVE: JEFF WOODLARD
CONTACT: JOHN BRADNER - FIRE CHIEF
LORRI HEADRICK - ADMIN
PHONE: 541.917.7751
EMAIL: john.brader@cityofalbany.net
lorri.headrick@cityofalbany.net

M/E/P

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ANDREW CHANG, PLUMBING
CHRIS LARSON, ELECTRICAL
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EMAIL: steve@interfaceeng.com
andrew@interfaceeng.com
chris@interfaceeng.com

DRAWING CRITERIA

- ALL DRAWINGS ARE IDENTIFIED BY TWO DIGITS AS FOLLOWS:
- CATEGORY LETTER REFERRING TO THE DISCIPLINE OR MAJOR DIVISION.
 - TITLE SHEET
 - CIVIL
 - LANDSCAPE
 - ARCHITECTURAL
 - STRUCTURAL
 - MECHANICAL
 - ELECTRICAL
 - PLUMBING
 - SUB-CATEGORY NUMBER REFERRING TO TYPE OF DRAWING OR GROUPING.
 - GENERAL PLANS
 - EXTERIOR ELEVATIONS/BUILDING SECTIONS
 - WALL SECTIONS
 - ENLARGED PLANS AND INTERIOR ELEVATIONS
 - REFLECTED CEILING PLANS
 - STAIR AND ELEVATOR SECTIONS, PLANS, AND DETAILS
 - DETAILS
 - SCHEDULES

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GENERAL CONTRACTOR

TBD

SITE INFORMATION

LEGAL DESCRIPTION: BLOCK 6 & 7, TAX LOTS 1003,
1001, 1000, 2000, 2100, & 2701
ADDRESS: FINAL TBD
CURRENT:
110 6TH AVE SE
ALBANY, OR 97321

DEFERRED SUBMITTALS

PER SECTION 107.4.2 DEFERRED SUBMITTALS: DOCUMENTS FOR DEFERRED SUBMITTAL ITEMS SHALL BE SUBMITTED TO THE REGISTERED DESIGN PROFESSIONAL IN CHARGE WHO SHALL REVIEW THEM AND FORWARD THEM TO THE BUILDING OFFICIAL WITH A NOTATION INDICATING THAT THE DEFERRED SUBMITTAL DOCUMENTS HAVE BEEN REVIEWED AND FOUND TO BE IN GENERAL CONFORMANCE TO THE DESIGN OF THE BUILDING. THE DEFERRED SUBMITTAL ITEMS SHALL NOT BE INSTALLED UNTIL THE DEFERRED SUBMITTAL DOCUMENTS HAVE BEEN APPROVED BY THE BUILDING OFFICIAL.

- FIRE SPRINKLER NFPA 13 SYSTEM
- FIRE ALARM SYSTEM
- ENGINEERED SUSPENDED ACoustICAL CEILING SYSTEM
- CARBON MONOXIDE DETECTION SYSTEM
- PROVIDE CALCULATIONS AND DETAILS FOR SEISMIC ANCHORAGE AND BRACING OF ALL MECHANICAL AND ELECTRICAL AND OTHER EQUIPMENT WEIGHING MORE THAN 400 LBS AND ATTACHED TO A FLOOR OR ROOF LEVEL OR WEIGHING MORE THAN 75 LBS AND ATTACHED MORE THAN 4" ABOVE THE FLOOR OR ROOF LEVEL.
- STOREFRONT SYSTEM DESIGN AND ATTACHMENT
- SKYLIGHT DESIGN AND ATTACHMENT

INDEX OF DRAWINGS

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- A2.3 ROOF PLAN
- A3.1 BUILDING ELEVATIONS

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Revision Schedule	
Revision Delta	Issue Date

SHEET TITLE:
**TITLE SHEET
AND DRAWING
INDEX**

DRAWN BY: LEJ
CHECKED BY: JRH
SHEET

T1.1

JOB NO. 2140087.02



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Fax: 503.659.6029

CODE ANALYSIS

GOVERNING CODES
BASED ON THE 2014 OREGON STRUCTURAL SPECIALTY CODE

BUILDING CONSTRUCTION DATA
CONSTRUCTION TYPE: V-8
FIRE PROTECTION: AUTOMATIC SPRINKLER SYSTEM PROVIDED THROUGHOUT
AUTOMATIC SMOKE DETECTION THROUGHOUT (SECTION 903.1.1)
OCCUPANCY: R-2 (LIVING QUARTERS)
S-2 (APPARATUS BAY)
B (OFFICE)
A-3 (MEETING ROOM)

*OCCUPANCY DETERMINED IN ACCORDANCE WITH STATEWIDE CODE INTERPRETATION NO. 04-01 OCCUPANCY CLASSIFICATIONS OF CLASSROOMS FOR FIRE STATIONS

ALLOWABLE AREAS AND HEIGHTS
*AREAS REFERENCED ARE FOR CODE ANALYSIS ONLY

FLOOR	AREA PROVIDED	SQUARE FEET
FIRST FLOOR	17,040 SF	
MEZZANINE	1,491 SF	
SECOND FLOOR	8,876 SF	
TOTAL BUILDING AREA:	25,918 SF	
EXTERIOR COVERED AREA:	110 SF	
TOTAL AREA PROVIDED:	26,028 SF	

*THE BUILDING CONSISTS OF (B), (R-2), AND (S-2) OCCUPANCIES. THE BUILDING HAS BEEN CALCULATED WITH THE MOST STRINGENT OCCUPANCY (R-2) BASED ON NON-SEPARATED OCCUPANCIES (SECTION 506.3) *MEZZANINE AREA NOT INCLUDED IN TOTAL BUILDING AREA PER SECTION 505.1

ALLOWABLE BUILDING AREA FORMULA (SECTION 506.3)

$A_n = 7,000 \text{ SF}$
 $F = 0.75$
 $L_s = 2$
 $A_n = A_1 + (A_2 \times L_1) + (A_3 \times L_2)$
 $A_n = 25,250 \text{ SF}$

TOTAL ALLOWABLE BUILDING AREA: 26,280 SF
TOTAL BUILDING AREA PROPOSED: 26,028 SF

FRONTAGE INCREASE CALCULATION (SECTION 506.3)

$F = 0.75$
 $P = 0.16$
 $W = 30.00$
 $F = (75 - 0.25) \text{ WID}$
 $F = 0.75$

BUILDING HEIGHT (SECTION 504.2)

ALLOWABLE: 60'-0" / 4 STORY
PROVIDED: 39'-0" / 3 STORY

FIRE RESISTANCE RATINGS REQUIREMENTS FOR BUILDING ELEMENTS

BUILDING FIRE RESISTIVE REQUIREMENTS (TABLE 601)

ELEMENT	REQUIREMENT
STRUCTURAL FRAME	NR
BEARING WALLS - EXTERIOR	NR
BEARING WALLS - INTERIOR	NR
EXTERIOR NON-BEARING WALLS (X ≥ 30)	NR (TABLE 602)
INTERIOR NON-BEARING WALLS	NR
FLOOR	NR
ROOF	NR
SHAFTS (503.4)	NR
STAIRS (1609.2, EXCEPTION 1)	NR

FIRE RESISTIVE RATING FOR EXTERIOR WALL BASED ON FIRE SEPARATION (TABLE 602)

> 3"

R-2 OCCUPANCY SEPARATION (DWELLING UNITS)

SEPARATION WALLS (SECTION 410.2)
* WALLS SEPARATING DWELLING UNITS IN THE SAME BUILDING. WALLS SEPARATING SLEEPING UNITS IN THE SAME BUILDING AND WALLS SEPARATING SLEEPING OR DWELLING UNITS FROM OTHER OCCUPANCIES CONTIGUOUS TO THEM IN THE SAME BUILDING SHALL BE CONSTRUCTED AS FIRE PARTITIONS IN ACCORDANCE WITH SECTION 705

FIRE PARTITIONS (SECTION 705.3, EXCEPTION 2 - FIRE RESISTANCE RATING)
* DWELLING UNIT AND SLEEPING UNIT SEPARATIONS IN BUILDINGS OF TYPE III, IIIA, AND VB CONSTRUCTION SHALL HAVE FIRE-RESISTANCE RATINGS OF NOT LESS THAN 1/2 HOUR IN BUILDINGS EQUIPPED THROUGHOUT WITH AN AUTOMATIC SPRINKLER SYSTEM IN ACCORDANCE WITH SECTION 903.3.1.1

OPENING PROTECTIVES (TABLE 716.5)

* FIRE DOOR AND FIRE SHUTTER ASSEMBLY RATING TO BE A MINIMUM 1/3 HR

FIRE EXTINGUISHERS

* FIRE EXTINGUISHERS WITH RATING OF NOT LESS THAN 2A-10-B PROVIDED FOR EACH 3,000 SF OF FLOOR AREA.

MAX SEPARATION = 75'-0"

OCCUPANCY LOAD

USE - 1600 - SEE PLAN FOR DEFINITION OF AREAS
OCCUPANT LOAD - TABLE 1004.1.1

AREA	OCCUPANCY	LOAD	OCCUPANCY	LOAD	REQUIRED	PROVIDED
APPARATUS BAY	S-2	200	47	1	1	1
MEZZANINE	B-2	200	6	1	1	1
FITNESS	B	50	11	1	1	1
OFFICE	B	60	154	1	1	1
COMMUNITY ROOM	A-3	15	72	2	2	2
LIVING QUARTERS	R-2	200	34	2	2	2
TOTAL GROSS AREA			25,809 SF			

PLUMBING FIXTURE CALCULATIONS TABLE 2902.1

OCCUPANCY	WATER FIXTURES				LAVATORIES				SHOWERS			
	U	F	M	R	U	F	M	R	U	F	M	R
APPARATUS BAY	42	21	21	0	0	0	0	0	0	0	0	0
OFFICE	165	83	83	1.25	2.68	2.68	1.50	2.0375	2.0375	NA	0	0
LIVING QUARTERS	34	17	17	1 PER 10	1.7	1.7	1 PER 10	1.7	1.7	1 PER 10	2.13	2.13
SUBTOTAL				4.36	4.36	4.36	4.2375	4.2375	4.2375		2.13	2.13
REQUIRED TOTALS:				5	5	5	5	5	5	3	3	3
PROVIDED TOTALS:				5	5	5	5	5	5	3	3	3

EXISTING

SECTION 1005 - EGRESS WIDTH
* WIDTH REQ'D PER TABLE 1006.1

STORAGE (S-2)	0.2 x 24 OCC	6.88 WIDE (30" MIN)
BUSINESS (B)	0.2 x 173 OCC	34.60 WIDE (30" MIN)
LIVING QUARTERS (R-2) (DWELLING UNITS)	0.2 x 36 OCC	7.20 WIDE (30" MIN)
ASSEMBLY (A-3)	0.2 x 71 OCC	21.3 WIDE (30" MIN)

SECTION 1006 - MEANS OF EGRESS ILLUMINATION
* MEANS OF EGRESS ILLUMINATION PROVIDED AT A MINIMUM OF ONE FOOT CANDLE AT PATH OF EGRESS SHOWN ON PLANS, TO MEET SECTION 1006 - SEE ELECTRICAL DRAWINGS

SECTION 1008 - DOORS, GATES, AND TURNSTILES
* DOORS
RATING SIZE AND HARDWARE PROVIDED TO MEET OCS SECTION 1008
SEE INDIVIDUAL FLOOR PLANS AND SPECIFICATIONS

SECTION 1014 - EXIT ACCESS
* ALL SPACES EXIT DIRECTLY TO THE EXTERIOR, THROUGH AN ENTRY FOYER OR THROUGH AN INTERVENING ROOM (SECTION 1014.2)

SECTION 1016 - EXIT ACCESS TRAVEL DISTANCE
* ALLOWABLE EXIT ACCESS TRAVEL DISTANCE (FULLY SPRINKLED BUILDING)
* R-2 OCCUPANCY: 400 FEET PROVIDED 106'-0"
* A-3 OCCUPANCY: 250 FEET PROVIDED 58'-0"
* B OCCUPANCY: 200 FEET PROVIDED 75'-0"
* R-2 OCCUPANCY: 250 FEET PROVIDED 93'-0"

* COMMON PATH OF EGRESS TRAVEL PER TABLE 1014.3 (FULLY SPRINKLED BUILDING)
* S-2 OCCUPANCY: 100 FEET PROVIDED
* A-3 OCCUPANCY: 75 FEET PROVIDED
* B OCCUPANCY: 100 FEET PROVIDED
* R-2 OCCUPANCY: 125 FEET PROVIDED

SECTION 1020 - EXITS
* COMPONENTS AND OPENINGS ARE SHOWN ON THIS SHEET, INDIVIDUAL FLOOR PLANS, AND IN THE SPECIFICATIONS

SECTION 1029 - EMERGENCY ESCAPE AND RESCUE
* EXTERIOR EMERGENCY ESCAPE PROVISION REQUIRED

* THE MINIMUM NET CLEAR OPENING FOR GRADE-LEVEL EMERGENCY ESCAPE OPENINGS SHALL BE 5.0 FT WITH A MINIMUM NET CLEAR HEIGHT DIMENSION OF 24 INCHES AND MINIMUM NET CLEAR WIDTH DIMENSION OF 20 INCHES. (SECTION 1029.2)

* MAXIMUM HEIGHT FROM FLOOR TO THE BOTTOM OF THE CLEAR OPENING MUST NOT MEASURE GREATER THAN 44 INCHES (SECTION 1029.3)

* SEE WINDOW SCHEDULE (SHEET A-2)

CHAPTER 11 - ACCESSIBLE ROUTE

SECTION 3011 - ACCESSIBILITY FOR EXISTING BUILDINGS
VALUATION OF WORK:
25% VALUATION OF UPGRADES:

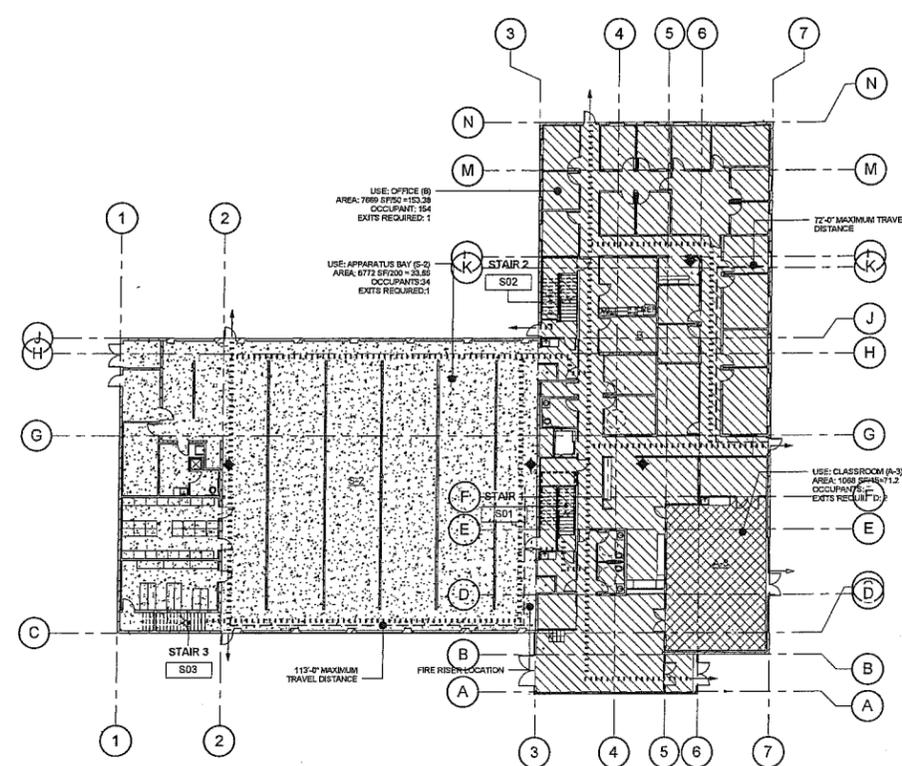
ADA UPGRADE PRIORITY (PER ORS 447.541)
PARKING
AN ACCESSIBLE ENTRANCE
AN ACCESSIBLE ROUTE TO THE ALTERED AREA
AT LEAST ONE ACCESSIBLE RESTROOM FOR EACH SEX OR A SINGLE UNISEX RESTROOM

GENERAL NOTES

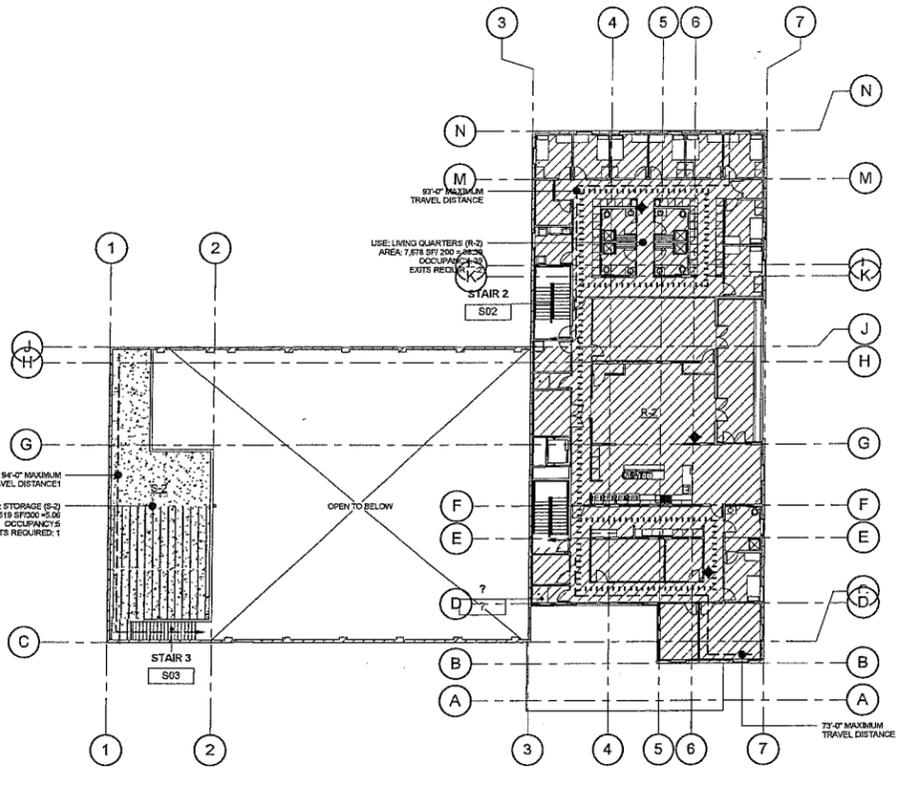
- A. SEE PLANS AND DETAILS FOR PARTITION TYPE AND CONSTRUCTION
- B. SEE INDIVIDUAL FLOOR PLANS FOR ADDITIONAL INFORMATION
- C. THIS ANALYSIS WAS BASED ON 2014 OREGON STRUCTURAL SPECIALTY CODE
- D. SEE ELECTRICAL DRAWINGS FOR EXT LIGHTING EMERGENCY POWER PROVIDED BY GENERATOR ON SITE, SEE ELECTRICAL DRAWINGS

LEGEND

- OFFICE (B)
- APPARATUS BAY, STORAGE (S-2)
- LIVING QUARTERS (R-2)
- CLASSROOM (A-3)
- FIRE EXTINGUISHER CABINET
- PRIMARY EXIT
- SECONDARY EXIT
- PATH OF EGRESS
- EXTERIOR BRICK VENEER / STRUCTURAL BRICK
- SHEAR WALL
- 1-HR RATED INTERIOR PARTITION
- INTERIOR PARTITION



1 FIRST FLOOR CODE ANALYSIS
1/16" = 1'-0"



2 SECOND FLOOR CODE ANALYSIS
1/16" = 1'-0"

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Revision	Schedule
Revision Date	Issue Date

SHEET TITLE:
CODE ANALYSIS

DRAWN BY: ILWLEJ

CHECKED BY: JRH

SHEET

T1.2

JOB NO. 2140087.02



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ALBANY, OR 97321

TOPOGRAPHIC SURVEY
FOR
ALBANY FIRE DEPARTMENT
LOCATED IN
NW 1/4 SEC. 7, T. 11 S., R. 3 W., W.M.
IN THE
CITY OF ALBANY, LINN COUNTY, OREGON
JUNE 26, 2015

HORIZONTAL DATUM:
THIS PROJECT IS ON A LOCAL DATUM PLAN. SEE COORDINATE TABLE FOR SURVEY CONTROL & BOUNDARY MONUMENTS. POINTS #50 AND #51 WERE HELD TO ESTABLISH THE BASIS OF BEARINGS PER C.S. 1999-5.

VERTICAL DATUM:
VERTICAL DATUM IS NOV. 1929 BASED ON CITY OF ALBANY OPS POINT #3260. SITE TBM IS SURVEY CONTROL POINT #201 AS SHOWN ON THE MAP.

- LEGEND:**
- FOUND MONUMENT, AS NOTED; SEE "FOUND MONUMENT LIST"
 - CALCULATED POINT FROM RECORD DATA
 - △ SURVEY CONTROL POINT
 - FOUND MONUMENT REFERENCE
 - [] CALCULATED DATA
 - () RECORD DATA
 - FB. FOUND
 - I.R. IRON ROD
 - I.P. IRON PIPE (INSIDE DIAMETER)
 - Y.P.C. YELLOW PLASTIC CAP
 - C.S. COUNTY SURVEY
 - S.F. SQUARE FEET
 - R-O-W RIGHT OF WAY
 - DATA PER CITY OF ALBANY GIS
 - PROJECT BOUNDARY
 - PROPERTY LINE
 - STREET CENTERLINE
 - EXIST. CURB/GUTTER WITH SIDEWALK
 - EXIST. WATER LINE
 - EXIST. SEWER LINE
 - EXIST. STORM DRAIN
 - P.U.E. OR OTHER EASEMENT
 - EXIST. FENCE
 - EXIST. TREE/VEGETATION
 - EXISTING FIRE HYDRANT
 - EXISTING WATER WALK
 - EXISTING WATER METER
 - EXISTING MONITORING WELL
 - EXIST. UTILITY POLE
 - EXIST. GUY WIRE ANCHOR
 - EXISTING STREET LIGHT
 - EXISTING MANHOLE
 - EXISTING CLEAN-OUT
 - EXISTING CATCH BASIN
 - EXISTING ELECTRICAL METER
 - EXISTING IRRIGATION VALVE
 - EXISTING NATURAL GAS METER
 - EXISTING FIRE DEPARTMENT CONNECTION
 - EXISTING TELEVISION PEDESTAL
 - EXISTING MAILBOX
 - EXISTING BASKETBALL HOOP

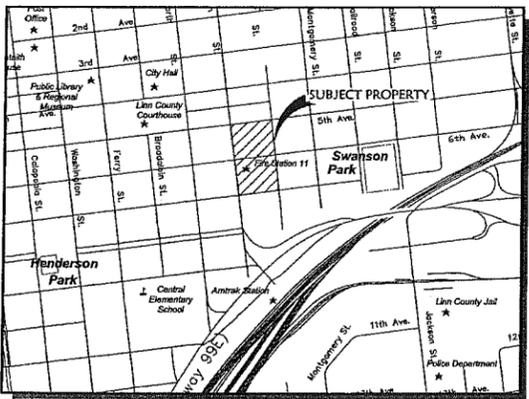
SURVEY CONTROL TABLE

Point #	Northing	Easting	Elevation	Description
1	5000.00	5000.00	211.01	cnt-spk
2	5168.04	4969.56	209.93	cnt-spk
3	5093.09	5068.95	211.49	cnt-mog
4	5021.21	5196.02	211.81	cnt-mog
5	5230.82	4985.99	209.96	cnt-mog
8	4715.99	5041.89	212.14	cnt spike
9	4848.73	5197.07	212.86	cnt spike
10	4696.93	5322.74	213.24	cnt pk
11	4785.06	5116.17	212.69	cnt moq
12	4832.77	5026.37	211.70	cnt bd
13	5167.98	4969.59	209.93	cnt spk
14	5036.55	5267.60	212.60	cnt ht
15	5244.77	5046.05	210.37	cnt ht
16	4690.49	5243.36	212.91	cnt moq
201	4723.12	5042.26	212.14	IBM i58 ypc

SURVEY MONUMENT TABLE

Point #	Northing	Easting	Description
50	5026.06	5133.83	lp34
51	5016.50	5067.56	lp34
52	5006.72	5001.14	lr12
53	5069.82	4988.99	lr12
54	5235.74	5035.74	lp34spk
55	5133.28	5101.12	lp34
56	5133.52	5117.94	lp34
57	4932.86	4945.20	lr58
58	4713.50	4978.04	pk
59	4657.80	5051.73	lr58
60	4695.88	5317.51	lr12
61	4746.06	5210.81	x
62	4781.10	5307.82	lr12
63	4740.98	5175.06	lr12
64	4808.55	5210.04	lr58
65	4858.04	5203.58	ball34
66	4807.23	5202.41	lr12

REGISTERED PROFESSIONAL LAND SURVEYOR
JOE J. COSTA
12/31/15



Date: 6/26/2015 Time: 12:50
Scale: 1"=100'
File: dwg\2014\14-02-014-02_topo.dwg (Brian)
K & D ENGINEERING, Inc.
276 S.W. Mainway Street, 2nd Floor, 97321
Albany, Oregon (503) 868-5220

DRAWN BY:
CHECKED BY:
SHEET

C11

JOB NO. 2140087.02



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Project
ALBANY FIRE STATION
110 6TH AVENUE SE
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REVISIONS:
DATE REVISIONS REVISION DATE
BY SHEET

SHEET TITLE:
**DEMOLITION
PLAN**

DRAWN BY: BTS

CHECKED BY: RVS

SHEET

C2.0

JOB NO. 2140087.02

LEGEND:

- REMOVE EXISTING CONCRETE
- SAW-CUT
- EXISTING CURB
- REMOVE EXISTING STORM
- REMOVE EXISTING SANITARY SEWER
- REMOVE EXISTING WATERLINE
- REMOVE EXISTING OVERHEAD POWER
- REMOVE EXISTING GAS
- REMOVE EXISTING CATCH BASIN
- EXISTING STREET LIGHT TO REMAIN
- REMOVE EXISTING WATER METER
- REMOVE EXISTING GAS METER
- REMOVE EXISTING ELECTRIC METER

GENERAL NOTES

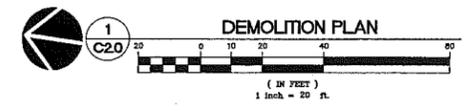
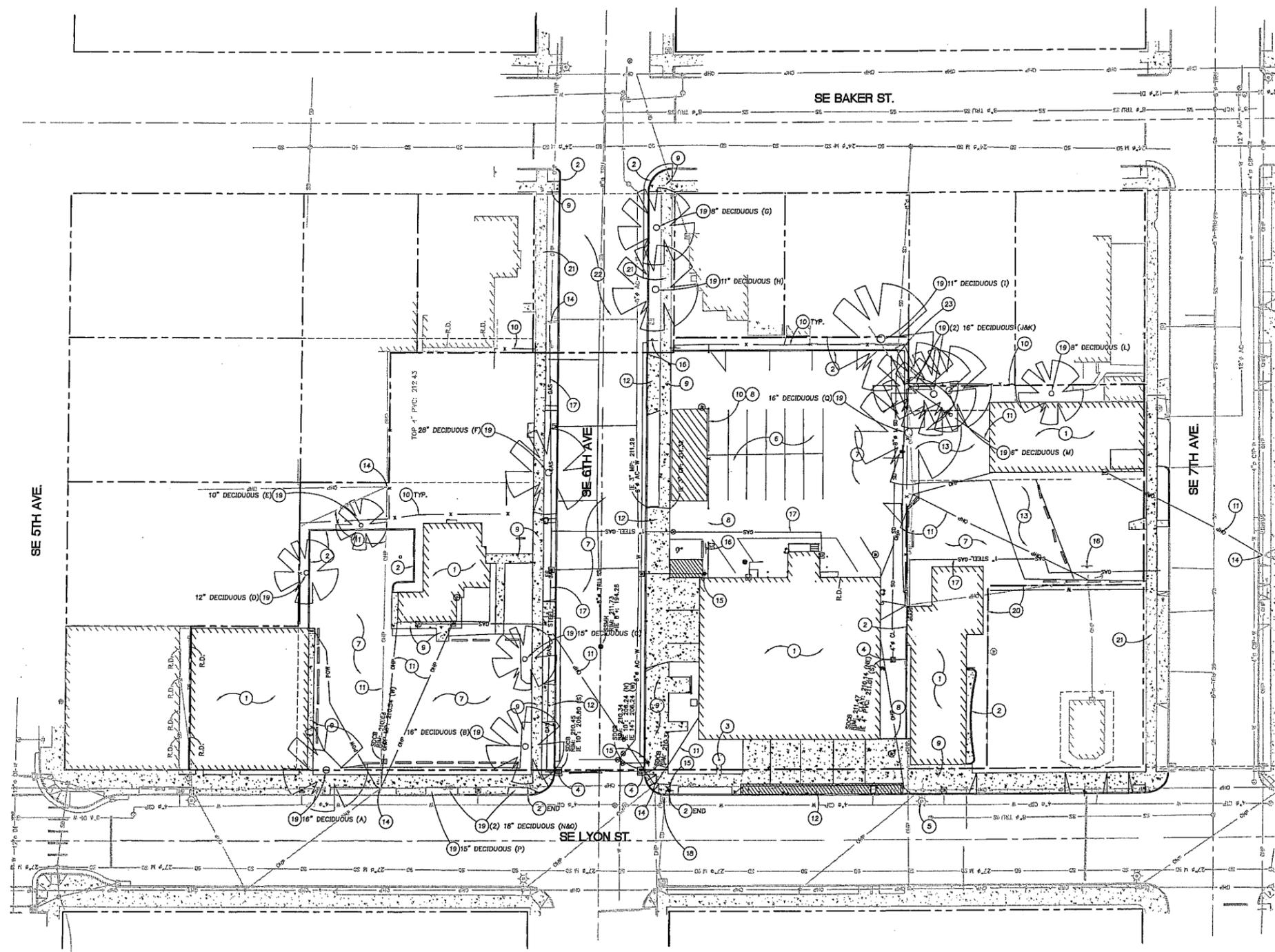
1. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ANY HAZARDOUS MATERIALS IN ACCORDANCE WITH DEQ REQUIREMENTS IF ANY SUCH MATERIALS ARE DISCOVERED ON SITE.
2. INSTALL EROSION CONTROL MEASURES AND TEMPORARY FENCING PRIOR TO DEMOLITION.
3. COORDINATE DEMOLITION WORK WITH PROPOSED OVERALL SITE PLAN.
4. CONTRACTOR SHALL VERIFY UTILITIES (GAS, ELECTRIC, ETC.) ARE PROPERLY TURNED OFF PRIOR TO DEMOLITION OF BUILDINGS. COORDINATE WITH FRANCHISE UTILITY PROVIDER PRIOR TO ANY DEMOLITION ACTIVITIES.

KEYNOTES:

1. REMOVE EXISTING STRUCTURE
2. REMOVE EXISTING CURB
3. REMOVE EXISTING FLAG POLE AND BASE
4. REMOVE EXISTING CATCH BASIN
5. EXISTING LIGHT POLE TO REMAIN
6. EXISTING PARKING TO BE REMOVED
7. REMOVE EXISTING AC AND BASE ROCK
8. REMOVE EXISTING GATE
9. REMOVE EXISTING SIDEWALK
10. REMOVE EXISTING FENCE
11. REMOVE/RELOCATE EXISTING OVERHEAD POWER LINES. COORDINATE WITH FRANCHISE UTILITY PROVIDER FOR RELOCATIONS OF OVERHEAD POWER SUPPLY TO ADJACENT PROPERTIES
12. REMOVE EXISTING DRIVEWAY APRON
13. REMOVE EXISTING GRAVEL
14. EXISTING UTILITY POLE TO REMAIN, PROTECT DURING DEMOLITION AND CONSTRUCTION
15. REMOVE EXISTING FIRE HYDRANT AND SUPPLY LINE
16. REMOVE EXISTING STREET SIGN
17. REMOVE EXISTING GAS LINE, COORDINATE WITH FRANCHISE UTILITY PROVIDER
18. REMOVE EXISTING CATCH BASIN AND REPLACE WITH NEW CLEANOUT, SEE SHEET C2.3
19. REMOVE EXISTING TREE
20. SAWCUT EXISTING AC AT PROPERTY LINE
21. EXISTING SIDEWALK TO REMAIN
22. PORTION OF SE 6TH AVE. TO BE REPAVED, SEE SITE PLAN C2.1
23. REMOVE EXISTING STORM LINE TO 3' FROM PROPERTY LINE AND CAP AND PLUG

TREE INVENTORY:

TREE ID #	TYPE	TRUNK DIAMETER	CANOPY (S.F.)	REMOVE/RETAIN	ON-SITE /ROW
A	DECIDUOUS	16" DIAMETER	1,256 S.F.	REMOVE (DEAD)	ON-SITE
B	DECIDUOUS	16" DIAMETER	1,017 S.F.	REMOVE	ON-SITE
C	DECIDUOUS	15" DIAMETER	706 S.F.	REMOVE	ON-SITE
D	DECIDUOUS	12" DIAMETER	706 S.F.	REMOVE	ON-SITE
E	DECIDUOUS	10" DIAMETER	452 S.F.	REMOVE	ON-SITE
F	DECIDUOUS	28" DIAMETER	1,256 S.F.	REMOVE	ON-SITE
G	DECIDUOUS	8" DIAMETER	1,017 S.F.	REMOVE	ROW
H	DECIDUOUS	11" DIAMETER	1,256 S.F.	REMOVE	ROW
I	DECIDUOUS	11" DIAMETER	1,863 S.F.	REMOVE	ON-SITE
J	DECIDUOUS	16" DIAMETER	1,256 S.F.	REMOVE	ON-SITE
K	DECIDUOUS	16" DIAMETER	1,256 S.F.	REMOVE	ON-SITE
L	DECIDUOUS	8" DIAMETER	706 S.F.	REMOVE	ON-SITE
M	DECIDUOUS	6" DIAMETER	1,256 S.F.	REMOVE	ON-SITE
N	DECIDUOUS	18" DIAMETER	804 S.F.	REMOVE	ROW
O	DECIDUOUS	18" DIAMETER	452 S.F.	REMOVE	ROW
P	DECIDUOUS	15" DIAMETER	1,017 S.F.	REMOVE	ROW
Q	DECIDUOUS	16" DIAMETER	1,809 S.F.	REMOVE	ON-SITE





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DEPT.
333 BROADALBIN ST. SW
ALBANY, OR 97321



FIRE DEPARTMENT

Project:
ALBANY FIRE STATION
110 6TH AVENUE SE
ALBANY, OR 97321

GENERAL NOTES

- ALL WORK SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF ALBANY AND THE CURRENT AMERICAN PUBLIC WORKS ASSOCIATION STANDARDS FOR PUBLIC WORKS CONSTRUCTION.
- THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIECE, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES LAYING WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO CONDITION, SIZE AND LOCATION BY UNCOVERING, PROVIDING SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR TO NOTIFY ENGINEER IF THERE ARE ANY DISCREPANCIES.
- EFFECTIVE EROSION PREVENTION AND SEDIMENT CONTROL IS REQUIRED. EROSION CONTROL DEVICES MUST BE INSTALLED AND MAINTAINED TO MEET CITY OF ALBANY REQUIREMENTS. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE EROSION CONTROL.
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE ROUTED SO THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- CONTRACTOR SHALL ADJUST ALL STRUCTURES IMPACTED BY CONSTRUCTION IMPROVEMENTS TO NEW FINISH GRADES.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) MUST COMPLY WITH O.R.S. 727.541 THROUGH 727.571; EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS SEVENTY-TWO (72) HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE. (OREGON UTILITY NOTIFICATION CENTER: 1-800-332-2844).
- WHERE CONNECTING TO AN EXISTING PIPE AND PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXPOSE THE END OF THE EXISTING PIPE VERIFY THE LOCATION, SIZE, AND ELEVATION. NOTIFY ENGINEER OF ANY DISCREPANCIES.
- REQUEST BY THE CONTRACTOR FOR CHANGES TO THE PLANS MUST BE APPROVED BY THE ENGINEER.
- ALL CURB RADI ARE 3.0' UNLESS OTHERWISE NOTED.

LEGEND

- PROPERTY LINE _____
- EXISTING EASEMENT _____
- VERTICAL CURB _____
- EDGE OF PAVEMENT _____

PAVEMENT LEGEND

- CONCRETE APPARATUS APRON
9" CONCRETE OVER 18" CRUSHED
ROCK BASE W/ #4 BARS @ 18" O.C.
EACH WAY
- 5" ASPHALT OVER 12" AGGREGATE BASE
- 3" ASPHALT OVER 9" AGGREGATE BASE

KEYNOTES

- VERTICAL CURB PER DETAIL 1/CB.0
- CONCRETE SIDEWALK PER LANDSCAPE DRAWINGS
- 4" WIDE WHITE PARKING STRIPE
- ADA COMPLIANT PARKING STALL PER DETAIL 3/CB.0
- PUBLIC SIDEWALK, CURBS, AND DRIVEWAYS PER SHEET R1.0
- LANDSCAPE AREA
- CURB STOP PER DETAIL 2/CB.0
- CONCRETE APPARATUS APRON: 9" CONCRETE OVER 18" CRUSHED ROCK BASE WITH #4 BARS @ 18" O.C. EACH WAY
- AC PAVEMENT PER PAVEMENT LEGEND
- FENCE PER LANDSCAPE DRAWINGS
- TRASH ENCLOSURE PER ARCHITECTURAL DRAWINGS
- PLAZA AND PARKING AREA, SEE LANDSCAPE DRAWINGS FOR PROPOSED PAVEMENT TYPES
- PATIO AREA PER LANDSCAPE DRAWINGS
- 2" CURB BREAK PER DETAIL 7/CB.0. PROVIDE 4"-6" WASHED RIVER ROCK PAD, 12" THICK, AS SHOWN
- BIKE RACK PER LANDSCAPE DRAWINGS
- 2" MAX ROCK WALL PER DETAIL 11/CB.0
- EXISTING SIDEWALK TO REMAIN
- 6'x6' TRANSFORMER PAD. PROVIDE 6" CONCRETE OVER 8" CRUSHED ROCK. CONFIRM SIZE REQUIREMENTS WITH MEP DRAWINGS PRIOR TO CONSTRUCTION
- 10'x6' GENERATOR PAD. PROVIDE 6" CONCRETE OVER 8" CRUSHED ROCK. CONFIRM SIZE REQUIREMENTS WITH MEP DRAWINGS PRIOR TO CONSTRUCTION

SITE DATA

TOTAL SITE	
SITE AREA	70,925 SF (1.63 AC)
BUILDING FOOTPRINT	17,530 SF (0.40 AC)
PAVED AREA	31,204 SF (0.72 AC)
LANDSCAPE AREA	22,171 SF (0.51 AC) (31.3%)
ZONE HM	
SITE AREA	24,405 SF (0.56 AC)
BUILDING FOOTPRINT	2,150 SF (0.05 AC)
PAVED AREA	12,903 SF (0.27 AC)
LANDSCAPE AREA	10,252 SF (0.24 AC) (42.0%)
ZONE LE	
SITE AREA	46,520 SF (1.07 AC)
BUILDING FOOTPRINT	15,400 SF (0.35 AC)
PAVED AREA	19,201 SF (0.44 AC)
LANDSCAPE AREA	11,819 SF (0.28 AC) (25.6%)

PARKING DATA

PUBLIC		BIKE PARKING	
STANDARD	26 SPACES	PUBLIC	12 SPACES
ACCESSIBLE	2 SPACES		
FIRE PARKING			
STANDARD	18 SPACES		
CARPPOOL	1 SPACE		

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REVISIONS:
REV. NO. REVISIONS
DATE

SHEET TITLE:
SITE PLAN

DRAWN BY: BTS

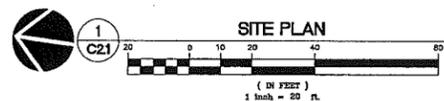
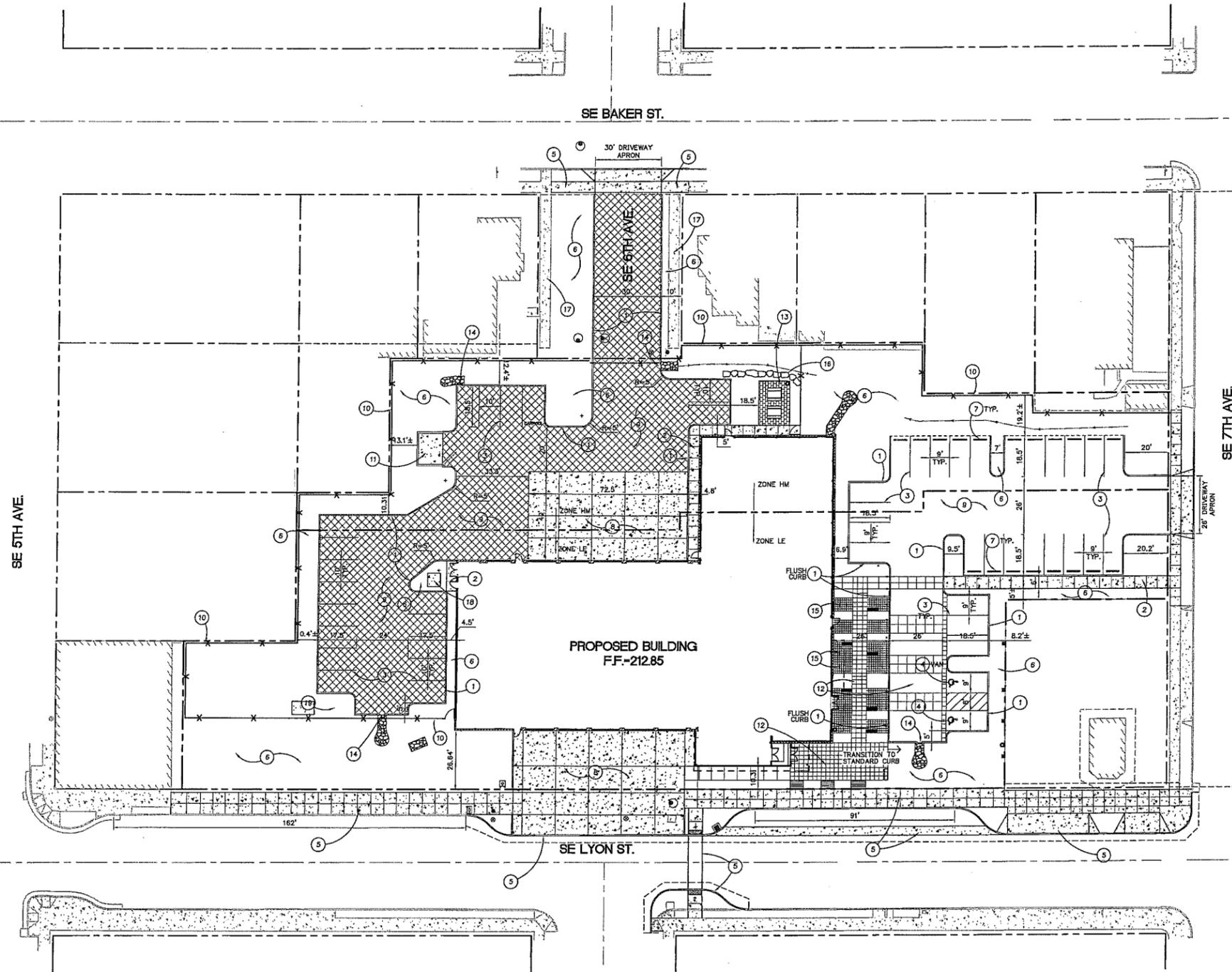
CHECKED BY: RVS

SHEET

C2.1

JOB NO. 2140087.02

REVISED: 10/09/15
DESIGN DEVELOPMENT SET: 10/13/15



SITE PLAN

(IN FEET)
1 inch = 20 ft.



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FIRE DEPARTMENT

Project
ALBANY FIRE STATION

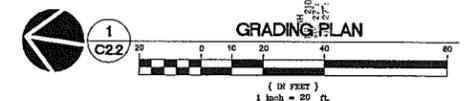
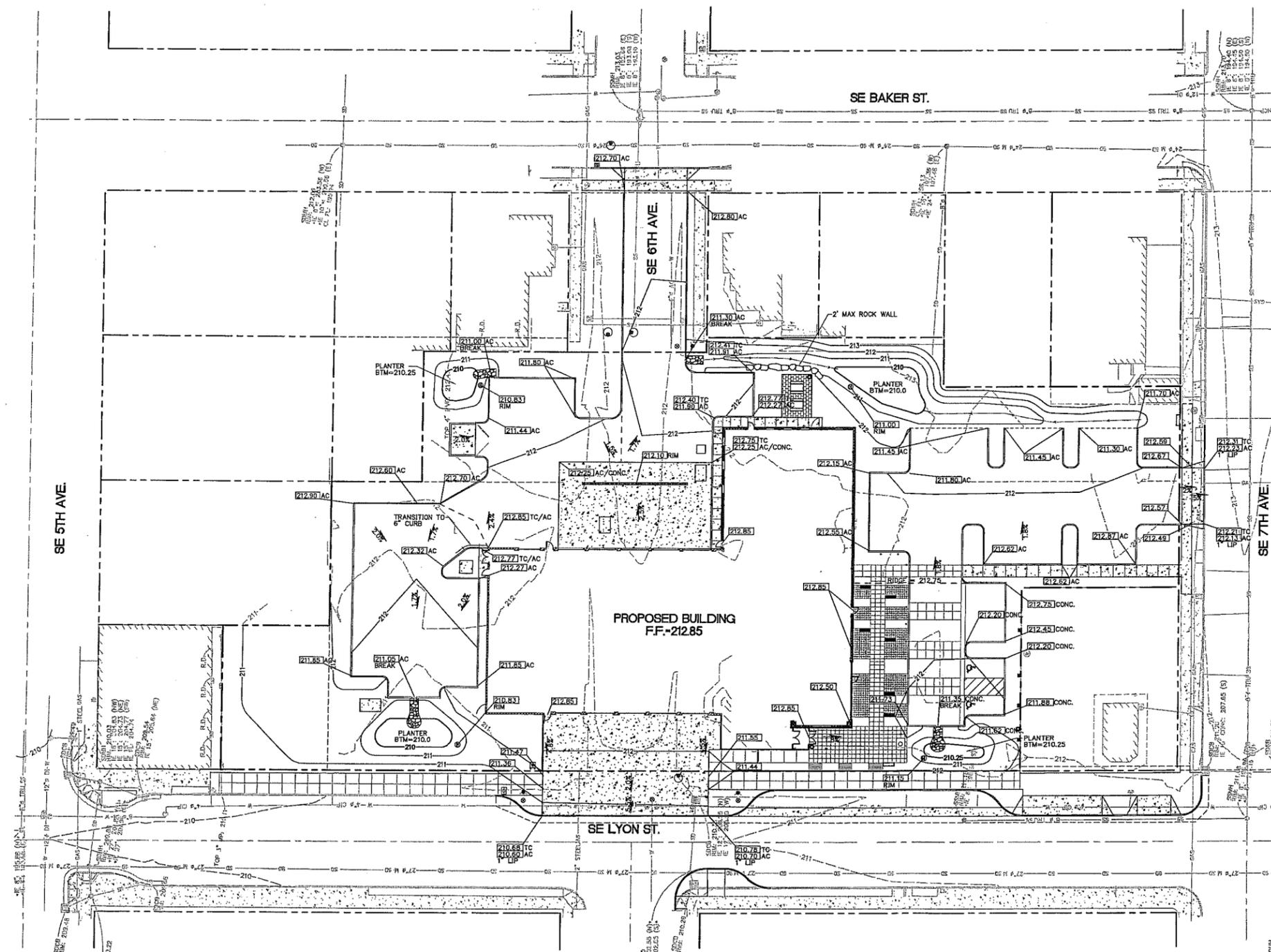
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ALBANY, OR 97321

GRADING NOTES

- ROUGH GRADING: BRING ALL FINISH GRADES TO APPROXIMATE LEVELS INDICATED. WHERE GRADES ARE NOT OTHERWISE INDICATED, FINISH GRADES ARE TO BE THE SAME AS ADJACENT SIDEWALKS, CURBS, OR THE OBVIOUS GRADE OF ADJACENT STRUCTURE. GRADE TO UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE GRADES ARE GIVEN. ROUND OFF SURFACES. AVOID ABRUPT CHANGES IN LEVELS. ROUGH GRADE TO ALLOW FOR DEPTH OF CONCRETE SLABS, WALKS, AND THEIR BASE COURSES. GRADE FOR PAVED DRIVES AND PAVED PARKING AREAS AS INDICATED AND SPECIFIED HEREIN, AND PROVIDE FOR SURFACE DRAINAGE AS SHOWN, ALLOWING FOR THICKNESS OF SURFACING MATERIAL.
- FINISH GRADING: AT COMPLETION OF JOB AND AFTER BACKFILLING BY OTHER CRAFTS HAS BEEN COMPLETED, REFILL AND COMPACT AREAS WHICH HAVE SETTLED OR ERODED TO BRING TO FINAL GRADES. GRADING TOLERANCES:
ROUGH GRADE AT PAVED OR LANDSCAPED AREAS: ±0.1 FT. FINISH GRADE PRIOR TO PLACING FINAL SURFACING: ±0.03 FT.
- EXCAVATION: EXCAVATE FOR SLABS, PAVING, AND OTHER IMPROVEMENTS TO SIZES AND LEVELS SHOWN OR REQUIRED. ALLOW FOR FORM CLEARANCE AND FOR PROPER COMPACTION OF REQUIRED BACKFILLING MATERIAL. EXCAVATOR(S) MUST COMPLY WITH O.R.S. 757.541 THROUGH 757.571. EXCAVATOR(S) SHALL NOTIFY ALL UTILITY COMPANIES FOR LINE LOCATIONS 72 HOURS (MINIMUM) PRIOR TO START OF WORK. DAMAGE TO UTILITIES SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE.
- EFFECTIVE EROSION PREVENTION AND SEDIMENT CONTROL IS REQUIRED. EROSION CONTROL DEVICES MUST BE INSTALLED AND MAINTAINED MEETING THE CITY OF ALBANY REQUIREMENTS. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE EROSION CONTROL.
- EFFECTIVE DRAINAGE CONTROL IS REQUIRED. DRAINAGE SHALL BE CONTROLLED WITHIN THE WORK SITE AND SHALL BE SO ROUTED THAT ADJACENT PRIVATE PROPERTY, PUBLIC PROPERTY, AND THE RECEIVING SYSTEM ARE NOT ADVERSELY IMPACTED. THE GOVERNING JURISDICTION MAY, AT ANY TIME, ORDER CORRECTIVE ACTION AND STOPPAGE OF WORK TO ACCOMPLISH EFFECTIVE DRAINAGE CONTROL.
- SITE TOPSOIL SHALL BE STOCKPILED DURING CONSTRUCTION AND USED FOR LANDSCAPING.
- THE SURVEY INFORMATION SHOWN AS A BACKGROUND SCREEN ON THIS SHEET IS BASED ON A SURVEY BY K&D ENGINEERING, INC. AND IS SHOWN FOR REFERENCE ONLY. CONTRACTOR TO VERIFY ALL EXISTING CONDITIONS WITH HIS OWN RESOURCES PRIOR TO START OF ANY CONSTRUCTION.
- CONTRACTOR TO COORDINATE GRADES AT ENTRANCE WITH ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.
- 2% MAXIMUM SLOPE AT ALL ADA-COMPLIANT PARKING SPACES AND LOADING ZONES.
- 5% MAX SLOPE (EXCLUDING RAMPS) AT PEDESTRIAN SIDEWALK CONNECTIONS BETWEEN PUBLIC R.O.W. AND BUILDING ENTRANCES.
- WHERE SLOPES ARE STEEPER THAN 3:1, CONTRACTOR SHALL INSTALL JUTE MATTING. SLOPE SHALL BE PREPARED TO ENSURE COMPLETE AND DIRECT CONTACT OF MATTING WITH SOIL. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

LEGEND

PROPERTY LINE	---
EXISTING CONTOUR	--- 214 ---
PROPOSED 1-FT CONTOUR	--- 214 ---
PROPOSED 5-FT CONTOUR	--- 215 ---
EXISTING EASEMENT	---
VERTICAL CURB	---



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REVISIONS:

NO.	DATE	REVISION	BY	DATE

SHEET TITLE:
GRADING PLAN

DRAWN BY: BTS
CHECKED BY: RYS
SHEET

C2.2

JOB NO. 2140087.02



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ALBANY, OR 97321



Project:
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ALBANY, OR 97321

UTILITY NOTES

1. ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF CITY OF ALBANY AND THE CURRENT EDITION OF THE UNIFORM PLUMBING CODE AND THE INTERNATIONAL BUILDING CODE. ALL WORK WITHIN THE PUBLIC R.O.W. REQUIRES A PUBLIC WORKS PERMIT.
2. THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIECE, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES LAYING WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO CONDITION, SIZE AND LOCATION BY UNCOVERING, PROVIDING SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR TO NOTIFY ENGINEER IF THERE ARE ANY DISCREPANCIES.
3. PROVIDE CLEANOUTS AS REQUIRED IN THE CURRENT UNIFORM PLUMBING CODE CHAPTER 7, SECTIONS 707 AND 719, AND CHAPTER 11, SECTION 1101.12. NOTE: NOT ALL REQUIRED CLEANOUTS ARE SHOWN ON THE PLANS.
4. ALL STORM PIPING IS SIZED FOR A MANNING'S "N" VALUE = 0.013. ALL STORM PIPING IS DESIGNED USING CONCENTRIC PIPE TO PIPE AND WYE FITTINGS, UNLESS OTHERWISE NOTED.
5. SEE MECHANICAL DRAWINGS FOR UTILITIES LOCATED WITHIN THE BUILDING AND TO 5' OUTSIDE THE BUILDING.
6. ALL DOWNSPOUT LEADERS TO BE 6" AT 2.0X MIN.
7. UNLESS NOTED OTHERWISE, VERIFY LOCATION, SIZE AND DEPTH OF EXISTING UTILITIES BY POT-HOLING PRIOR TO CONSTRUCTION. NOTIFY ENGINEER OF DISCREPANCIES.
8. PROVIDE 2" PVC DRAIN LINE FROM DOMESTIC WATER METER VAULT AND BACKFLOW PREVENTER VAULT TO THE DOUBLE DETECTOR CHECK VALVE (FIRE) VAULT. PROVIDE 1/3 HP SUMP PUMP AT BASE OF FIRE VAULT AND INSTALL 2" PVC DRAIN LINE WITH BACKFLOW VALVE FROM SUMP PUMP TO DAYLIGHT AT NEAREST CURB. FURNISH 25/38 INCH DIAMETER CONDUIT FROM BUILDING ELECTRICAL ROOM TO FIRE VAULT FOR SUMP PUMP ELECTRICAL SERVICE. NOTE: COORDINATE WITH FIRE PROTECTION CONTRACTOR FOR FLOW SENSOR INSTALLATION AND CONDUIT REQUIREMENTS.
9. THE SURVEY INFORMATION SHOWN AS A BACKGROUND SCREEN ON THIS SHEET IS BASED ON A SURVEY PREPARED BY K&O ENGINEERS, INC., DATED JUNE 26, 2015.
10. CONTRACTOR TO PROVIDE POWER TO IRRIGATION CONTROLLER. SEE SPECIFICATIONS AND LANDSCAPE PLANS.
11. SEE BUILDING PLUMBING DRAWINGS FOR PIPING WITHIN THE BUILDING AND UP TO 5' OUTSIDE THE BUILDING, INCLUDING ANY FOUNDATION DRAINAGE PIPING.
12. CONTRACTOR TO MAINTAIN MINIMUM 3 FT OF COVER OVER ALL WATER LINE.

STORM DIVERSION + ISOLATION NOTES

1. INTERCEPTOR DRAIN SHALL BE PLUMBED TO DRAIN TO SANITARY SEWER AND THE STORM SYSTEM THROUGH THE ISOLATION VALVE. UNDER TYPICAL CONDITION THE STORM VALVE SHALL BE IN THE OPEN POSITION. DURING WASHING OF EQUIPMENT OR VEHICLES, THE STORM VALVE SHALL BE AUTOMATICALLY MOVED INTO THE CLOSED POSITION AND THE VALVE TO THE SANITARY TO THE OPEN POSITION. VALVE AUTOMATION SHALL BE CONTROLLED BY FLOW SWITCH IN ADJACENT HOSE BIB. COORDINATE LOCATION OF FLOW SWITCH WITH OWNER. USE VSI (VALVE SOLUTIONS INCORPORATED) SERIES 1000 ELECTRIC ACTUATOR. VALVE SHALL BE VSI 2000 SERIES 3 WAY BUTTERFLY VALVE OR APPROVED EQUAL. INSTALL VALVE IN UN-444A VAULT WITH A TRAFFIC-RATED LID. SEE NOTES BELOW FOR SPECIFIC SYSTEM CONTROL.
- CONTROL NOTES:**
2. INSTALL FLOW SWITCH ON LINE TO HOSE BIB USED FOR TRUCK WASHING. FLOW SWITCH SHALL BE CONNECTED TO A TIMER.
 3. WHEN WATER IS ON SWITCH WILL SENSE FLOW AND ACTUATE VALVE TO CLOSE STORM AND OPEN SANITARY LINE.
 4. TIMER SHALL BE SET SUCH THAT FOLLOWING 20 MINUTES OF NO FLOW IN LINE TO HOSE BIB, THE ACTUATOR SHALL SWITCH THE DIVERTER VALVE BACK, THUS ALLOWING FLOW TO STORM SYSTEM AND CLOSING FLOW TO SANITARY SEWER LINE.
 5. 120V POWER WILL NEED TO BE PROVIDED TO DIVERTER VALVE TO OPERATE ACTUATOR.

LEGEND

	EXISTING	PROPOSED
PROPERTY LINE	---	---
1-FT CONTOUR	--- 214 ---	--- 214 ---
5-FT CONTOUR	--- 215 ---	--- 215 ---
EASEMENT	---	---
VERTICAL CURB	---	---
STORM	---	---
SANITARY SEWER	---	---
FIRE WATER (FW)	---	---
DOMESTIC WATER (DW)	---	---
OVERHEAD POWER	---	---
GAS	---	---
STORM MANHOLE	⊙	⊙
STORM CATCH BASIN	⊞	⊞
LIGHT	⊕	⊕

STORMWATER PLANTER SIZING TABLE

STORMWATER PLANTER	BOTTOM AREA (SF)	BOTTOM ELEVATION	SIZING FACTOR	IMPERVIOUS AREA TREATED (SF)	MAXIMUM IMPERVIOUS AREA ALLOWED (SF)
SP-1	428	210.00	0.018	13,900	20,000
SP-2	157	210.25	0.018	6,375	8,722
SP-3	557	210.00	0.018	18,840	20,000
SP-4	211	210.00	0.018	3,870	11,722

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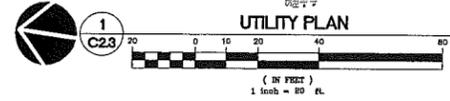
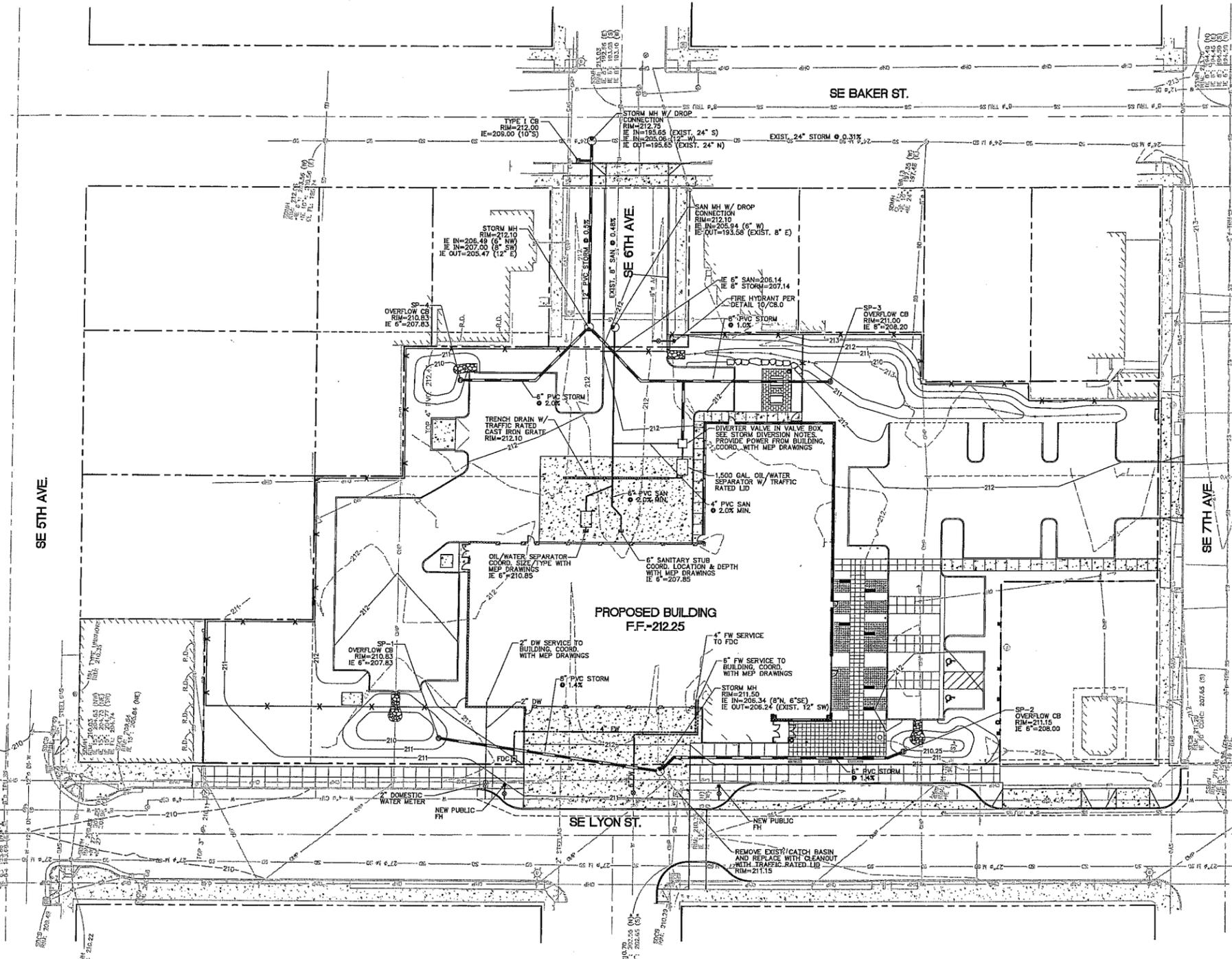
REVISIONS:
REVISION NUMBER
REVISION DESCRIPTION
DATE

SHEET TITLE:
UTILITY PLAN

DRAWN BY: B.TS
CHECKED BY: R.VS
SHEET

C2.3

JOB NO. 2140087.02





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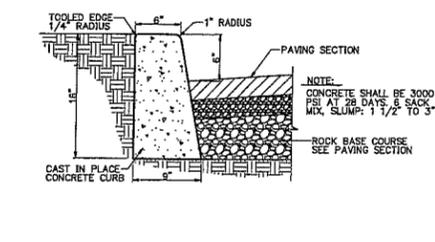
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DETAILS

DRAWN BY: BTS
CHECKED BY: RVS
SHEET

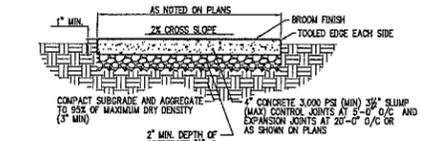
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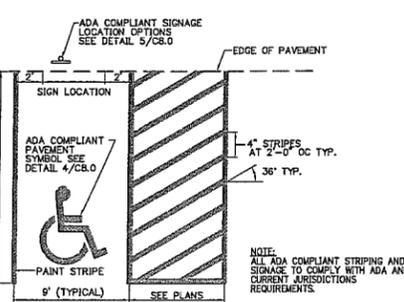
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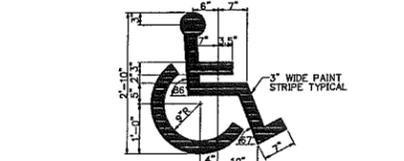
1 CONCRETE VERTICAL CURB
C8.0 N.T.S.



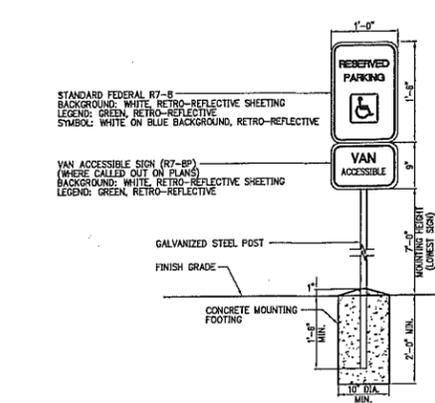
2 CONCRETE SIDEWALK
C8.0 N.T.S.



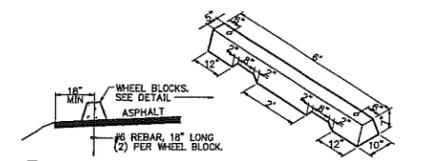
3 ADA COMPLIANT PARKING STALL
C8.0 N.T.S.



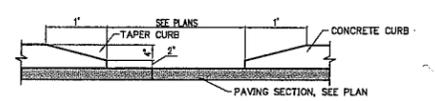
4 ADA COMPLIANT PARKING SYMBOL
C8.0 N.T.S.



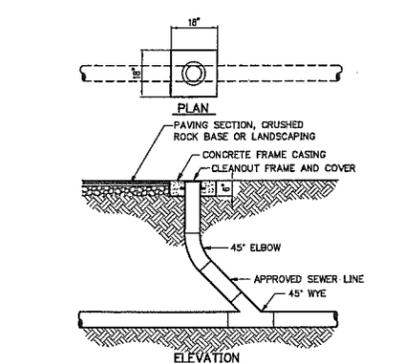
5 ADA COMPLIANT VAN PARKING SIGN
C8.0 N.T.S.



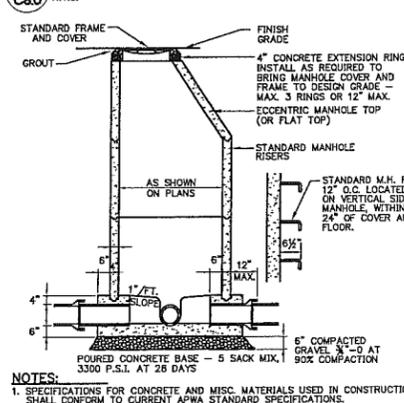
6 PRECAST CURB STOP
C8.0 N.T.S.



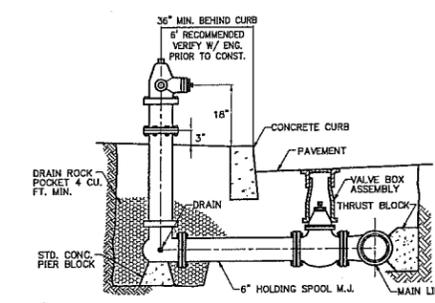
7 CONCRETE CURB BREAK
C8.0 N.T.S.



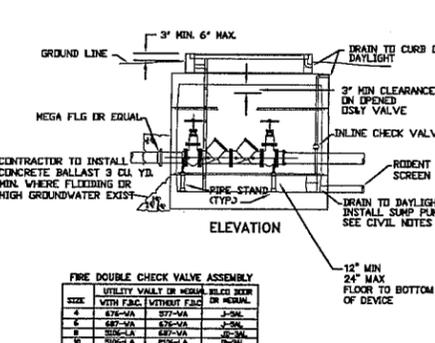
8 CLEANOUT
C8.0 N.T.S.



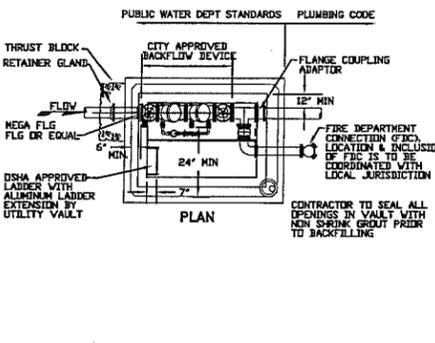
9 STANDARD MANHOLE
C8.0 N.T.S.



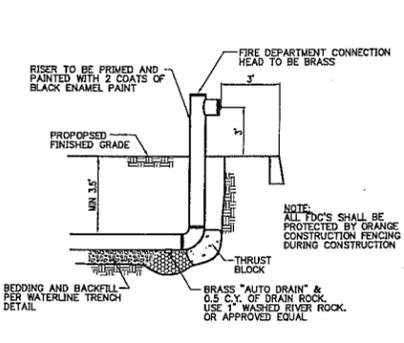
10 FIRE HYDRANT
C8.0 N.T.S.



11 DOUBLE DETECTOR CHECK VALVE AND VAULT
C8.0 N.T.S.



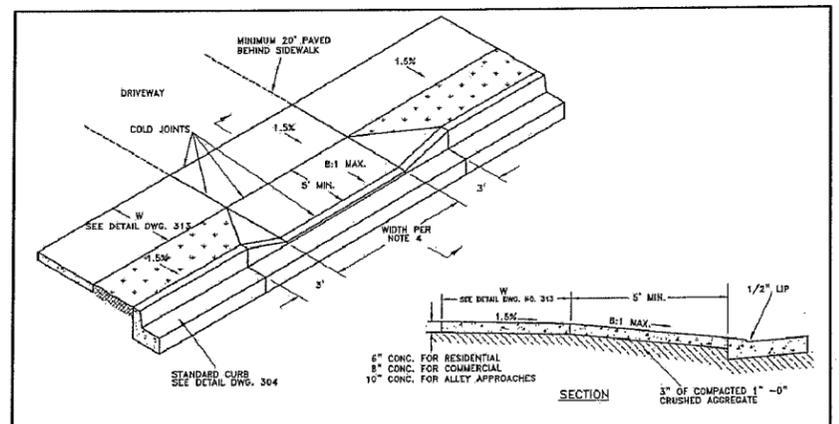
12 FDC DETAIL
C8.0 N.T.S.



13 CITY OF ALBANY DETAIL NO. 308
C8.0 N.T.S.

SIZE	WITH FIBC (WITHIN FIBC OR MESH)	UTILITY VAULT OR NORMAL WELLED BOTT
4	475-VA	577-VA
6	687-VA	826-VA
8	899-VA	1038-VA
10	1111-VA	1250-VA

CITY OF ALBANY, OREGON PUBLIC WORKS DEPARTMENT		
RESIDENTIAL AND COMMERCIAL DRIVEWAYS WITH SETBACK SIDEWALK		
NO SCALE	JANUARY 2011	NO. 308



- NOTES:
1. CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4000 P.S.I.
 2. CURB AND GUTTER SHALL BE CONSTRUCTED INDEPENDENTLY, AND SEPARATED BY A COLD JOINT, FROM ALL ADJACENT CONCRETE CONSTRUCTION, INCLUDING SIDEWALKS AND DRIVEWAY RAMPS.
 3. PROVIDE CONTRACTION JOINT AT MIDPOINT OF DRIVEWAY IF DRIVEWAY IS 16' WIDE OR GREATER.
 4. RESIDENTIAL DRIVEWAY WIDTH 10' - 24'. COMMERCIAL DRIVEWAY WIDTH PER DEVELOPMENT CODE, ARTICLE 12, CHAPTER 12.100
 5. NEW DRIVEWAY ACCESS CONSTRUCTION IN AN EXISTING CURB SHALL BE CONSTRUCTED WITH THE USE OF HORIZONTAL CONCRETE SAWCUTTING EQUIPMENT, SEE 306.02.07B.



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REVISIONS:

NO.	DATE	REVISIONS	REVISION DATE

SHEET TITLE:
**MATERIALS AND
LAYOUT PLAN -
NORTH**

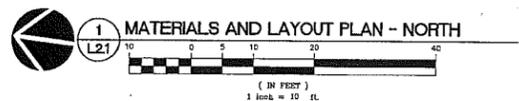
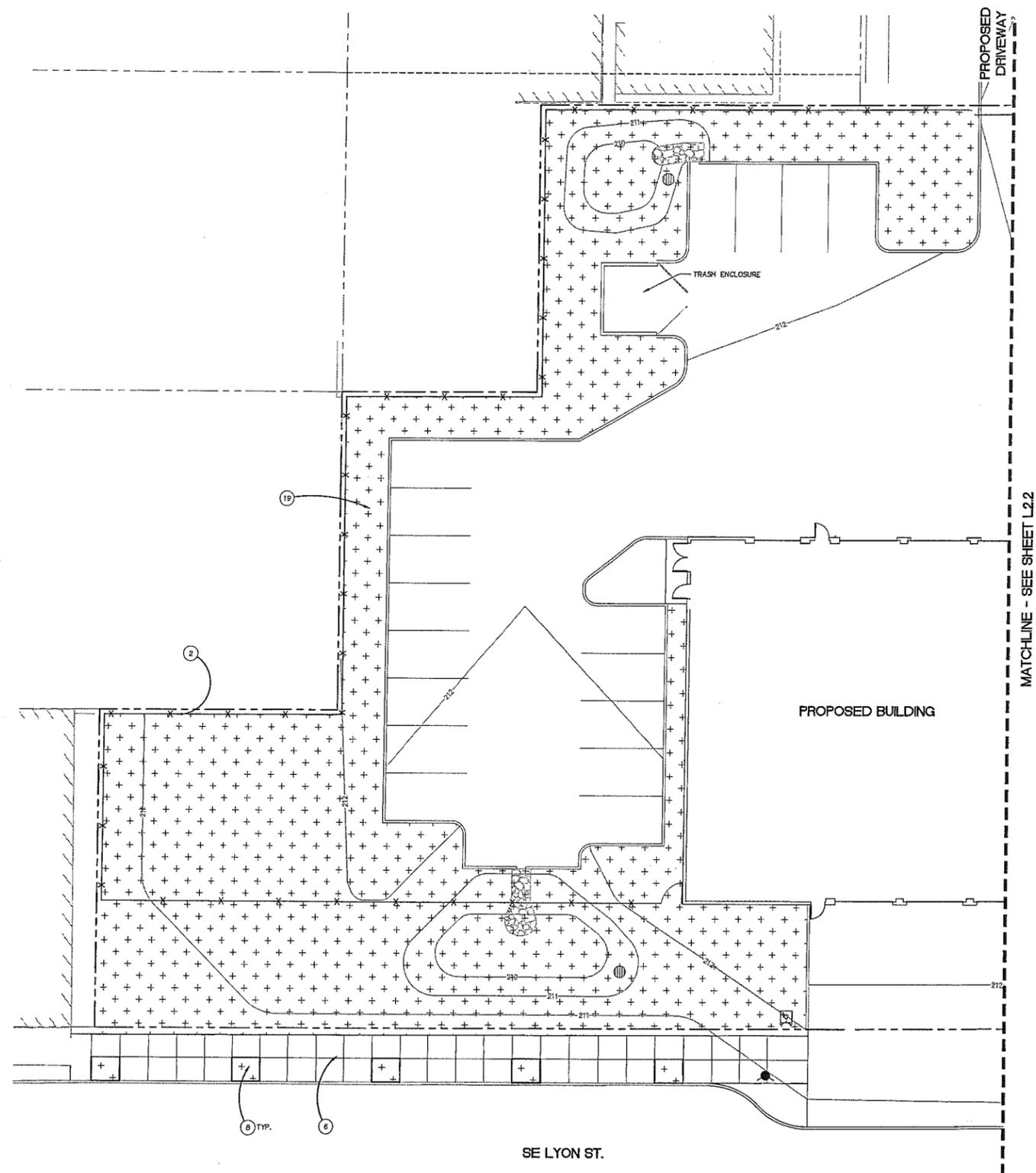
DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L2.1

JOB NO. 2140087.02

KEYNOTES, LAYOUT AND MATERIALS SCHEDULE

- ② ——— CHAIN LINK FENCE WITH SLATS
- ⑥ [Grid Pattern] PAVING TYPE C - SCORED CONCRETE
SEE CIVIL
- ⑧ [Square] TREE WELL
- ⑱ [Cross Pattern] AREA TO BE PLANTED
SEE L4.1 & L4.2 FOR PLANTING PLAN





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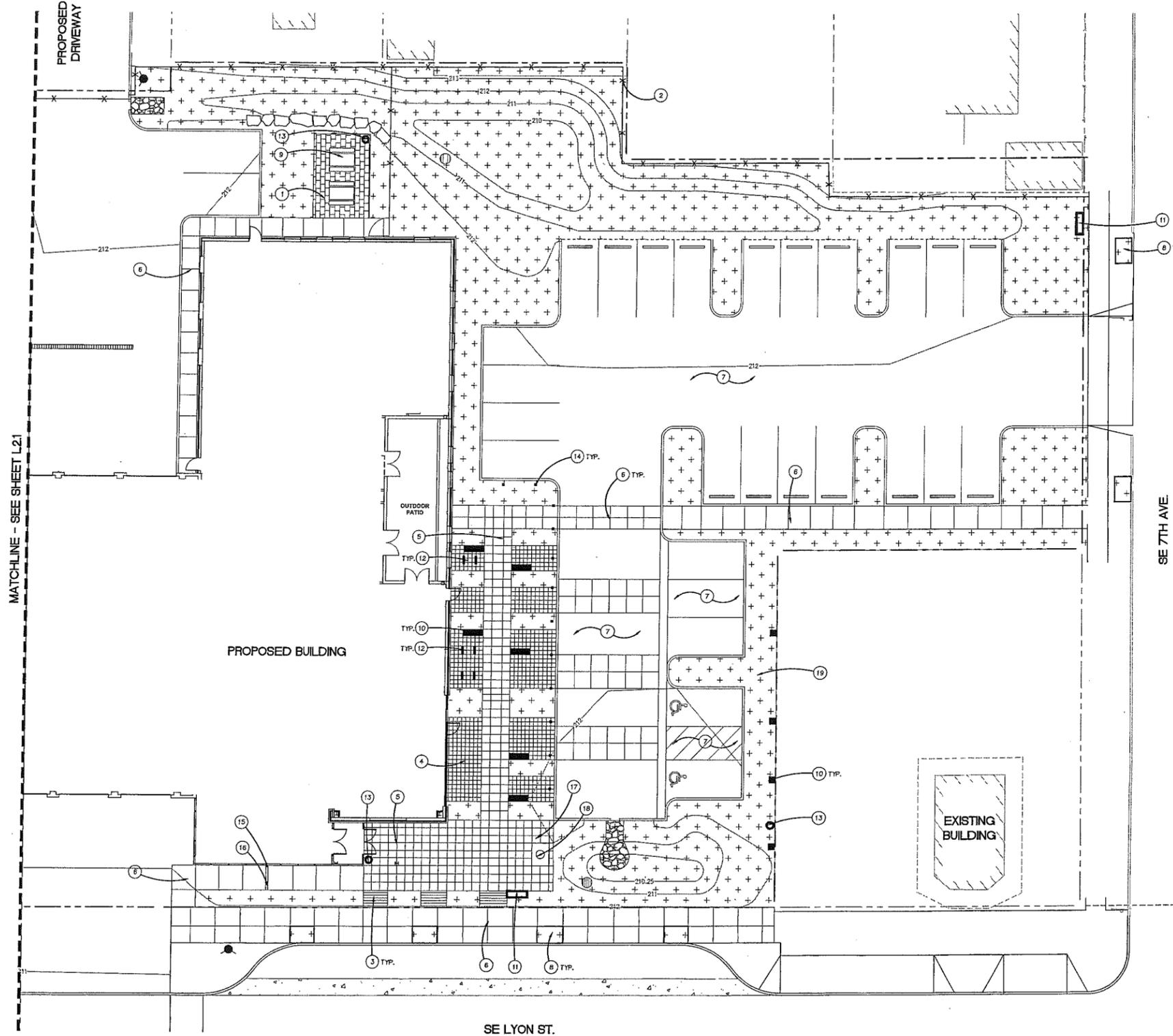
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KEYNOTES, LAYOUT AND MATERIALS SCHEDULE

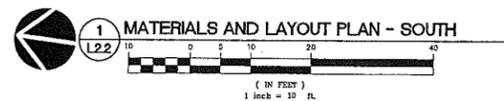
- ① GRAVEL MULCH WITH PAVERS UNDER TABLE LEGS
- ② CHAIN LINK FENCE WITH SLATS
- ③ IPE FOOTBRIDGE
- ④ PAVING TYPE A - PAVLHS
SEE DETAIL 16/LB.1
- ⑤ PAVING TYPE B - CONCRETE (3ID ALT. COLORED CONCRETE)
SEE DETAIL 17/LB.1
- ⑥ PAVING TYPE C - SCORED CONCRETE
SEE CIVIL
- ⑦ PAVING TYPE D - ASPHALT
SEE CIVIL
- ⑧ TREE WELL
- ⑨ PICNIC TABLE
- ⑩ CONCRETE SEAT WALL
- ⑪ CONCRETE SIGN WALL
- ⑫ BICYCLE RACK
- ⑬ TRASH RECEPTACLE
- ⑭ BOLLARD
- ⑮ DOWNSPOUT (SEE ARCH. DRAWINGS)
- ⑯ CHANNEL DRAIN
- ⑰ FLAG POLE
- ⑱ HISTORIC BEL.
- ⑲ AREA TO BE PLANTED
SEE LA.1 & LA.2 FOR PLANTING PLAN



MATCHLINE - SEE SHEET L21

SE 7TH AVE

SE LYON ST.



1 MATERIALS AND LAYOUT PLAN - SOUTH

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SHEET TITLE:
MATERIALS AND LAYOUT PLAN - SOUTH

DRAWN BY: TSD
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SHEET

L22

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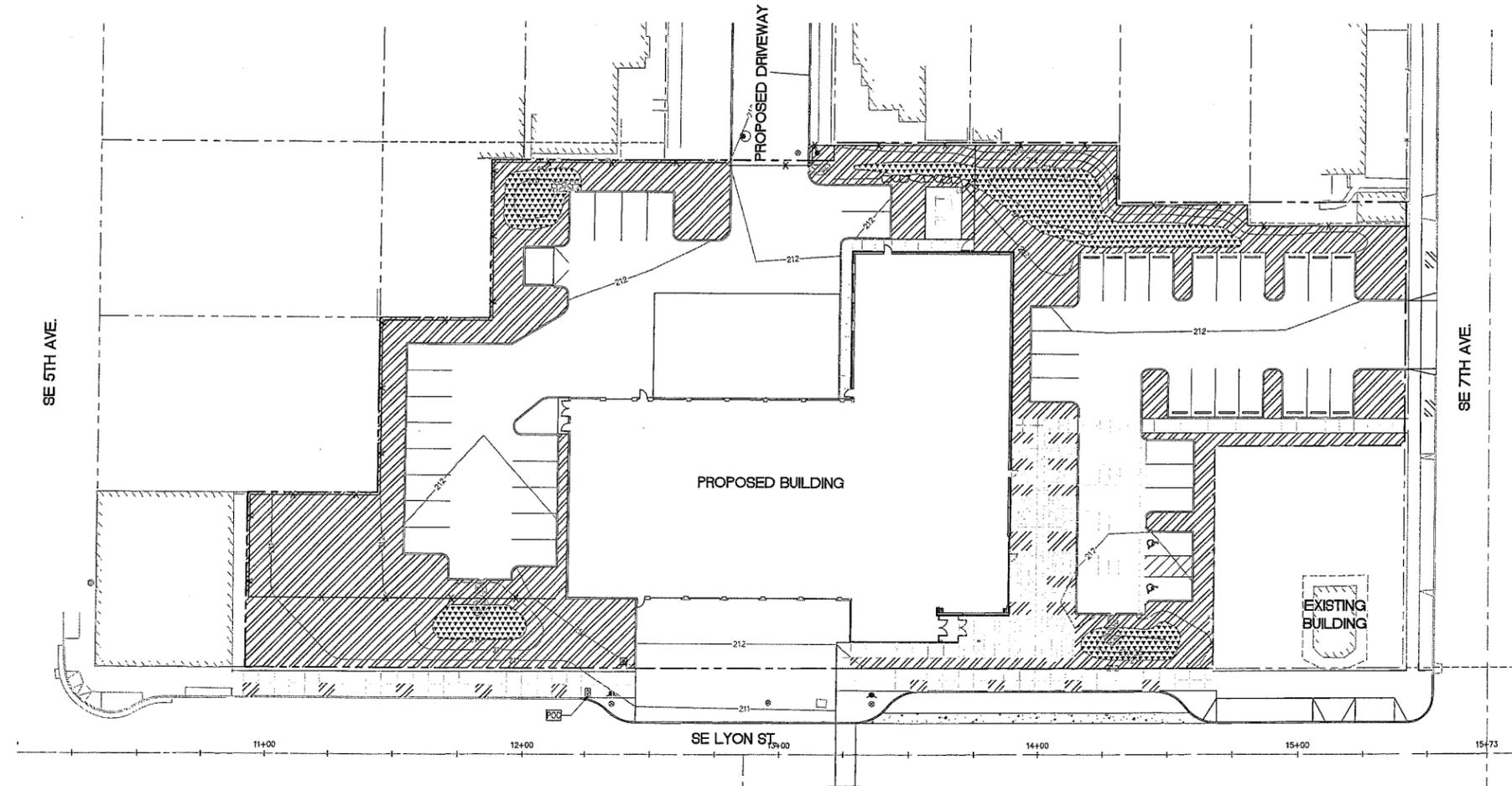
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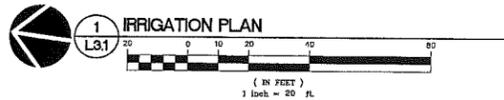


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IRRIGATION SCHEDULE

SYMBOL	DESCRIPTION	SHEET #
POC	POINT OF CONNECTION	L3.1
[Hatched Pattern]	SPRAY IRRIGATION AREAS, ROTARY SPRAY NOZZLES	
[Dotted Pattern]	SPRAY IRRIGATION AT STORMWATER AREAS, ROTARY SPRAY NOZZLES	



IRRIGATION NOTES

1. A FULLY AUTOMATIC IRRIGATION SYSTEM TO BE DESIGNED, BUILT AND INSTALLED BY OTHERS TO MAINTAIN ALL LANDSCAPE MATERIAL.
2. ALL NEW LANDSCAPE AREAS TO BE IRRIGATED WITH A FULLY AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
4. IRRIGATION SYSTEM SHALL BE DESIGNED SO THAT PLANTING BEDS, SLOPED BANKS, STORM WATER FACILITIES AND LAWN ZONES ARE ON SEPARATE CONTROL VALVES TO FACILITATE THE DIFFERENT WATER REQUIREMENTS OF EACH AREA.
5. IRRIGATION SHALL BE INSTALLED SIMULTANEOUSLY WITH PLANTING TO ENSURE PLANTS RECEIVE ADEQUATE WATER AT TIME OF INSTALLATION.
6. VALVES SHALL BE WIRED AND INSTALLED PER MANUFACTURER'S RECOMMENDED INSTALLATION PROCEDURES AND CONNECTED TO THE IRRIGATION CONTROLLER.
7. IRRIGATION SYSTEM SHALL PERFORM WITHIN THE TOLERANCES AND SPECIFICATIONS OF MANUFACTURERS RECOMMENDATIONS.
8. ALL IRRIGATION PIPE MATERIAL AND INSTALLATION SHALL CONFORM TO APPLICABLE CODE FOR PIPING AND COMPONENT REQUIREMENTS.
9. SYSTEM SHALL BE DESIGNED TO SUPPLY MANUFACTURER'S SPECIFIED MINIMUM OPERATING PRESSURE TO FARTHEST EMITTER FROM WATER METER.
12. REFERENCE L3.1 FOR IRRIGATION DETAILS FOR POINT OF CONNECTION AND BACKFLOW PREVENTION INFORMATION.
13. IRRIGATION SHALL BE WINTERIZED THROUGH LOW PRESSURE, HIGH VOLUME AIR BLOWOUT CONNECTION THROUGH QUICK COUPLER.

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SHEET TITLE:
IRRIGATION PLAN

DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L3.1

JOB NO. 2140087.02



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REV. NO. REVISIONS REVISION DATE
BY DATE

SHEET TITLE:
**PLANTING
PLAN - NORTH**

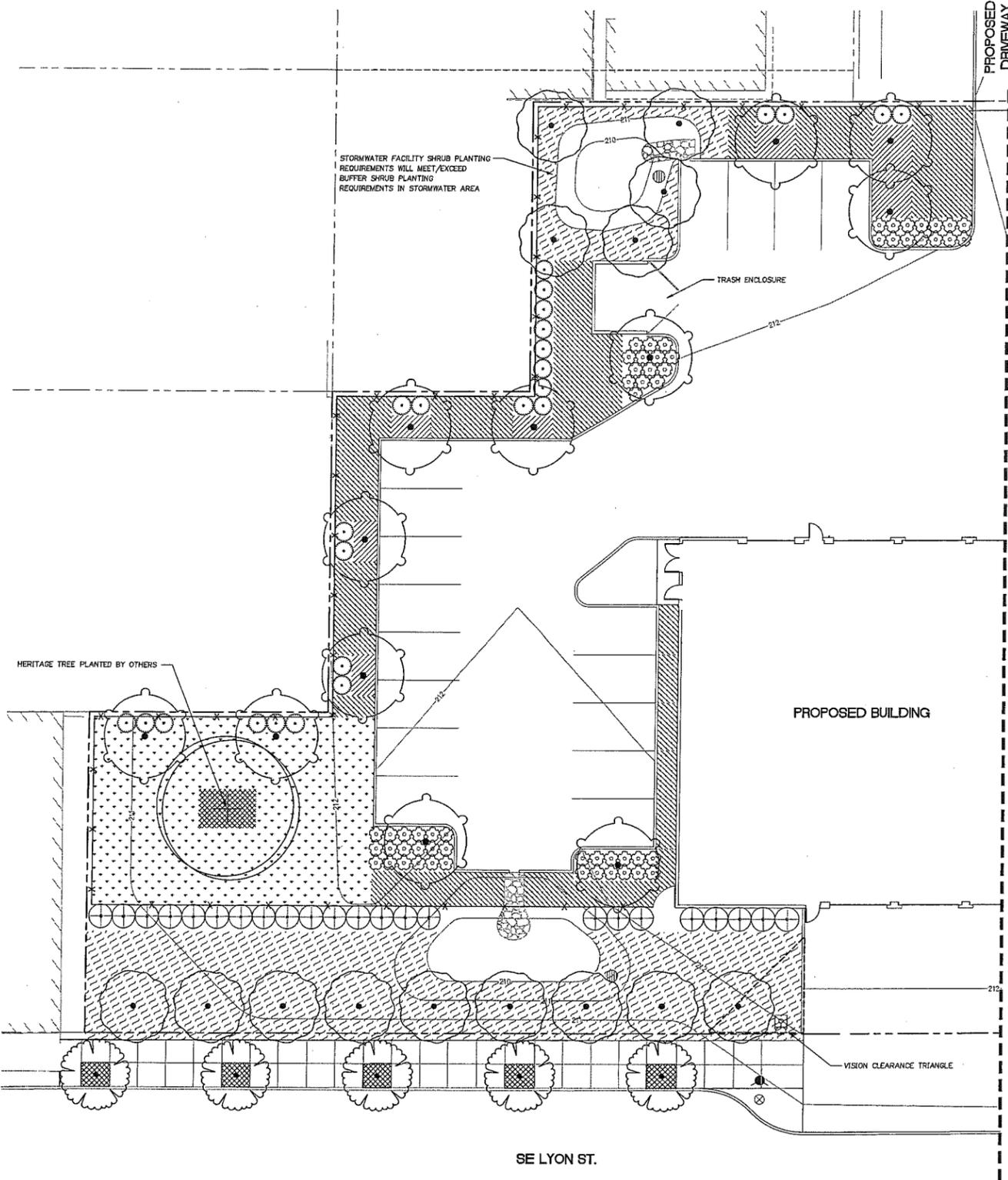
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SHEET

L41

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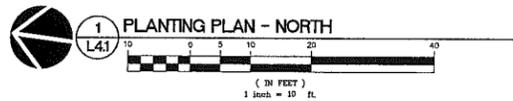


MATCHLINE - SEE SHEET L.42

PLANT SCHEDULE			
TREES	BOTANICAL NAME / COMMON NAME	SIZE	
	ACER CIRCINATUM / VINE MAPLE	2" CAL. B&B, 10' HT. MIN.	
	GINKGO BILOBA 'PRINCETON SENTRY' / PRINCETON SENTRY GINKGO	2" CAL. B&B	
	PYRUS CALLERYANA 'CAPITAL' / CAPITAL CALLERY PEAR	2" CAL. B&B	
	QUERCUS COCCINEA / SCARLET OAK	2" CAL. B&B, 10' HT. MIN.	
	QUERCUS PALUSTRIS 'GREEN PILLAR' / GREEN PILLAR OAK	2" CAL. B&B	
	ZELKOYA SERRATA 'GREEN VASE' / SAWLEAF ZELKOYA	2" CAL. B&B, 10' HT. MIN.	
SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	SPACING
	BERBERIS THUNBERGII 'CRIMSON PYGMY' / CRIMSON PYGMY BARBERRY	3 GAL	30" o.c.
	FARGESIA RUFA / CLUMPING BAMBOO	1 GAL	24" o.c.
	ILEX CRENATA 'HELENI' / HELEN JAPANESE HOLLY	3 GAL	30" o.c.
	LIGUSTRUM JAPONICUM 'TEXANUM' / WAX LEAF PRIVET	5 GAL	48" o.c.
	MYRTICA CALIFORNICA / PACIFIC WAX MYRTLE	5 GAL	60" o.c.
	NANDINA DOMESTICA 'GULF STREAM' TM / HEAVENLY BAMBOO	3 GAL	30" o.c.
	ROSA MEDILAND SERIES 'FAIRY' / FAIRY MEDILAND ROSE	3 GAL	30" o.c.
	VIBURNUM DAVIDII / DAVID VIBURNUM	3 GAL	36" o.c.
	VIBURNUM TINUS 'SPRING BOUQUET' / SPRING BOUQUET LAURESTINUS	5 GAL	48" o.c.
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
	BARK MULCH	N/A	
	JUNIPERUS HORIZONTALIS 'BLUE CHIP' / BLUE CHIP JUNIPER	4" POT	24" o.c.
	LAWN	SOO	
	FENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS	1 GAL	
	ROCK MULCH	N/A	
	RUBUS CALYCIINODES / GREEN CARPET RASPBERRY	4" POT	12" o.c.
STORMWATER AREAS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
	ZONE A PLANTING AREA		
	HERBACEOUS PLANTS		
	CAREX GENUATA / SLOUGH SEDGE	4" POT	12" o.c.
	JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	4" POT	12" o.c.
	ZONE B PLANTING AREA		
	GROUNDCOVER		
	ARCTOSTAPHYLOS UVA-URSI / KINKKINNIK	1 GAL	12" o.c.
	JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	4" POT	12" o.c.
	SHRUBS		
	CORNUS SERICEA / RED TWIG DOGWOOD	30" HT. MIN.	48" o.c.
	MAHONIA REPENS / CREEPING MAHONIA	1 GAL	18" o.c.
	ROSA NUTKANA / NOOTKA ROSE	30" HT. MIN.	48" o.c.
	SPIRAEA BETULIFOLIA / BIRCHLEAF SPIRAEA	1 GAL	24" o.c.
	STYMPHORICARPOS ALBA / SNOWBERRY	1 GAL	36" o.c.

LANDSCAPE NOTES

- GENERAL NOTES:**
- CONTRACTOR SHALL CONFIRM ALL EXISTING CONDITIONS PRIOR TO COMMENCING WORK AND NOTIFY THE OWNER OR OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES OR CONFLICTS.
 - CONTRACTOR SHALL VERIFY EXISTING TREES IN THE FIELD PRIOR TO COMMENCEMENT OF WORK.
 - CONTRACTOR SHALL VERIFY INVERT ELEVATIONS OF ALL UNDERGROUND UTILITIES AND NOTIFY LANDSCAPE ARCHITECT IF THERE ARE ANY DISCREPANCIES WITH PLANTING ROOT ZONES. TO LOCATE SITE UTILITIES PRIOR TO PROPOSED EXCAVATION CALL 1-800-332-2244.
 - CONTRACTOR SHALL COORDINATE WITH THE OWNER ANY DISRUPTION TO VEHICULAR CIRCULATION PRIOR TO COMMENCEMENT OF ANY WORK.
 - CONTRACTOR SHALL KEEP PEDESTRIAN TRAVEL WAYS AND ACCESS TO ALL STRUCTURES PROTECTED AT ALL TIMES.
 - CONTRACTOR SHALL REPLACE OR REPAIR DAMAGE TO EXISTING CONCRETE CURB, ASPHALT PAVING, OR OTHER STRUCTURES TO PRE CONSTRUCTION CONDITIONS.
 - ALL LANDSCAPE AREAS SHALL BE INSTALLED AND MAINTAINED AS A MINIMUM TO STANDARDS ACCORDING TO CITY REVISED CODE.
 - ALL NEW PLANTING AREAS TO BE IRRIGATED BY AUTOMATIC IRRIGATION SYSTEM DESIGNED AND BUILT BY OTHERS. REFERENCE L3.1 FOR IRRIGATION PLAN AND NOTES.
- PLANTING NOTES:**
- ALL EXISTING TREES, PLANTS, AND ROOTS SHALL BE PROTECTED FROM DAMAGE DURING ANY CONSTRUCTION PREPARATION, REMOVAL OR INSTALLATION.
- ACTIVITIES WITHIN AND ADJACENT TO PROJECT LIMITS:**
- IF DISTURBANCE IS NECESSARY AROUND EXISTING TREES, CONTRACTOR SHALL PROTECT THE CROWN AND ALL WORK WITHIN THE TREE DRIPZONE SHALL BE LIMITED TO THE USE OF HAND TOOLS AND MANUAL EQUIPMENT ONLY.
 - REPLACE, REPAIR AND RESTORE DISTURBED LANDSCAPE AREAS DUE TO GRADING, TRENCHING OR OTHER REASONS TO PRE CONSTRUCTION CONDITION AND PROVIDE MATERIAL APPROVED BY THE OWNER OR OWNER'S REPRESENTATIVE.
 - EXISTING AREAS PROPOSED FOR NEW PLANT MATERIAL SHALL BE CLEARED AND LEGALLY DISPOSED UNLESS NOTED OTHERWISE.
 - ALL PLANT MATERIAL SHALL BE HEALTHY NURSERY STOCK, WELL BRANCHED AND ROOTED, FULL FOLIAGE, FREE FROM INSECTS, DISEASES, WEEDS, WEED ROT, INJURIES AND DEFECTS WITH NO LESS THAN MINIMUMS SPECIFIED IN AMERICAN STANDARDS FOR NURSERY STOCK, ANSI Z60.1-2004.
 - ALL LANDSCAPED AREAS SHALL BE COVERED BY A LAYER OF ORGANIC MULCH TO A MINIMUM DEPTH OF 2-INCHES.
 - A SOILS ANALYSIS, BY AN INDEPENDENT SOILS TESTING LABORATORY RECOGNIZED BY THE STATE DEPARTMENT OF AGRICULTURE, SHALL BE USED TO RECOMMEND AN APPROPRIATE PLANTING SOIL AND/OR SPECIFIED SOIL AMENDMENTS.
 - TOPSOIL SHALL BE AMENDED AS RECOMMENDED BY AN INDEPENDENT SOILS TESTING LABORATORY AND AS OUTLINED IN THE SPECIFICATION.



TREE CANOPY NOTES - NORTH PARKING LOT

NORTH PARKING LOT AREA = 10,893 SF
REQUIRED 20% PARKING LOT AREA COVERAGE = 2,178 SF
PROVIDED 23% PARKING LOT AREA COVERAGE = 2,585 SF



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SHEET TITLE:
**PLANTING
PLAN - SOUTH**

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SHEET

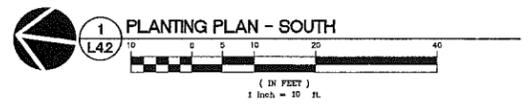
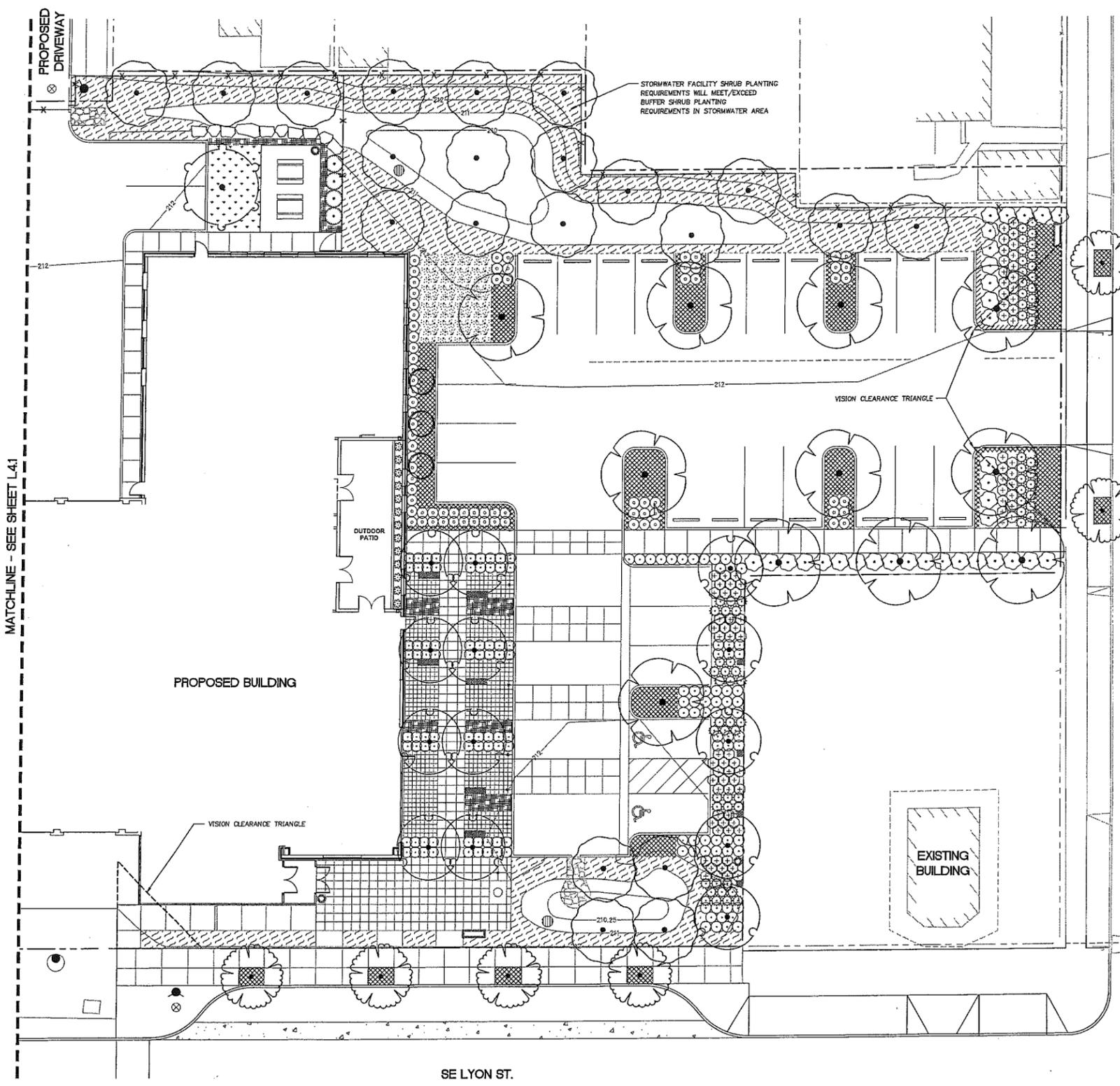
L4.2

JOB NO. 2140087.02

PLANT SCHEDULE			
TREES	BOTANICAL NAME / COMMON NAME	SIZE	
	ACER CIRCINATUM / VINE MAPLE	2" CAL. BAB, 10' HT. MIN.	
	GINKGO BILOBA 'PRINCETON SENTRY' / PRINCETON SENTRY GINKGO	2" CAL. BAB	
	PYRUS CALLERYANA 'CAPITAL' / CAPITAL CALLERY PEAR	2" CAL. BAB	
	QUERCUS COCCINEA / SCARLET OAK	2" CAL. BAB, 10' HT. MIN.	
	QUERCUS PALUSTRIS 'GREEN PILLAR' / GREEN PILLAR OAK	2" CAL. BAB	
	ZELKOVA SERHATA 'GREEN VASE' / SAWLEAF ZELKOVA	2" CAL. BAB, 10' HT. MIN.	
SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	SPACING
	BERBERIS THUNBERGII 'CRIMSON PYGMY' / CRIMSON PYGMY BARBERRY	3 GAL	30" o.c.
	FARGESIA RUFA / CLUMPING BAMBOO	1 GAL	24" o.c.
	ILEX CRENATA 'HELERII' / HELER JAPANESE HOLLY	3 GAL	30" o.c.
	LIGUSTRUM JAPONICUM 'TEXANUM' / WAX LEAF PRIVET	5 GAL	48" o.c.
	MYRICA CALIFORNICA / PACIFIC WAX MYRTLE	5 GAL	60" o.c.
	NANDINA DOMESTICA 'GULF STREAM' TM / HEAVENLY BAMBOO	3 GAL	30" o.c.
	ROSA MEIDLAND SERIES 'FAIRY' / FAIRY MEIDLAND ROSE	3 GAL	30" o.c.
	VIBURNUM DAVIDII / DAVID VIBURNUM	3 GAL	36" o.c.
	VIBURNUM TINUS 'SPRING BOUQUET' / SPRING BOUQUET LAURESTINUS	5 GAL	48" o.c.
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
	BARK MULCH	N/A	
	JUNIPERUS HORIZONTALIS 'BLUE CHIP' / BLUE CHIP JUNIPER	4" POT	24" o.c.
	LAWN	SOD	
	PENNISETUM ALOPECUROIDES 'HAEMEL' / HAEMEL DWARF FOUNTAIN GRASS	1 GAL	
	ROCK MULCH	N/A	
	RUBUS CALYCIROIDES / GREEN CARPET RASPBERRY	4" POT	12" o.c.
STORMWATER AREAS	BOTANICAL NAME / COMMON NAME	CONT	SPACING
ZONE A PLANTING AREA			
	HERBACEOUS PLANTS		
	CAREX OENOPTEA / SLOUGH SEDGE	4" POT	12" o.c.
	JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	4" POT	12" o.c.
ZONE B PLANTING AREA			
	GROUND COVER		
	ARCTOSTAPHYLOS UVA-URSI / KINKKINICK	1 GAL	12" o.c.
	JUNCUS PATENS 'ELK BLUE' / ELK BLUE RUSH	4" POT	12" o.c.
	SHRUBS		
	CORNUS SERICEA / RED TWIG DOGWOOD	30" HT. MIN.	48" o.c.
	MAHONIA REPENS / CREEPING MAHONIA	1 GAL	18" o.c.
	ROSA NUTKANA / NOOTKA ROSE	30" HT. MIN.	48" o.c.
	SPIRAEA BETULIFOLIA / BIRCHLEAF SPIRAEA	1 GAL	24" o.c.
	SYMPHORICARPOS ALBA / SNOWBERRY	1 GAL	36" o.c.

TREE CANOPY NOTES - SOUTH PARKING LOT

SOUTH PARKING LOT AREA = 10,163 SF
REQUIRED 20% PARKING LOT AREA COVERAGE = 2,032 SF
PROVIDED 35% PARKING LOT AREA COVERAGE = 3,636 SF





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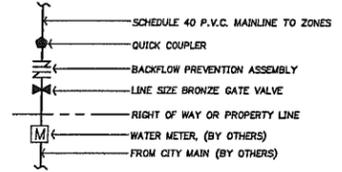
Client:
ALBANY FIRE
110 6TH AVENUE SE
ALBANY, OR 97321



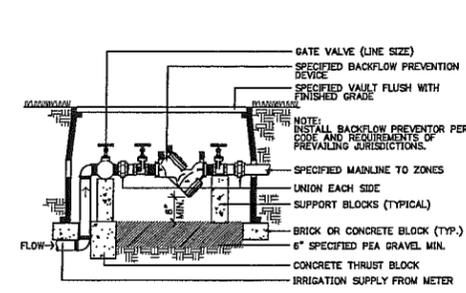
FIRE DEPARTMENT

Project:
ALBANY FIRE STATION

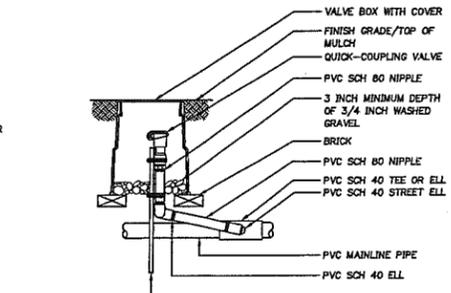
110 6TH AVENUE SE
ALBANY, OR 97321



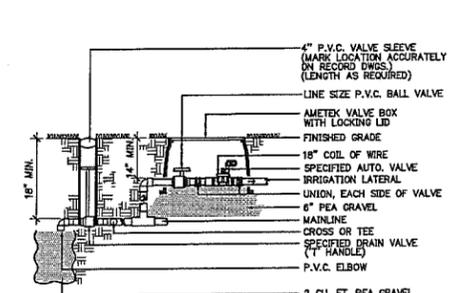
1 POINT OF CONNECTION
L8.1 SCALE: N.T.S.



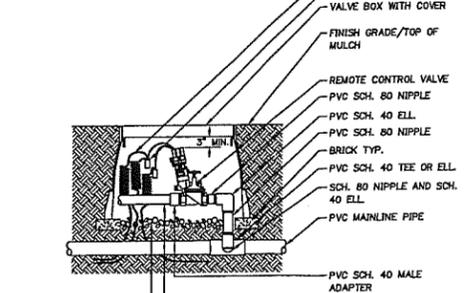
2 DOUBLE CHECK VALVE
L8.1 BACKFLOW PREVENTER (BELOW GRADE) SCALE: N.T.S.



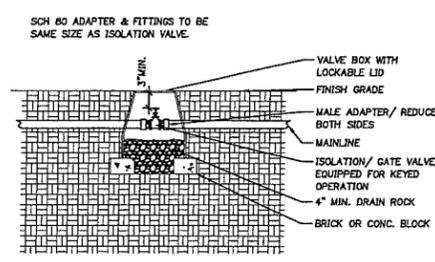
3 QUICK COUPLING VALVE
L8.1 SCALE: N.T.S.



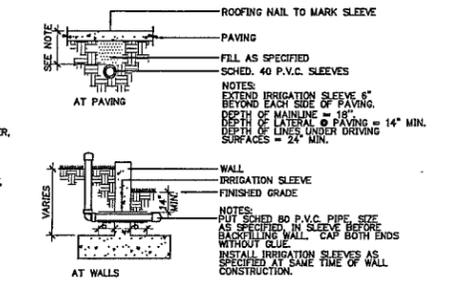
4 CONTROL AND MANUAL DRAIN VALVE
L8.1 ASSEMBLY SCALE: N.T.S.



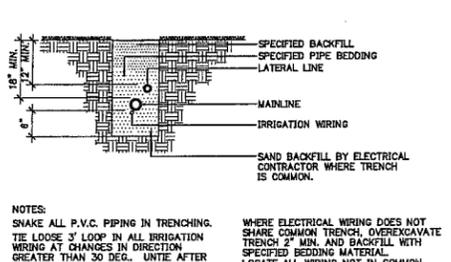
5 ELECTRIC REMOTE CONTROL VALVE
L8.1 SCALE: N.T.S.



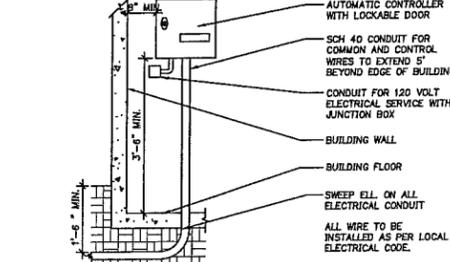
6 ISOLATION/GATE VALVE
L8.1 SCALE: N.T.S.



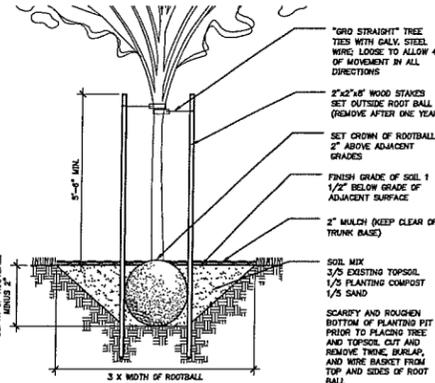
7 IRRIGATION SLEEVES
L8.1 SCALE: N.T.S.



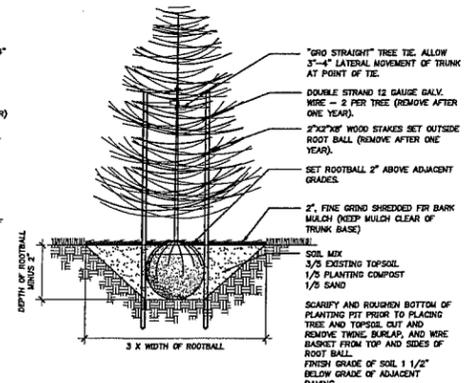
8 TYPICAL TRENCHING
L8.1 SCALE: N.T.S.



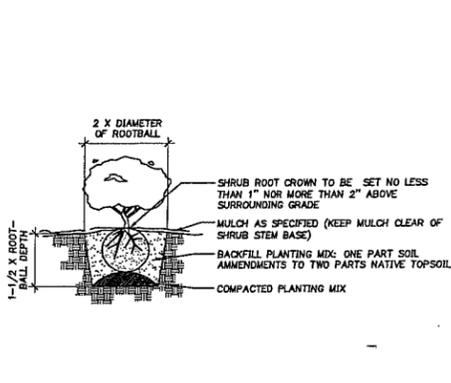
9 WALL MOUNT CONTROLLER
L8.1 INTERIOR SCALE: N.T.S.



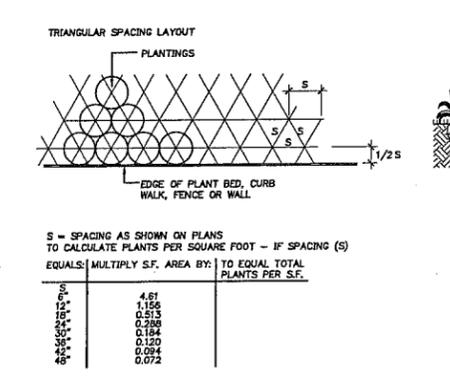
10 DECIDUOUS TREE STAKING DETAIL
L8.1 SCALE: N.T.S.



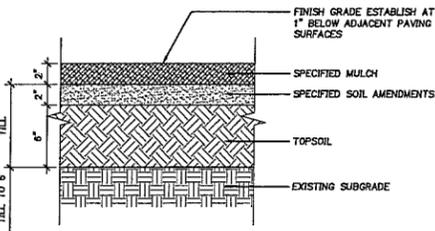
11 CONIFER STAKING DETAIL
L8.1 SCALE: N.T.S.



12 SHRUB PLANTING DETAIL
L8.1 SCALE: N.T.S.

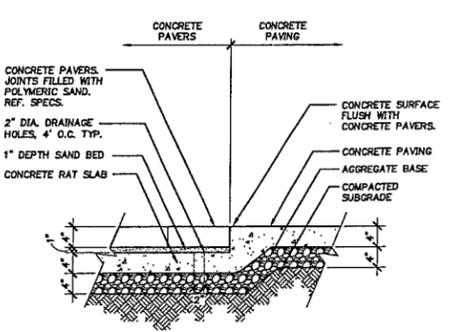


13 PLANT SPACING
L8.1 SCALE: N.T.S.

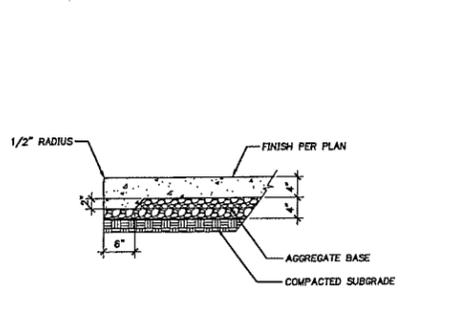


NOTES:
1. REMOVE ALL ROCK, DEBRIS AND OTHER FOREIGN MATTER OVER 1\"/>

14 SOIL PREP. DETAIL
L8.1 SCALE: N.T.S.



16 CONCRETE PAVERS
L8.1 1\"/>



17 CONCRETE PAVING
L8.1 1\"/>

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REVISIONS:
BY: REVISIONS
DATE: REVISIONS
BY: TSD
DATE: CLOSING DATE
BY: RAH
DATE: SHEET

SHEET TITLE:
IRRIGATION AND PLANTING DETAILS

DRAWN BY: TSD
CHECKED BY: RAH
SHEET

L8.1

JOB NO. 2140087.02

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Revision Schedule	
Revision Delta	Issue Date

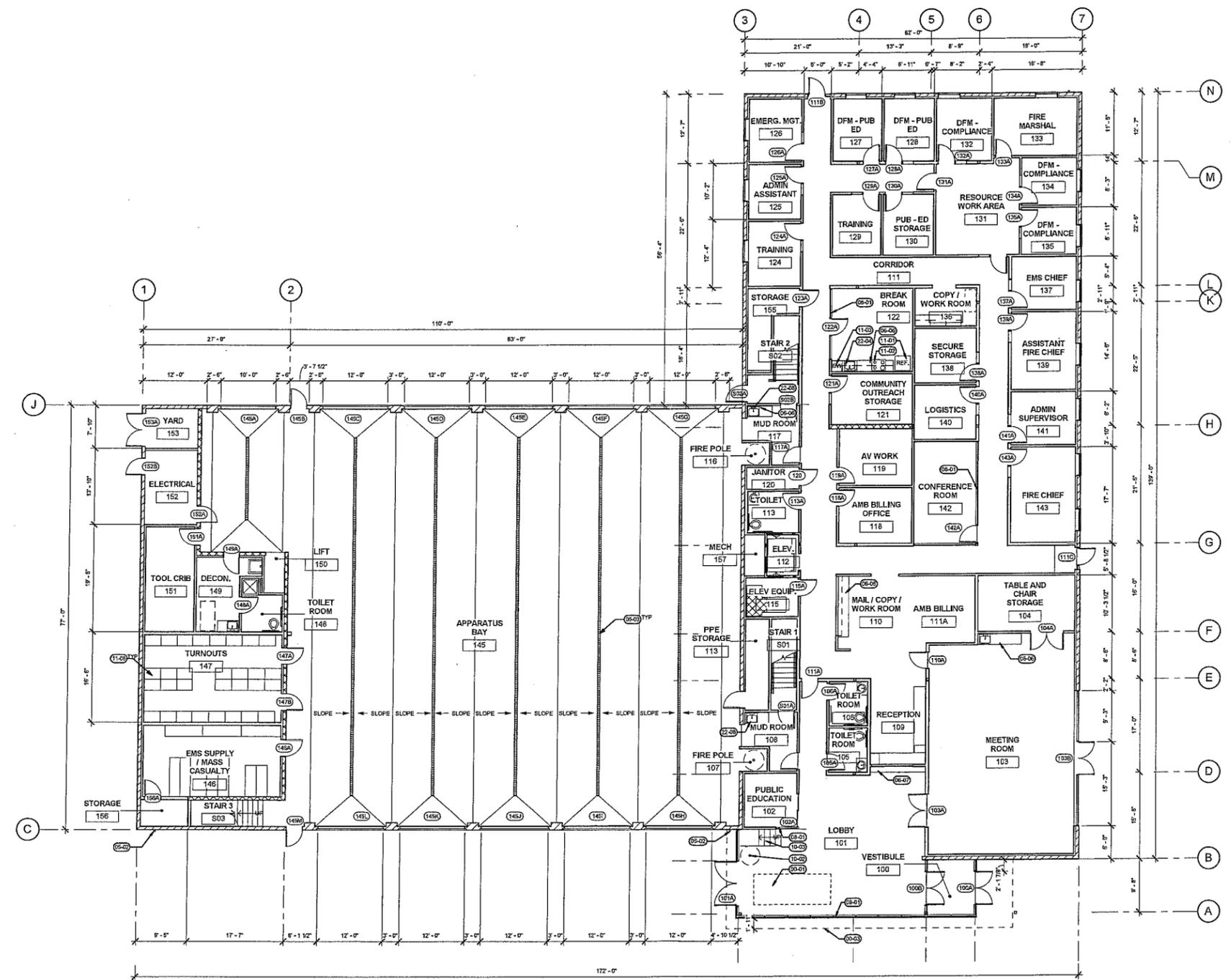
SHEET TITLE:
FIRST FLOOR PLAN

DRAWN BY: RLYLEJ
CHECKED BY: JRH
SHEET

A2.1

JOB NO. 2140087.02

CONDITIONAL USE SUBMITTAL: 09/14/15



1 FIRST FLOOR PLAN
A2.1 1/8" = 1'-0"

GENERAL NOTES

- A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION
- B. SEE FURNITURE AND EQUIPMENT PLANS FOR ADDITIONAL INFORMATION
- C. DIMENSIONS REFLECT FACE OF FINISH UNLESS NOTED OTHERWISE
- D. WALL THICKNESSES ARE ACTUAL UNLESS NOTED OTHERWISE
- E. SEE SHEET A1.1 FOR WALL TYPE DEFINITION AND STANDARD DETAILS. WALLS ARE PIB U.L.D.
- F. ELEVATOR 6'-0" = FINISH FLOOR ELEVATION MAIN FLOOR AS INDICATED IN CIVIL DRAWINGS
- G. SEE FINISH PLAN FOR CASEWORK AND FINISH RELATED INFORMATION
- H. CONTRACTOR SHALL VERIFY AND CORRECT ALL DIMENSIONS AND LAYOUT INFORMATION PRIOR TO START OF WORK. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS.
- I. REFER TO ENLARGED PLANS WHERE INDICATED FOR ADDITIONAL INFORMATION.
- J. ENLARGED PLANS TIME PRECISELY OVER PLANS OF SIMILAR SCALE.
- K. DOORS NOT DIMENSIONED ARE TO BE LOCATED 4" FROM FACE OF WALL TO OUTSIDE EDGE OF JAMB, TYPICAL.
- L. CONTRACTOR TO PROVIDE ADEQUATE GYPSUM BOARD CONTROL JOINTS AS REQUIRED THROUGHOUT ENTIRE BUILDING, INTERIOR AND EXTERIOR. PROVIDE BLOCKING AS REQUIRED ADJACENT TO FIRE EXTINGUISHERS FOR OWNER INSTALLED AED STATIONS.

LEGEND

- (T) KEYNOTE
- (D) GRIDLINE
- [Pattern] EXTERIOR WALL - STRUCTURAL BRICK VENER
- [Pattern] EXTERIOR WALL - CMU WITH BRICK VENER
- [Pattern] EXTERIOR WALL - BRICK VENER AND METAL STUD S
- [Pattern] EXTERIOR WALL - METAL PANEL AND METAL STUDS
- [Pattern] INTERIOR WALL - CMU SHEARWALL
- [Pattern] INTERIOR PARTITION, SEE SHEET A1.0
- [Pattern] 1-HR RATED INTERIOR PARTITION
- [Pattern] INTERIOR GLAZING

KEYNOTES

- 00-01 DISPLAY APPARATUS, OFO
- 00-03 LINE OF GRADIENT ABOVE
- 05-02 METAL DOWNSPOUT AND SCUPPER
- 05-03 TRENCH DRAIN, CENTER ON APPARATUS BAY
- 06-06 SOLID SURFACE VANITY COUNTER
- 06-07 PASS THROUGH RECEPTION COUNTER
- 06-01 STOREFRONT SYSTEM
- 10-02 BRASS FIRE POLE
- 10-03 3'-0" X 1'-0" X 1/2'-0" WOOD STAIR
- 11-01 REFRIGERATOR
- 11-02 COOKTOP AND RANGE HOOD
- 11-03 DISHWASHER
- 11-06 2 X 3 TURNOUT STORAGE SYSTEM
- 22-04 KITCHEN SINGLE SINK
- 22-08 UTILITY SINK

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Revision	Schedule

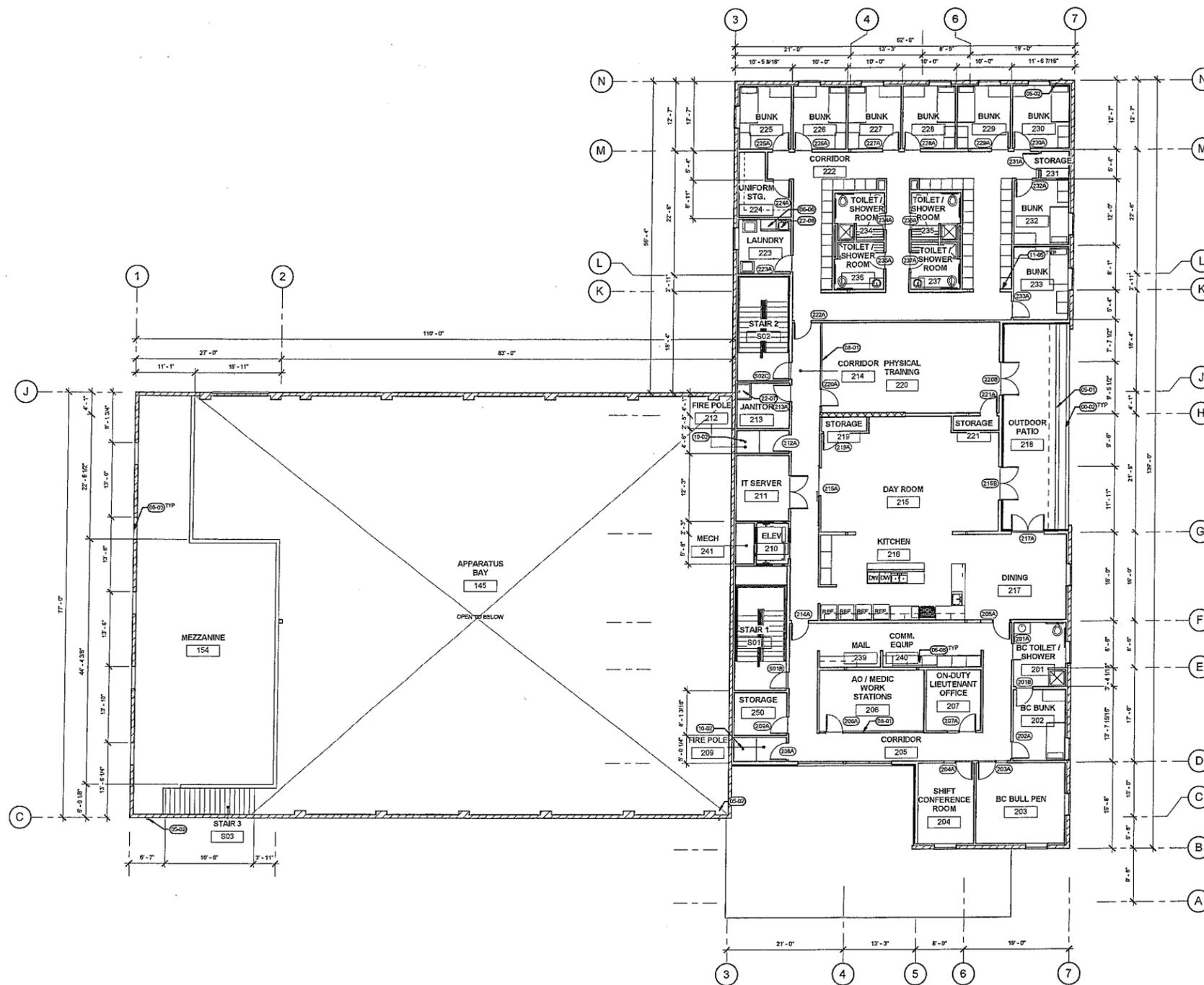
SHEET TITLE:
SECOND FLOOR PLAN

DRAWN BY: KLVLEJ
 CHECKED BY: JRH
 SHEET

A2.2

JOB NO. 2140087.02

CONDITIONAL USE SUBMITTAL: 09/14/15



1 SECOND FLOOR PLAN
 1/8" = 1'-0"

GENERAL NOTES

- A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION
- B. SEE FURNITURE AND EQUIPMENT PLANS FOR ADDITIONAL INFORMATION
- C. DIMENSIONS REFLECT FACE OF FINISH UNLESS NOTED OTHERWISE
- D. WALL THICKNESSES ARE ACTUAL UNLESS NOTED OTHERWISE
- E. SEE SHEET A1.1 FOR WALL TYPE DEFINITION AND STANDARD DETAILS, WALLS ARE PFB U/L/D.
- F. ELEVATION 0'-0" = FINISH FLOOR ELEVATION MAIN FLOOR AS INDICATED IN CIVIL DRAWINGS
- G. SEE FINISH PLAN FOR CASEWORK AND FINISH RELATED INFORMATION
- H. CONTRACTOR SHALL VERIFY AND CONFIRM ALL DIMENSIONS AND LAYOUT INFORMATION PRIOR TO START OF WORK. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS.
- I. REFER TO BALANCED PLANS WHERE INDICATED FOR ADDITIONAL INFORMATION. ENLARGED PLANS TAKE PRECEDENCE OVER PLANS OF SMALLER SCALE.
- J. DOORS NOT DIMENSIONED ARE TO BE LOCATED 4" FROM FACE OF WALL TO OUTSIDE EDGE OF JAMB, TYPICAL.
- K. CONTRACTOR TO PROVIDE ADEQUATE GYPSUM BOARD CONTROL JOINTS AS REQUIRED THROUGHOUT ENTIRE BUILDING, INTERIOR AND EXTERIOR.
- L. PROVIDE BLOCKING AS REQUIRED ADJACENT TO FIRE EXTINGUISHERS FOR OWNER INSTALLED AED STATIONS.

LEGEND

- (7) KEYNOTE
- (0) GRIDLINE
- EXTERIOR WALL - STRUCTURAL BRICK VENEER
- EXTERIOR WALL - CMU WITH BRICK VENEER
- EXTERIOR WALL - BRICK VENEER AND METAL STUDS
- EXTERIOR WALL - METAL PANEL AND METAL STUDS
- INTERIOR WALL - CMU SHEARWALL
- INTERIOR PARTITION, SEE SHEET A1.0
- 1-HR RATED INTERIOR PARTITION
- INTERIOR GLAZING

KEYNOTES

- 00-02 3'-0" LOW WALL PLANTER AT PATIO, METAL PANEL FINISH
- 05-01 EXTERIOR STEEL CANOPY - SEE STRUCTURAL
- 05-02 METAL DOWNSPOUT AND SCUPPER
- 06-06 SOLID SURFACE VANITY COUNTER
- 08-01 STEREFRONT SYSTEM
- 08-03 CLEGGSTORY WINDOWS
- 10-02 BRASS FIRE POLE
- 11-00 3 X 2 PERSONAL LOCKER
- 22-07 WOP SINK
- 22-08 UTILITY SINK

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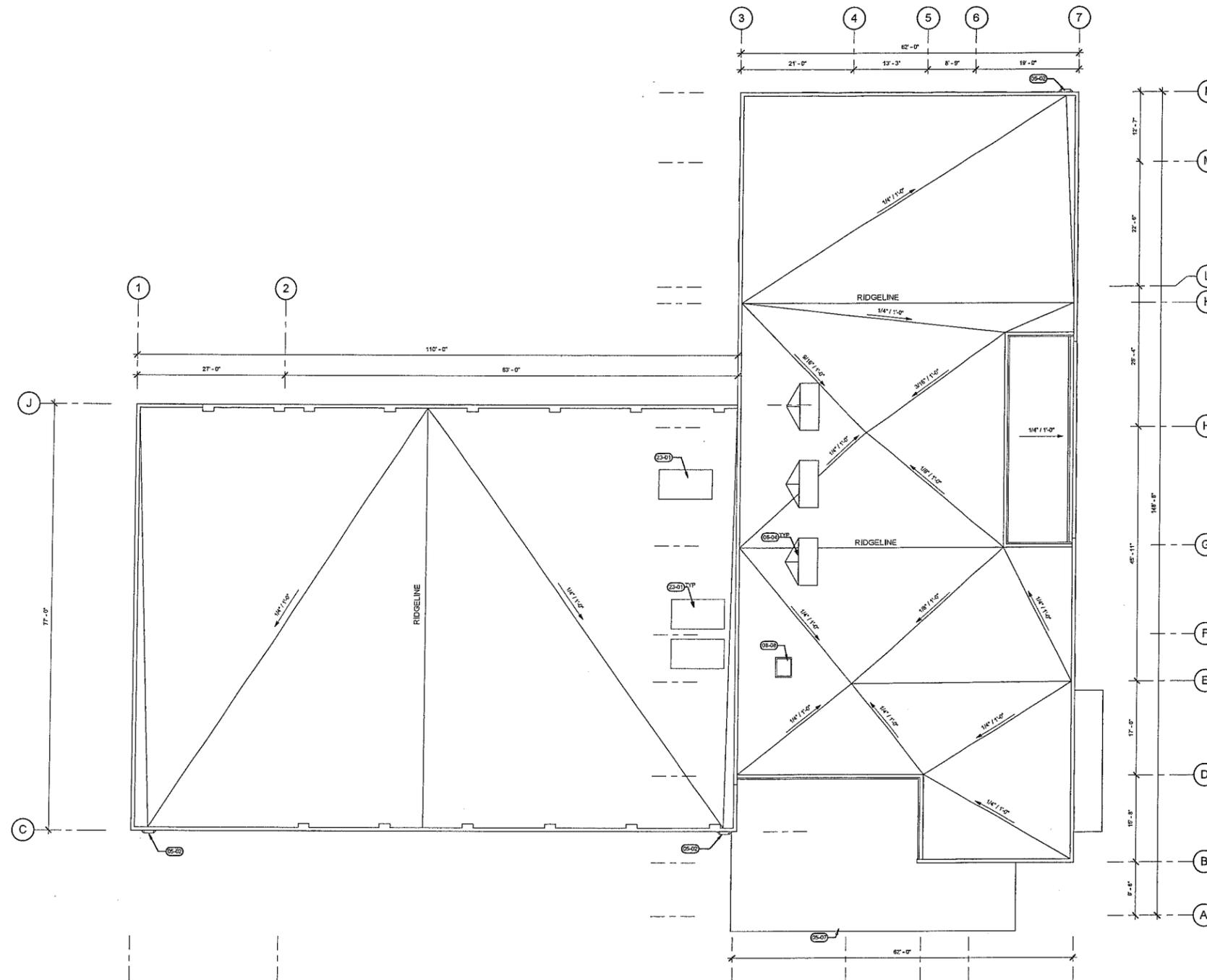
SHEET TITLE:
ROOF PLAN

DRAWN BY: ELW/LEJ
 CHECKED BY: JRM
 SHEET

A2.3

JOB NO. 2140087.02

CONDITIONAL USE SUBMITTAL: 09/14/15



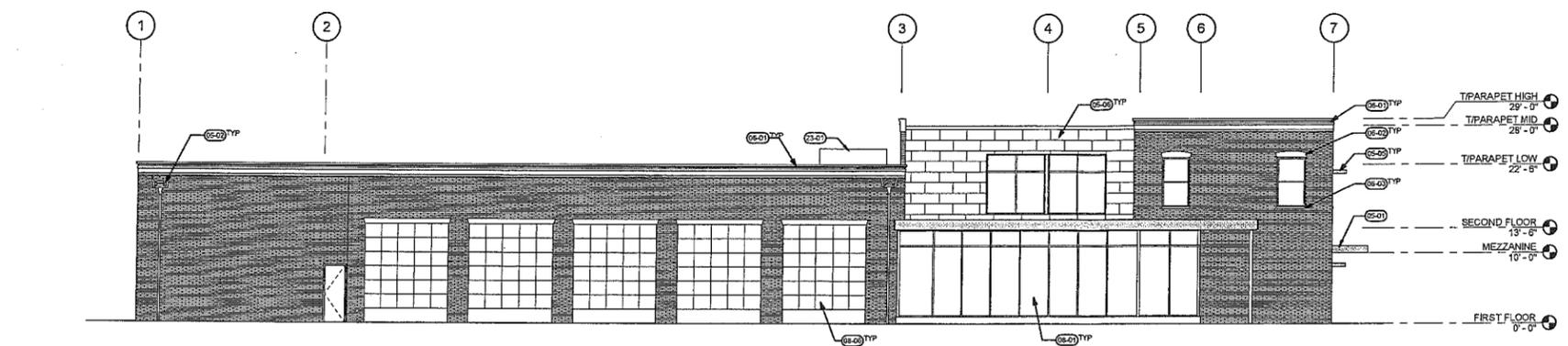
1 ROOF PLAN
 A2.3 1/8" = 1'-0"

GENERAL NOTES

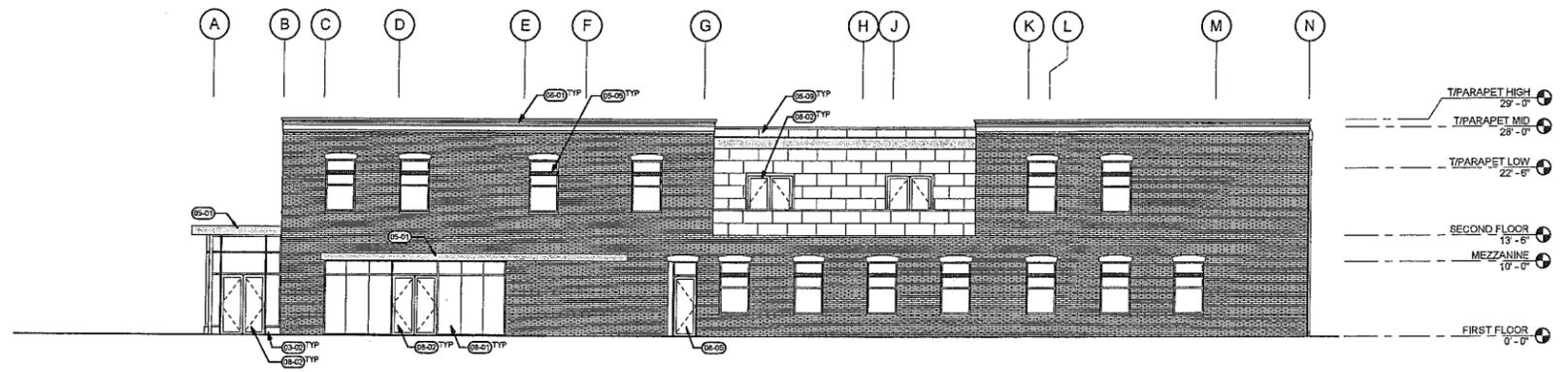
- A. MECHANICAL EQUIPMENT IS SHOWN FOR REFERENCE ONLY. COORDINATE EXACT LOCATION OF ROOFTOP MECHANICAL EQUIPMENT WITH ARCHITECT/MECH.
- B. FOR PIPE PENETRATIONS SEE MECHANICAL DRAWINGS.
- C. FOR MECHANICAL EQUIPMENT CURBS SEE MECHANICAL DRAWINGS.
- D. REFER TO DETAIL S1A.1 FOR TYPICAL ROOFING.
- E. ELEVATION (6'-0" CIVIL (P)) AS INDICATED ON CIVIL DRAWINGS.
- F. PLACE DOWNSPOUTS AT UP-SLOPE SIDE OF ALL ROOF TOP EQUIPMENT, ROOF HATCHES, ETC. AND WHERE SHOWN - MAINTAIN 1/4" PER FOOT MINIMUM SLOPE TO ENSURE PROPER DRAINAGE AT ALL PORTIONS OF ROOF.

KEYNOTES

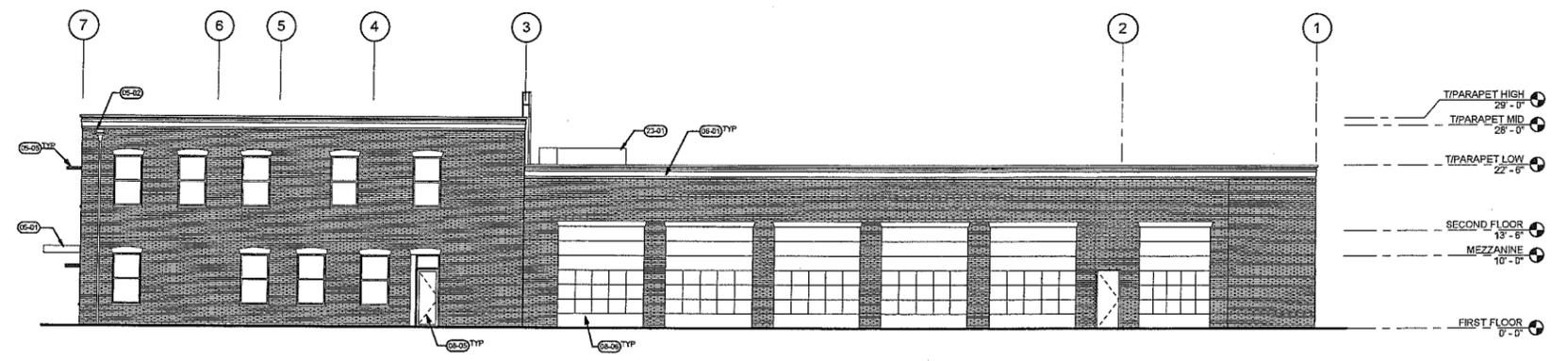
- 05-02 METAL DOWNSPOUT AND SCUPPER
- 05-07 EXTERNAL DOWNSPOUT
- 05-04 SKYLIGHT
- 05-08 ROOF HATCH
- 23-01 HYDRO UNIT, SEE MECHANICAL DRAWINGS



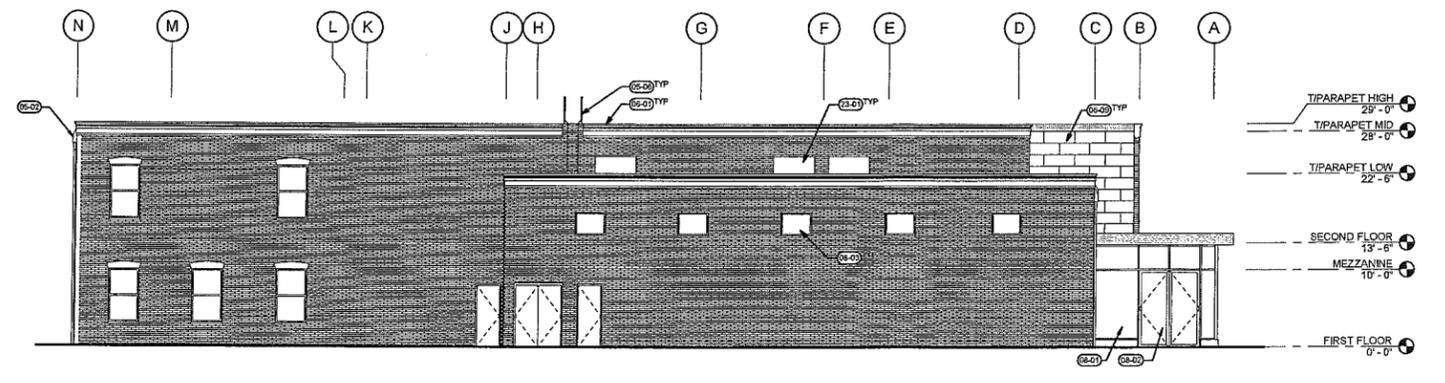
1 WEST ELEVATION
A3.1 1/8" = 1'-0"



2 SOUTH ELEVATION
A3.1 1/8" = 1'-0"



3 EAST ELEVATION
A3.1 1/8" = 1'-0"



4 NORTH ELEVATION
A3.1 1/8" = 1'-0"

GENERAL NOTES

- A. SEE ELEVATIONS FOR EXTERIOR WINDOW TYPE DESIGNATION
- B. ELEVATION 0'-0" FINISH FLOOR ELEVATION MAIN FLOOR INDICATED IN CIVIL DRAWINGS
- C. CONTRACTOR SHALL VERIFY AND CONFIRM ALL DIMENSIONS AND LAYOUT INFORMATION. NOTIFY ARCHITECT OF ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION. DO NOT SCALE DRAWINGS
- D. REFER TO ENLARGED ELEVATIONS WHERE INDICATED

KEYNOTES

- 05-02 12" CONCRETE CURB, BOARD FORM FINISH
- 05-01 EXTERIOR STEEL CANOPY - SEE STRUCTURAL
- 05-02 METAL DOWNROUT AND SCUPPER
- 05-06 PREFABRICATED METAL SUNSHADE, SEE SPEC
- 05-06 ROOF ACCESS LADDER AT PARAPET
- 06-01 PRE-MANUFACTURED FIBERGLASS CORNICE
- 06-02 COMPOSITE CAST STONE HEADER
- 06-03 COMPOSITE CAST STONE SILL
- 06-09 FIBER CEMENT PANELS - GRAY FINISH
- 08-01 STOREFRONT SYSTEM
- 08-02 EXTERIOR STOREFRONT DOORS
- 08-03 CLERESTORY WINDOWS
- 08-05 EXTERIOR PASSAGE DOORWAY WITH TRANSOM WINDOW
- 08-06 GLASS OVERHEAD DOORS, SEE DOOR SCHEDULE
- 23-01 HVAC UNIT, SEE MECHANICAL DRAWINGS

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Revision Schedule	
Revision	Issue Date

SHEET TITLE:
BUILDING ELEVATIONS

DRAWN BY: RLW / LEJ
CHECKED BY: JRH
SHEET

A3.1

JOB NO. 2140087.02



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Information

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Revision Schedule	
Revision Delta	Issue Date

SHEET TITLE:
**SITE PLAN -
PHOTOMETRICS**

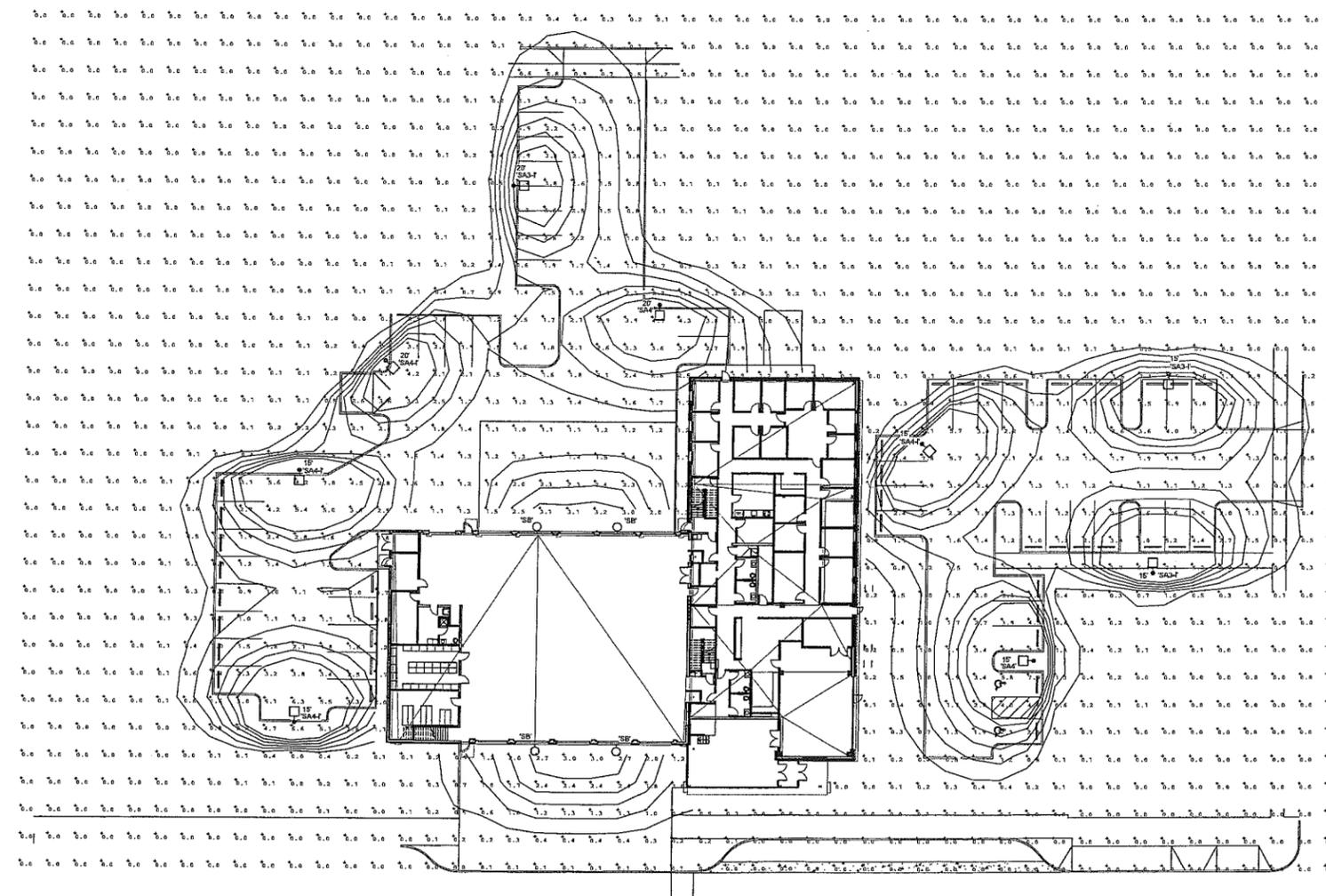
DRAWN BY: Author
CHECKED BY: Checker
SHEET

E1.1PH

JOB NO. 2140087.00

LUMINAIRE SCHEDULE											
TYPE	DESCRIPTION	HOUSING	SHIELDING	MOUNTING	FINISH	UL/IP RATING	BALLAST	LAMP(S)	INPUT WATTS	MFG/CATALOG #	NOTES
SA4	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 3 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 6.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	INTERNAL HOUSESIDE SHIELD	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLIVING GL18 SERIES, OR APPROVED	
SA4	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 4 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 6.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLIVING GL18 SERIES, OR APPROVED	
SA44	EXTERIOR ARCHITECTURAL LED SITE LUMINAIRE WITH TYPE 4 OPTICS	NOMINAL 18-INCH WIDE BY 31.5" DEEP BY 6.5-INCH DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	INTERNAL HOUSESIDE SHIELD	SINGLE	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	105W LED ARRAY OR 10,400 LUMENS, 4000K	105W	GARDCO GULLIVING GL18 SERIES, OR APPROVED	
88	EXTERIOR WALL MOUNTED CUT-OFF LUMINAIRE WITH TYPE 3 OPTICS	DIE-CAST ALUMINUM AND DIE-CAST DOOR FRAME	NONE	WALL MOUNTED	AS SELECTED BY ARCHITECT	WET	ELECTRONIC	25W LED ARRAY OR 3,600 LUMENS, 4000K	25W	GARDCO 107 GULLIVING SCORCE, OR APPROVED	

NOTES:
 1 THIS LUMINAIRE SCHEDULE IS NOT COMPLETE WITHOUT A COPY OF THE PROJECT MANUAL CONTAINING THE ELECTRICAL SPECIFICATIONS.
 2 FLUORESCENT BALLASTS: UNIVERSAL VOLTAGE, PROGRAM START WITH END-OF-LIFE PROTECTIVE CIRCUITRY.
 3 METAL HALIDE BALLASTS: PULSE START FOR LAMPS ABOVE 150 WATTS AND ELECTRONIC FOR LAMPS 150 WATTS AND BELOW, UNLESS OTHERWISE NOTED.
 4 DIMMING CONTROL: PROTOCOL (0-10VDC LINE VOLTAGE, DALL ETC) COMPATIBLE WITH LIGHTING CONTROL SYSTEM AS SPECIFIED AND SHOWN ON DRAWINGS.
 5 TS LAMPS: NOMINAL 4 FOOT LAMPS TO BE RATED FOR 20000 HOURS ON 3 HOUR SWITCHING CYCLES.
 6 TS LAMPS: NOMINAL 4 FOOT LAMPS TO BE RATED FOR 20000 HOURS ON 3 HOUR SWITCHING CYCLES.
 7 T8 LAMPS: NOMINAL 4 FOOT LAMPS TO BE RATED FOR 20000 HOURS ON 3 HOUR SWITCHING CYCLES AND 20000 HOURS ON 12 HOUR SWITCHING CYCLES.
 8 FLUORESCENT LAMPS TO HAVE 4000 COLOR TEMPERATURE AND 80- CRI FOR LINEAR LAMPS AND 90- CRI FOR COMPACT FLUORESCENT LAMPS UNLESS OTHERWISE NOTED.
 9 CERAMIC METAL HALIDE LAMPS: 80- CRI WITH MAXIMUM +/- 15% COLOR SHIFT OVER LAMP LIFE.
 10 PROVIDE +/- 12 INCH ADJUSTABILITY IN AIRCRAFT CABLE LENGTH WHERE USED.
 11 COORDINATE ALL CEILING TYPES WITH LUMINAIRE LOCATIONS PRIOR TO ORDERING LUMINAIRE. COORDINATE INSTALLATION WITH REFLECTED CEILING PLAN.
 12 SPECIFIED MANUFACTURERS ARE APPROVED TO SUBMIT (BID). INCLUSION DOES NOT RELIEVE MANUFACTURER FROM SUPPLYING PRODUCT AS DESCRIBED.
 13 PROVIDE SUBMITTALS THAT INCLUDE THE LUMINAIRE, LAMP AND BALLAST INFORMATION OF EACH LUMINAIRE, WITH APPLICABLE OPTIONS CLEARLY CHECKED OR UNCHECKED. SUBMITTALS NOT INCLUDING THIS INFORMATION WILL BE RETURNED AS REJECTED BY THE ENGINEER OF RECORD.
 14 REMOTE BALLASTS OTHER THAN LISTED FOR THEIR APPLICATION. BALLASTS OTHER THAN REMOTE BALLASTS ARE SUBJECT TO REMOVAL AND REPLACEMENT AT NO COST TO OWNER.
 15 (DELETE IF PROJECT IS NOT IN THE STATE OF WASHINGTON) PROVIDE COMMISSIONING OF THE LIGHTING IN ACCORDANCE WITH THE WASHINGTON STATE NON-RESIDENTIAL ENERGY CODE 191.3.7.
 16 (DELETE IF PROJECT IS NOT IN THE STATE OF CALIFORNIA) PROVIDE COMMISSIONING OF THE LIGHTING AND LIGHTING CONTROLS IN ACCORDANCE WITH CALIFORNIA TITLE 24 LIGHTING COMMISSIONING REQUIREMENTS.



1 SITE PLAN - PHOTOMETRICS
 1" = 20'-0"

PROJECT 2015-0045
 CONTACT Chris Larson
INTERFACE
 ENGINEERING
 100 SW Main St.
 Suite 1100
 Portland, OR 97204
 TEL 503.342.2266
 FAX 503.342.2262
 www.interfaceeng.com

CONDITIONAL USE SUBMITTAL:

ORDINANCE NO. _____

AN ORDINANCE AMENDING AMC 2.04.060 CONCERNING VOTING ABSTENSIONS AT CITY COUNCIL MEETINGS, AND DECLARING AN EMERGENCY.

WHEREAS, Section 14 of the Albany Charter authorizes and directs the City Council to, "...adopt rules for the government of its members and proceedings;" and

WHEREAS, pursuant to the aforesaid charter authority, the City Council has adopted rules set forth at Chapter 2.04 of the Albany Municipal Code; and

WHEREAS, from time to time it is appropriate to review and revise Council's rules to deal with unforeseen circumstances and/or as necessary to promote the timely and efficient conduct of City business; and

WHEREAS, Section 18 of the Albany Charter provides, in part, that the Mayor shall have no vote, except in the case of a tie vote of the Councilors then voting and, Section 20 of the Albany Charter requires the concurrence of four members of the Council to decide any question before the Council; and

WHEREAS, given that Section 7 of the Albany Charter provides that the Mayor is also a member of the Council, the procedure historically employed by the City Council, in the event of a 3-3 tie vote is to then have the Mayor vote to break the tie as he or she determines to be in the best interest of the City. This procedure is necessary to allow the efficient and timely conduct of City business when four members of the Council agree on an item of City business; and

WHEREAS, the efficient and timely operation of the Council and the fundamental democratic principal of majority rule is frustrated if and when a member of the Council abstains and thereby prevents a 3-3 tie that would otherwise allow the Mayor to break the tie and the matter to be decided by four votes as called for in the Charter; and

WHEREAS, an abstention in the foregoing circumstance prevents the Mayor from voting and thereby frustrates the purpose and intent of the Charter, prevents the efficient and timely conduct of City business, leaves important pending City decisions unresolved, and frustrates the democratic principal of majority rule; and

WHEREAS, this amendment to AMC 2.04.060 is called for to address and remedy the foregoing concerns.

NOW, THEREFORE, THE PEOPLE OF THE CITY OF ALBANY DO ORDAIN AS FOLLOWS:

Section 1 : AMC 2.04.060 is amended to read as follows:

2.04.060 Voting Abstentions

Unless a member of the Council states that he/she is not voting, his/her silence shall be recorded as an affirmative vote. ~~If In a member circumstance where the full Council states that he/she is not voting,~~ **his/her present and where a motion, properly before the Council, fails for want of the concurrence of four City Councilors, an abstention shall be deemed a vote is not against the motion. In all other circumstances, an announced abstention shall not be** considered, either for or against the proposition. Following ~~any~~ **the** vote, the City Clerk shall announce the vote count. If the Recorder's count is disputed by any Council member, a roll call shall be taken to decide the issue.

Section 6: Emergency Clause. In as much as this ordinance is necessary for the immediate preservation of the public peace, health, and safety of the City of Albany, or to facilitate the prompt and timely completion of important City business, an emergency is hereby declared to exist; and this Ordinance shall take effect and be in full force and effect when signed by the Mayor.

Passed by the Council: _____

Approved by the Mayor: _____

Effective Date: _____

Mayor

ATTEST:

City Clerk



TO: Albany City Council
VIA: Wes Hare, City Manager *WH*
FROM: Human Relations Commission
DATE: January 6, 2016, for the January 13, 2016, City Council Meeting
SUBJECT: Revising Albany Municipal Code Chapter 2.25, Human Relations Commission

RELATES TO STRATEGIC PLAN THEME: ● An Effective Government

Action Requested:

Revise AMC Section 2.25.030, Term of office and initial appointment, to remove the following restriction: *Members may not serve more than six consecutive years on the Commission. After serving six consecutive years, a member must leave the Commission for one year before being eligible for reappointment.*

The attached ordinance shows the HRC's request of deleting the language in AMC Section 2.25.030 shown in double strikethrough.

Discussion:

Given that we do not generally have restrictions on the terms of our board and commission members and it is sometimes difficult to find volunteers, I see no problem in doing away with this requirement. I do not recall the reason for the restriction, but I believe it may have just been included because it was in the template we revised from another city.

There are only two other City board-commissions that have a restriction regarding how many years members can serve. One is the Library Board with the restriction set by ORS 357.465(3). The second is the Hospital Facility Authority with the restriction set by Council policy. Both of these boards have the following restriction: Members cannot serve more than two full consecutive terms but may be reappointed after a one-year interval.

Budget Impact:

None.

WH:ldh
Attachment

G:\Administrative Services\City Manager's Office\Human Relations Commission\Memo to CC-revising AMC 2.25.030-1 13 16.docx

ORDINANCE NO. _____

AN ORDINANCE AMENDING ALBANY MUNICIPAL CODE (AMC) 2.25, ALBANY HUMAN RELATIONS COMMISSION

WHEREAS, after discussion at its December 15, 2015, meeting, the Human Relations Commission is requesting that the City Council consider amending AMC Section 2.25.030, Term of office and initial appointment, to remove the following restriction: *Members may not serve more than six consecutive years on the Commission. After serving six consecutive years, a member must leave the Commission for one year before being eligible for reappointment.*

NOW, THEREFORE, THE PEOPLE OF THE CITY OF ALBANY DO ORDAIN AS FOLLOWS:

Section 1: AMC Chapter 2.25 is amended to read as follows:

AMC 2.25
Albany Human Relations Commission

AMC 2.25.005 Established.

There is hereby established a Human Relations Commission for the City of Albany which shall consist of seven (7) members. Each Councilor and the Mayor shall appoint one member to the Commission; the appointment will then be ratified by the City Council.

AMC 2.25.010 Purpose.

The Human Relations Commission is established by the Albany City Council to promote harmonious relations among the citizens of Albany. It is created to recommend programs, activities, ordinances, expenditures, and other appropriate governmental activities which will serve the goal of maintaining respectful interactions within our community.

AMC 2.25.020 Criteria to be considered for appointment.

It is expected that each appointee will be committed to the philosophy of inclusion, equal opportunity, and fair treatment for all residents of Albany. The City Council will endeavor to appoint commissioners who represent a cross-section of the entire community. Appointees must reside within the Albany city limits.

AMC 2.25.030 Term of office and initial appointment.

The initial membership of the Human Relations Commission shall be established by the Mayor and Council. Three of the initially appointed members shall serve a three-year term, two of the initially appointed members shall serve a two-year term, and two of the initially appointed members shall serve a one-year term. Initial terms of appointment will be staggered according to the alphabetical order of the appointee's surname, beginning with three-year terms for those closest to the beginning of the alphabet. Thereafter, as the terms expire, the Mayor and each Councilor, shall appoint replacement members for three-year terms. Members shall serve without compensation. ~~Members may not serve more than six consecutive years on the Commission. After serving six consecutive years, a member must leave the Commission for one year before being eligible for reappointment.~~ All members of the Human Relations Commission will serve at the pleasure of the Council and may be removed from their position by the Council at any time without cause. Any vacancies in the Commission shall be filled through appointment by the appropriate Councilor and ratification by the City Council.

AMC 2.25.040 Duties and responsibilities.

The Human Relations Commission shall meet periodically in public sessions at dates, times, and places as may be established by the Commission. The Commission shall have the duty to make recommendations to the City Council and the City Manager as outlined in AMC 2.25.010. In formulating its recommendations, the Human Relations Commission shall strive for actions that lead to positive and respectful relations in the community. The duties of the Commission will not include the investigation or sanctioning of individual acts of unlawful or discriminatory behavior but will, instead, focus on recommending community activities that may be expected to promote positive change. The duties of the Human Relations Commission may be revised at any time, after public notice, by resolution.

AMC 2.25.050 Procedures.

The Human Relations Commission shall elect a chairperson and a vice chairperson. A majority of the members shall constitute a quorum. The Commission shall keep a journal of its proceedings, which shall be kept on file in the office of the City Recorder.

Passed by the Council: _____

Approved by the Mayor: _____

Effective Date: _____

Mayor

ATTEST:

City Clerk



TO: Albany City Council
VIA: Wes Hare, City Manager
FROM: John R. Bradner, Fire Chief *JR*
DATE: January 7, 2016, for the January 13, 2016, City Council Meeting
SUBJECT: Intergovernmental Agreement with Lebanon and Tangent Fire Districts for a Shared Medic Unit
RELATES TO STRATEGIC PLAN THEME: ● A Safe City

Action Requested:

City Council approval to enter into an Intergovernmental Agreement with Lebanon Fire District and Tangent Rural Fire Protection District to collaboratively staff a single-role medic unit (Medic 71) for medical responses in each department's district; approval to reclassify one Emergency Medical Technician (EMT) to Paramedic; and approval to add up to four part-time EMT or Paramedic positions to maintain Medic 71 staffing.

Discussion:

The Fire Department responded on 7,657 emergency calls in 2015, an unprecedented 24.6 percent increase over the past five years. This increased workload occurred while the Department experienced a five percent reduction in emergency response personnel. The combination of more calls with less staff is creating a negative impact on personnel and our ability to provide emergency medical and fire services to our community.

During the FY 2015-16 budget process the Fire Department received approval to add two EMT positions to staff a Basic Life Support (BLS) medic unit for non-emergency medical calls. These positions were EMTs only and would not provide fire or rescue services. The intent was to provide lower cost positions that could reduce the impact of the increasing workload on dual-role firefighter/paramedics that respond to all emergencies, including fire suppression, rescue, and BLS and Advanced Life Support (ALS) medical services.

Lebanon Fire District was also preparing to establish a similar program to address their increase in emergency responses, specifically patient transfers between hospitals; a situation our Department also deals with on a daily basis. It quickly became evident that collectively the two departments could staff an ALS/BLS unit in place of the BLS unit originally proposed. A shared ALS/BLS unit will provide a higher level of service, be available more days per week, and be staffed more consistently to the benefit of both communities.

Over the past several months Albany and Lebanon departments, working with Tangent Fire District, have created a collaborative program called Medic 71. Albany and Lebanon will each provide two 36-hour EMTs or Paramedics to staff an ALS medic unit to be centrally located at Tangent's fire station. This unit will respond to emergency medical calls in each district, with a focus on hospital transfers. Doing so will allow dual-role units from each department to remain in their district more often to handle the increased call volume for fire and medical emergencies in each community.

Albany City Council

Page 2

January 7, 2016

The request to reclassify one EMT position to a Paramedic position is required to allow ALS capabilities for the shared medic unit above the previously approved positions for a BLS level of care. The request for up to four additional part-time positions, working a minimum of 12 hours per month, will provide a pool of qualified, trained staff to cover vacancies on the shared medic unit to increase reliability of the unit being staffed.

Budget Impact:

The costs associated with reclassifying one position and creating four new part-time positions are supported in the Fire Department's current budget.

JB

Attachment

RESOLUTION NO. _____

A RESOLUTION TO ENTER INTO AN INTERGOVERNMENTAL AGREEMENT WITH LEBANON FIRE DISTRICT AND TANGENT RURAL FIRE PROTECTION DISTRICT; RECLASSIFY ONE EMT POSITION TO A PARAMEDIC POSITION; AND ADD UP TO FOUR PART-TIME EMT OR PARAMEDIC POSITIONS FOR AN ADVANCED LIFE SUPPORT MEDIC UNIT TO RESPOND TO MEDICAL EMERGENCIES WITHIN ALBANY AND LEBANON AMBULANCE SERVICE AREAS.

WHEREAS, the Fire Department was approved in its FY 2015-16 budget to add two EMT positions for the purpose of establishing a daytime medic unit for basic life support (BLS) medical calls within Albany's ambulance service area; and

WHEREAS, the Fire Department has determined that in cooperation with the Lebanon Fire District and Tangent Rural Fire Protection District, the agencies can provide an advanced life support (ALS) medic unit that will provide a higher level of medical care, be available more days per week, and be staffed more consistently for response within Albany's ambulance service area; and

WHEREAS, Lebanon Fire District and Tangent Rural Fire Protection District also recognize the advantages to their departments and upon approval of their respective Boards support the concept of a shared ALS medic unit; and

WHEREAS, in order to increase the level of care from BLS to ALS as originally approved in the FY 2015-16 budget process, the Fire Department would need to reclassify one EMT position to a Paramedic position; and

WHEREAS, in order to maintain more consistent staffing on the shared medic unit, the Fire Department has identified the need to have a pool for qualified, trained EMT and/or Paramedics on call to cover for vacancies on the shared ALS medic unit

NOW, THEREFORE, BE IT RESOLVED, the Albany City Council authorizes the Fire Chief to enter into an Intergovernmental Agreement with Lebanon Fire District and Tangent Rural Fire Protection District for the purpose of a shared ALS medic unit to respond to emergency medical calls within Albany and Lebanon ambulance service areas; and

BE IT FURTHER RESOLVED, the Albany City Council approves the reclassification of one EMT position to a Paramedic position and the addition of up to four part-time EMT or Paramedic positions to provide for staffing of the shared ALS medic unit.

DATED AND EFFECTIVE THIS 13TH DAY OF JANUARY 2016.

Mayor

ATTEST:

City Clerk

CITY OF ALBANY
 CITY COUNCIL
 Council Chambers
 Wednesday, October 14, 2015
 7:15 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 7:15 p.m.

PLEDGE OF ALLEGIANCE TO THE FLAG

Konopa led the pledge of allegiance to the flag.

ROLL CALL

Councilors present: Mayor Sharon Konopa and Councilors Rich Kellum, Bill Coburn, Bessie Johnson, Ray Kopczynski, Dick Olsen, and Floyd Collins.

Councilors absent: None.

SPECIAL PRESENTATION

Government Financial Officers Association (GFOA) Awards.

Roger Dawes, Controller at Washington County, is a member of the board of GFOA (Government Finance Officers Association). The Oregon Municipal Financial Officers Association (OMFOA) is a chapter of GFOA. OMFOA has 500 members from counties, cities, school districts, special districts, and other government entities, along with partners from associated businesses such as banks, software companies, attorneys, and bond underwriters, and was created to promote professionalism and excellence in public financial management. Dawes said his role is to serve as GFOA's representative. GFOA was founded in 1906 and represents public finance officials throughout the United States and Canada. It has 18,000 members from federal, local, and state government. GFOA provides best practices guidance, consulting, networking opportunities, publications, recognition programs, research, and training.

Dawes said he would present three awards: the Popular Financial Reporting Award (PAFR), the Distinguished Budget Presentation Award, and the Certificate of Achievement for Excellence in Financial Reporting.

Dawes said the PAFR was established in 1991 to encourage governments to extract information from their comprehensive financial reports to produce financial reports designed to be readily accessible and understandable to the general public. The reports are judged by four judges in five categories that give varying weights of importance towards the overall final grade. Categories include reader appeal, understandability, distribution methods, and other; and overall quality and usefulness. Oregon has six award recipients out of 48 nationwide. Dawes said the Albany Finance Department is in elite company and the City Council should be very proud. This is the third consecutive year Albany has received the award.

The second award is for the City's budget. The GFOA established this award program in 1984 to help governments produce the highest quality budget documents, based on guidelines and best practices. Budgets are judged on 27 criteria in four basic categories. He explained the scoring. Oregon has 51 recipients, 23 of which are cities. This is the 25th consecutive year Albany has received this award. Albany also received special recognition for the performance measures included in the budget. Only two cities in Oregon received this special recognition.

The third award is for the City's Comprehensive Annual Financial Report (CAFR). The award was established in 1945 to help governments go beyond the required minimums to develop financial reports that shows transparency and full disclosure. Dawes feels this is the most difficult award to achieve. The GFOA's checklist for the award is over 79 pages long, with over 500 items. One failure to meet requirements can disqualify the entire report. Thirty-eight Oregon cities received this award. This is the 31st consecutive year Albany has received the award.

Dawes said the citizens of Albany can be assured that the City's financial position is transparent, fully disclosed, and of the highest quality, comparable at the national level. In addition to Senior Accountant Anne Baker, Senior Accountant Jeanna Yeager, and Finance Director Stewart Taylor, Dawes recognized Finance staff, City Manager Wes Hare, Mayor Sharon Konopa, and the Albany City Councilors for establishing an environment of excellence.

Dawes presented Senior Accountant Anne Baker with the GFOA 2013-2014 Award for Outstanding Achievement in Popular Annual Financial Reporting and the 2014-2015 Distinguished Budget Presentation Award on behalf of the City of Albany.

Dawes presented Senior Accountant Jeanna Yeager with the GFOA 2013-2014 Certificate of Achievement for Excellence in Financial Reporting on behalf of the City of Albany.

Konopa said Albany is very fortunate to have staff of this quality. She congratulated Baker, Yeager, and Taylor on a great job.

SCHEDULED BUSINESS

Public Hearing

Accepting conveyance of City-owned real property to the Trustees of the Frank and Winifred Moore Revocable Living Trust and reserving an easement.

There was a revised Quitclaim deed on the dais, which is an attachment to the resolution (see agenda file).

Konopa opened the public hearing at 7:20 pm.

Hare said that when the City bought the property for the new fire station, they discovered that neighbors, the Moore's, believed they owned about eight feet of the property and had been using it. Hare said they came up with a solution that satisfies everyone's interests. The City did not want to disrupt the Moore's garage or yard, and is not interested in making money on the transaction. However, the City does want to ensure that we would not potentially lose parking spaces as a result of being too close to their property line. Community Development interpreted that the eight feet could be used as a buffer that would ordinarily have to be on the Fire Department's side of the property. This led to the conclusion that the City could give the piece of property to the Moore's so they would have unencumbered title to the property and be able to sell it, while the City would retain an easement for the purpose of ensuring that the landscape buffer would not cause the City to lose the use of our property. Hare said he thinks both parties were in agreement although it took a while to come up with the plan. He thinks this transaction is to everyone's benefit.

Konopa asked if anyone in the audience wished to speak. No one did.

There was no one signed up on the signup sheet (see agenda file).

Konopa closed the public hearing at 7:27 p.m.

MOTION: Councilor Floyd Collins moved to approve the resolution, including the revised Quitclaim deed attachment on the dais, and Councilor Ray Kopczynski seconded it. The motion passed 6-0 and was designated Resolution No. 6453.

Business from the Public

Michael Lesmeister, 3391 Lyon Street SE, said he was present to support West Coast Alternative Medicine, the business across the street from him. He has lived there for 13 years. They have had no complaints, and no trouble with the owners, who are nice people. The prior owner was not good. He said, Michael and Ricky bought the business and cleaned it up, so it looks nice. He doesn't care if they are there. They are people who run a business to support their families. He wants them to stay.

Ken Fonder, 3371 Lyon Street SE, said he also lives across the street from West Coast Alternative Medicine. He is a homeowner who thinks there may be some concern that homeowners aren't being represented. The owner of West Coast contacted neighbors at the beginning to discuss parking and traffic issues. He has security, and has cleaned up and upgraded the property. The previous owner had issues with the neighbors from the start. The new owner is a positive influence on the property. Fonder thinks the City should let him be a businessman. Instead of banning recreational marijuana, the City should wait a year so West Coast can develop a track record.

Konopa asked if the Council wanted to hear more on the marijuana issue that evening. She suggested that the City Attorney could come back to a future work session to present all possible options. City Attorney James Delapoe said it was his suggestion to the Mayor to dedicate a future work session to the subject. He said banning recreational marijuana is not on tonight's agenda and he is not sure where that story originated. He said it's clear that it's difficult for the Council to figure out what all the options are, and without knowing all the options it is difficult to choose. He suggested holding a work session dedicated solely to the subject, and the City Attorneys would list all of the Council's options and decision points. He suggested devoting the last half hour of the work session to public comment in case the public thinks there are other options. The meeting's purpose would not be to advocate for one option or another, but to agree on what the choices are and the legalities that accompany each choice. The choices can be listed in a document so it is clear. Once the Council knows what their choices are, they could choose to take action.

Kopczynski said that sounds plausible but he thinks there's a time crunch for some of the options. He wants to do it sooner rather than later. City Attorney Sean Kidd said it would be preferable to have some decisions made by the end of the year. The Oregon Liquor Control Commission (OLCC) will start accepting applications for recreational marijuana facilities on January 4, 2016. The OLCC says they won't issue licenses until late summer

or early fall, but the gray area is that in that process, once they receive an application, they will send a letter of compliance to the City to see if the applicant meets the City's current land use criteria. The City then has 21 days to respond. If we get a letter of compliance before the Council has made decisions, City staff will have to evaluate the letter based on current code. So, if the Council wants to make Code changes, they need to do it sooner rather than later. It doesn't have to be done on Monday, but the City Attorneys could present the options then if the City Council would like.

Kopczynski said he would like to do it at Monday's work session.

Councilor Bessie Johnson agreed. She would like to get all the Council's options in writing.

Kidd clarified that there are a variety of options. He and Delapoer will list the general options and the Council can revise them. The options can be ready by Monday, but all of the Councilors should be present. Delapoer clarified that the goal of that meeting should not be to pick an option, but to get on the table the various choices available and the associated timelines.

Councilor Rich Kellum said the bottom line is to do it right. He said, we have a history of doing things quickly and then there are unintended consequences. He thinks it is better to take two or three weeks and be sure to get it right.

Delapoer said that would give the attorneys time to make sure they don't miss anything. It would also allow time for staff to discuss whether regulations should be land use regulations or contained in the Municipal Code, and what the process should be, so that the Council knows the impact of their decisions. Delapoer reminded the public that the public input would not be to advocate one way or the other, but to make sure all the choices are on the table. Then, at another public meeting, the Council would decide what they want to do.

Councilor Dick Olsen wants the meeting to be in the evening.

The meeting was set for Monday, November 2, 2015, at 7:15 p.m.

Kellum asked that the list of options be emailed to the Councilors as soon as possible. Delapoer said it could also be put on the website for the public to see.

Tom Cordier, 2240 Park Terrace NW, asked if the list will be available on the Friday before the work session. Konopa said yes.

Skyler McCollaum, 2175 Pine Meadow Drive SE, said he represents combat veterans in Albany. He said they sincerely appreciate all the Council is doing. He said, we really do want cannabis. It's one of the only things that help him, whereas pills and alcohol, which are readily available, do not. It isn't smoking it, but eating it that helps. Unless it's readily available in all forms, they are unable to live here and have businesses here. Konopa said medical marijuana is already allowed in Albany. McCollaum described the difference between the flowers and the other parts of the plant in making treatments.

John McNeil, 1735 23rd Court SE, said he's talked to his doctor about marijuana. Without marijuana he is unable to walk. He has to go to a dispensary to get it. He wants the dispensaries to stay open. He has met older people like himself that have similar problems. Konopa said the Council is not considering closing down the medical marijuana dispensaries.

First Reading of Ordinance

Amending Albany Municipal Code Chapter 2.27, Community Development Commission.

Konopa said this ordinance clarifies language in the Code about Community Development Commission appointments.

Delapoer read the Ordinance for the first time in title only: AN ORDINANCE AMENDING ALBANY MUNICIPAL CODE CHAPTER 2.27, COMMUNITY DEVELOPMENT COMMISSION.

MOTION: Councilor Bill Coburn moved to read the Ordinance for the second time in title only. Kopczynski seconded the motion and it passed failed 5-1, with Kellum voting no. It takes a unanimous vote of the Council for a second reading in the same meeting. This item will come back to the October 28, 2015, Regular Session for a second reading.

Adoption of Consent Calendar

- 1) Approval of Minutes
 - a) August 12, 2015, Regular Session minutes.
 - b) August 31, 2015, Special Meeting minutes.
- 2) Accepting a sanitary sewer and storm drain easement from Glorietta Bay, LLC.

RES. NO. 6454

- 3) Executing a quitclaim deed for the release of the western 370 feet of a 420-foot wide sewer easement across tax lot 11S03W18BD-00501. RES. NO. 6455

MOTION: Coburn moved to adopt the Consent Calendar as presented. Kellum seconded the motion and it passed 6-0.

Report

Code Enforcement Team annual report for Fiscal Year 2014-2015.

Hare said that the Code Squad does a good job with limited resources. There is discussion about transferring some responsibilities to the Police Department and relying more on Community Service Officers (CSO). He said, finding the right balance between resolving neighbor disputes and limiting people's use of their property is one of the most difficult things we do. In general, the Code Squad does an outstanding job by working to consider the interests of all the parties involved in disputes. Konopa agreed, and said she hopes the Council will consider adding additional staff to handle the work load.

Kopczynski asked where the Big Pickup will be in 2016. Public Information Officer Marilyn Smith said that is usually decided in January.

Coburn asked about progress on 2215 Jefferson Court SE. Smith said bids have been received, and demolition will happen in November. The neighbors will be happy when it comes down.

Coburn asked about the property on 33rd Avenue. Smith said a CSO spoke to the new manager of the apartments on the north side, where the problem is. The manager has hauled off three dumpster loads of trash and moved the dumpster into the parking lot from the cul-de-sac. She is telling tenants that they can't congregate or dump stuff there. The neighbors still would like that end of the cul-de-sac fenced off. There's a continuing problem at the complex, which is also a problem complex for the Police Department. The bulb of the cul-de-sac is not City right-of-way. If it were fenced, we would have to maintain fire access. The CSO is hopeful the new manager will take care of the problem. Discussion followed about the complex.

Johnson asked if the new owners of the old radio station on Ninth Avenue were doing anything about the problems there. Smith said she hasn't had an update on the property, but it has been boarded up and secured.

Smith said that 4030 Shortridge Street SE, which has been vacant for five years, is up for auction on October 20. It has been pulled from auction a couple of times. Discussion followed about why lenders pull properties from auction.

Collins said the owner of the building next door to the KRKT building says he has rat problems and broken windows in his own building. Collins asked what the Council can do to put pressure on the owners of the vacant building to do something about the rats. Konopa said a nonprofit bought the building for transitional housing, but the property isn't zoned for that. She can give their information to Smith to contact them about the problems. Discussion followed about mixed-use zoning. Hare said commercial and residential uses can conflict, so we try to separate them without being too restrictive.

BUSINESS FROM THE COUNCIL

Olsen said he has talked to Fire Chief John Bradner about the possibility of enlarging the lobby at the new fire station to accommodate more than one engine. Bradner said he's reluctant to do that because they need the sight distance. Olsen said Bradner told him that ODOT says the current stop light on Lyon Street will have to be removed when the new station is built. Olsen asked if ODOT is prepared to take it out if the City leaves it there. Hare said the City would probably have to pay the cost to remove it. Olsen asked if we should raise the issue that it doesn't make sense to remove a stoplight that has prevented accidents. Hare said Traffic Engineer Ron Irish has worked effectively with ODOT for years. Hare will talk to him about it.

City Engineer Staci Belcastro said she did not attend the meeting with Irish and ODOT, but she understands that progress was made. Initially the City was looking at full traffic signals on Lyon and on Ellsworth, but ODOT said they don't think that's necessary. The City proposes a crosswalk on Lyon Street, and ODOT favors stop signs before the station driveways. Irish will discuss options further with ODOT.

Olsen mentioned the recent bomb threat at Central School. A teacher told him, "the cops were awesome." Olsen thanked the Police and Fire Department for their response to the threat.

Kellum said the crosswalk at Les Schwab has no lights, and also the one on Waverly Drive, south of Grand Prairie Drive. He asked, are these in the Capital Improvement Program? Public Works Engineering Director Jeff Blaine said Public Works is working with ODOT on the lights at Les Schwab. Belcastro said the plans are complete. Public Works is working to verify that there's enough right-of-way for the poles. The plans could be advertised as early as November.

Blaine said Public Works has applied for grant money for the other location. Irish thinks they will get the grants, but the process takes a while.

Konopa mentioned Ed Melvin, who asked at Monday's Work Session for free bus passes or Call-A-Ride transportation. She spoke to him by phone for 45 minutes the week before, when she explained the process and told him that if we gave him free passes we would have to do it for others. Many others in community are handicapped and the City doesn't have the means to give them all rides whenever they would like. She suggested he contact Disability Services for help. She doesn't know if he made contact with them or not. She asked the Councilors if they wanted to have staff pursue the issue.

Collins asked if this is a service that could be funded by Community Development Block Grant (CDBG) for disabled vets and people on Social Security Income. Hare said he isn't sure, but he can look into it. Collins said it could be a valuable service, and he thinks CDBG would be an appropriate source. Smith said the Cascades West Rideline already does that at no charge. Discussion followed. Hare said he will report back to the Collins about the services available.

Konopa said she received an e-mail request to ban fireworks in Albany or to step up enforcement against illegal fireworks. She asked if the Council wanted to pursue the issue. Coburn said he thinks Albany is already more restrictive on sales than Linn County. Hare said he doesn't think Albany is more restrictive than state law. Discussion followed about restrictions and enforcement. The Council was not interested in having staff pursue this further.

Konopa mentioned an email from Mr. Talley, who wants a train-whistle quiet zone. He contacted ODOT Rail and National Rail and still wants the City to look into quiet zones. Irish responded with an e-mail and explained that a quiet zone could cost \$300,000 to \$500,000 per crossing. Talley still wants the Council to pursue it. Konopa asked if the Council wants to have a study done.

Kellum asked, what do we want to cut in order to do this? Kellum said, Talley moved in next to a railroad. Collins said the North Albany line has very few trains, but he can clearly hear trains on the main line from his home in North Albany. He thinks that it would be good to do a brief staff report to provide the definition of a quiet zone and the cost involved.

Hare described studies done on quiet zones in LaGrande when he was City Manager there. He thinks Irish did a good job of summarizing the costs.

Delapoe said the City Council can't adopt an ordinance that could be enforced against the railroad. All the City can do is ask the Federal Rail Administration to adopt a rule, which may be subject to change by the federal government.

Collins said documenting the discussion and Irish's response to Talley would memorialize the information for future City Councils.

RECESS TO EXECUTIVE SESSION TO DISCUSS LITIGATION LIKELY TO BE FILED IN ACCORDANCE WITH
ORS 192.660 (2)(h)

The Council recessed to executive session at 8:42 p.m.

RECONVENE

The Council reconvened at 8:47 p.m.

MOTION: Collins moved to execute the settlement agreement with Tornado Soft as outlined by the City Attorney. Kellum seconded the motion and it passed 5-1, with Coburn voting no.

NEXT MEETING DATE: Work Session: October 26, 2015, and Regular Session: October 28, 2015

ADJOURNMENT

There being no other business, the meeting was adjourned at 8:48 p.m.

Respectfully submitted,

Mary A. Dibble, MMC
City Clerk

Reviewed by,

Stewart Taylor
Finance Director

CITY OF ALBANY
CITY COUNCIL WORK SESSION
Municipal Court Room
Monday, October 26, 2015
4:00 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 4:00 p.m.

ROLL CALL

Councilors Present: Councilors Bill Coburn, Bessie Johnson, Rich Kellum, Ray Kopczynski, and Dick Olsen

Councilors Absent: Councilor Floyd Collins (excused)

BUSINESS FROM THE PUBLIC

None.

FINANCIAL POLICIES REVIEW

Finance Director Stewart Taylor said the City Council reviews and adopts the City's Financial Policies, Investment Policy, and Risk Management Policy on an annual basis. Adoption of the policies will be included on the October 28, 2015, Council Meeting agenda. At this time, Council is asked to give direction regarding any questions or revisions.

Stewart said there are no recommended changes in the Financial Policies this year. He drew attention to the Policy around Debt, noting the Securities Exchange Commission (SEC) had previously identified failings that resulted in Council direction to implement a periodic review of all the securities and ensure that the City is current on filings due with those instruments. He reported that the periodic review was adopted administratively in March and staff feels confident the issues raised by the SEC have been addressed. He said that a review by Standard & Poor's in January resulted in a rating increase for the City that was of benefit in issuing bonds for the police and fire facilities.

Councilor Dick Olsen asked how high the City's rating could potentially go. Taylor said the current rating is about as high as the City can get with our demographics. The rating represents the community's ability to pay.

In response to an inquiry from Councilor Ray Kopczynski, Taylor briefly discussed the PERS liability.

Sally Walton, Government Portfolio Advisors (GPA), was asked to provide a market update and discuss the Investment Policy, which sets parameters for investment of available cash not needed to meet current obligations. Walton said the interest rate was expected to increase last September but that did not happen due in part to economic slowdown overseas. The two-year Treasury note is unchanged from last year. Going forward, market consensus seems to be for a rate hike in 2016. Walton provided information about the City's portfolio and returns. With regard to the Investment Policy, GPA is recommending to separate certificates of deposit and bank deposits into individual line items, and to clarify the ratings limit on corporate and municipal bonds to AA- for Standard & Poor's and Aa3 for Moody's Investors Services.

Steve Uerling, Barker Uerlings Insurance, distributed and reviewed the City's Risk Management Report, dated October 26, 2015 (see agenda file). The report included a Schedule of Property and Liability Insurance, and information regarding premiums, deductibles, and losses in Property, Tort Liability, Automobile Liability, and Automobile Damage, both Citywide and by Department.

Kopczynski asked if the reduced amount of earthquake insurance will be sufficient to cover the new police and fire buildings. Uerlings explained that the previous insurance carrier, which had insured the City at \$100 million, pulled out of Oregon. Travelers now provides earthquake coverage and the City of Albany currently has \$50 million in coverage. Cities in the City/County Pool have a \$10 million limit per city and a total of \$150 million. In the event of major damage, some cities will get cents on the dollar. Corvallis and Albany are the only two cities he insures that have the full \$50 million coverage. The modeling showed damage in a 500-year event was about \$32 million so the City should be covered even with the new facilities. Taylor noted that Uerlings has encouraged a reassessment of City properties.

HIGHWAY 20 SHEEP CREEK BRIDGE UPDATE

Jerry Wolcott, Oregon Department of Transportation (ODOT), gave a presentation on US20: Sheep Creek Bridge Phases 1 & 2 (see agenda file). The presentation included information regarding a large slide in the area of Sheep Creek Bridge on Highway 20 east of Sweet Home and a two-phase solution. Phase 1 will utilize a "picket fence" project to redirect and reduce the slide activity. Phase 2 will involve a lengthy process to fix the bridge: close the road, demolish the east end of the bridge, remove dirt under the east end, refill with rock, rebuild the bridge, pave,

stripe, and reopen the road. The project is estimated to take six to eight weeks and is scheduled to occur during the construction weather window of August and September 2016, after the Oregon Jamboree.

Wolcott reviewed detour routes and traffic volumes, consideration and issues associated with a detour structure, and efforts to shorten the highway closure, including contractor pre-qualification and incentives/disincentives per day for early/late. The project is still under development and ODOT will look for any way possible to shorten the closure time. Other potential risks are that El Nino could delay the start of Phase 2 or activate a landslide before the fixes can be completed, in which case, full replacement would take about a year; and fire or fire danger could delay completion. ODOT is trying to get this information out early and he asked for help in spreading the word. Brief discussion followed regarding the project and the timeline.

ARE WE READY FOR THE BIG ONE?

City Manager Wes Hare referenced a recent New Yorker article which laid out what a large earthquake could do in terms of damage to communities on the West Coast. He said this has been talked about for years, and steps have been taken from time to time to address the issue: the School District received grant money to upgrade Central Elementary; we have used Central Albany Revitalization Area (CARA) money to help with seismic upgrades; and new projects are required to meet Code. He said this is a tough call for policy makers because we have gotten along this far without a catastrophic event which lulls people into a sense of complacency and makes them feel there are other priorities, yet the consequences of inaction can be quite horrible as seen in other places. An earthquake in Pakistan in 2005 killed close to 100,000 people, 17,000 of which were children in their schools. In contrast, legislation in California in the 1930s required schools to be earthquake resistant and there has not been a child injured in an earthquake in a school in California despite that state having had earthquakes of comparable intensity to the one that occurred in Pakistan. There has been internal discussion and the management team agreed it would be a good time to update City Council on these issues.

Emergency Management Specialist Darrel Tedisch said the City's updated Hazard Analysis shows earthquake as fifth of the six high-priority items primarily because of the amount of destruction that could occur. Snow storms and ice storms are first and second priority, primarily because of snow and ice a couple of years ago and the impact it had on the community, and the others are high winds and river floods.

Tedisch said the article in the New Yorker July 2015 edition was followed by other information on earthquakes in the region, including an Oregon Public Broadcasting series that talked about how bad the infrastructure is in Oregon and possible impacts if there was a large quake; blogger Hasso Hering's article that summarized some of the problems and impacts; a meeting of the Corvallis City Club attended by over 100 people and three presenters which looked specifically at the downtown buildings and what could be expected to occur; an editorial in the *Albany Democrat-Herald* about preparedness and what steps citizens could take to be prepared; and an American Geosciences Institute Webinar: "Communicating Cascadia's Earthquake Risk," a key point of which is that those on the I-5 corridor could experience a long shake, damage, and disruptions including lack of fuel, lack of natural gas, downed utilities, food shortages, and liquefaction.

Tedisch said in his experience, the community expects that government will be quick to respond, have a handle on the action to be taken, and be able to provide relief to the community. In reality, most of that could not be done. In evaluating critical facilities, it was found they will in most cases be damaged. City Hall was built in 1996 at a higher level, and it can be expected that the ceiling will fall and windows will break but from a stand and use standpoint, it should be okay. In looking at power, the Police Department gets backup power from the Linn County Sheriff's Department. Two-way power was put into the sewer plant and, depending on what remains, we could have power there. There are generators at most places that would be considered critical but, again, fuel will be a concern.

Interim Public Works Operations Director Chris Bailey added that it's likely the City won't have water or sewer for weeks if there is a big event. There may be some rudimentary water making ability if everything holds up but distribution could be a huge problem. There are a lot of unknowns and it is just not expected that the City will have a lot of water available for people.

Tedisch reviewed past staff training and exercises, and he said the City will participate in the state's full-scale exercise in June 2016. As a community, we are doing very well; however, the City doesn't have the resources to be able to provide fuel, water, food, and even potentially a shelter for 500 people. He said the community is encouraged to be prepared with 14 to 21 days worth of supplies.

Olsen asked if there is a list of supplies that people should have. Tedisch said there is a lot of information on line and a County intern will be undertaking an effort to encourage people to be prepared and come up with a standardized list of what they should have available in their homes.

Hare said it would be a good idea to include this as part of the conversation when talking about facilities, spending priorities, and ways we can help the school district. He said we don't want our kids in a place where they are vulnerable and, frankly, that is the way it is now in most of our schools.

CHANNEL 28 PROGRAMMING

Assistant City Manager/Chief Information Officer Jorge Salinas said Council had previously asked staff to look at usage for Channel 28 and to add information about the schedule. Channel 28 now shows when Council meetings are scheduled; however, there are not additional videos to add to the schedule. Comcast doesn't provide usage information about Channel 28, but staff has monitored stats from the YouTube channel. Salinas distributed and reviewed information regarding the past six month's usage and the top videos viewed (see agenda file). The total viewing time for the top ten events was 18,000 minutes or 304 hours, which is about 1.7 hours per day. The City doesn't have the ability to produce content nor the funds to buy content for the channel.

Councilor Bessie Johnson asked if staff looked at what other cities do. Public Information Officer/Management Assistant Marilyn Smith said other cities and counties Albany's size tend to use content that someone else produced. Unless it is a city of at least 100,000 population, there is not the staff or equipment to produce content. She noted that cable viewership is dropping across the country because videos can be viewed without subscribing to cable. In discussion, it was noted that videos from others would need to be in the right format and they would need to be reviewed for content. Hare added that staff can handle Channel 28 programming as it currently exists but, short of dedicating funds for that purpose, there is not much more that can be done.

PUBLIC SAFETY FACILITIES PROJECT UPDATE

Engineering Manager Staci Belcastro said applications for both the police and fire buildings have been submitted and deemed complete. Staff is working on the report and notice for the November 16, 2015, public hearing at the Planning Commission. The Edward Jones building is being offered up for auction to move the building. A contract has been awarded to a firm out of Eugene to demolish Richards Cleaners.

Fire Chief John Bradner said it was not feasible to move or refurbish the Ralston building. A company has been retained to dismantle the building and provide back some of the reclaimed wood which can then be used in construction of the new station. He said staff has been working with tenants Edward Jones and Richards Cleaners on their move dates to ensure they are not without a place of business.

COUNCILOR COMMENTS

Konopa commended staff for their work on the Simpson Trail area. She went on a Gator with the Police Department as they went along the trail and walked down mini-trails that led to homeless campsites. Eleven sites had been shut down and there was a huge amount of garbage, clothing, and waste. She said it takes a lot of time between Police, Parks Maintenance, and Public Works staff to patrol, shut down, and clean up these sites and stay on top of that situation.

CITY MANAGER REPORT

Hare said he was asked by Portland State University to talk to some international students and faculty about the City's e-government efforts and the services offered electronically. It was a good meeting and that same group will visit on Thursday to see how it works first-hand. He has also been asked to participate in a national webinar by the Alliance for Innovation related to the City's electronic services.

ADJOURNMENT

There being no other business, the meeting was adjourned at 5:55 p.m.

Respectfully submitted,

Reviewed by,

Teresa Nix
Administrative Assistant

Wes Hare
City Manager

CITY OF ALBANY
CITY COUNCIL
Council Chambers
Wednesday, October 28, 2015
7:15 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 7:15 p.m.

PLEDGE OF ALLEGIANCE TO THE FLAG

Konopa led the pledge of allegiance to the flag.

ROLL CALL

Councilors present: Mayor Sharon Konopa and Councilors Rich Kellum, Bill Coburn, Bessie Johnson, Ray Kopczynski, and Dick Olsen.

Councilors absent: Councilor Floyd Collins was excused.

PROCLAMATION

Extra Mile Day.

Konopa read the proclamation. She said she feels this is very timely for November, because of the Veterans Day Parade. People go the extra mile for the wonderful parade.

SCHEDULED BUSINESS

Business from the Public

There was none.

Second Reading of Ordinance

Amending Albany Municipal Code Chapter 2.27, Community Development Commission.

City Attorney Jim Delapoer read the ordinance for the second time in title only: AN ORDINANCE AMENDING ALBANY MUNICIPAL CODE CHAPTER 2.27, COMMUNITY DEVELOPMENT COMMISSION.

MOTION: Councilor Bill Coburn moved to adopt the ordinance and Councilor Ray Kopczynski seconded the motion.

Councilor Rich Kellum restated his earlier objection. He thinks it is in the best interest of every commission that appointments are made by a broad base of the Council.

VOTE: A vote was taken on the motion and it failed 3-2, with Kellum and Councilor Bessie Johnson voting no. It takes four votes of the Council to decide any question. This item will come back to the November 4, 2015, Council meeting.

First Reading of Ordinance

Levying assessments against property specially benefited by sewer connections and the assessment of sewer and transportation system development charges for property described as Tax Lot 01010, Parcel 11S03W18B, and site address 353 29th Avenue SW.

Delapoer read the ordinance for the first time in title only: AN ORDINANCE TO LEVY ASSESSMENTS AGAINST PROPERTY SPECIALLY BENEFITED BY SEWER CONNECTIONS AND THE ASSESSMENT OF SEWER AND TRANSPORTATION SYSTEM DEVELOPMENT CHARGES FOR PROPERTY DESCRIBED AS TAX LOT 01010, OF PARCEL 11S 03W 18B, AND SITE ADDRESS 353 SW 29th AVE; AND DECLARING AN EMERGENCY.

MOTION: Coburn moved to have the ordinance read for a second time in title only. Kopczynski seconded it and it passed 5-0.

Delapoer read the ordinance for the second time in title only.

MOTION: Coburn moved to adopt the ordinance and Kopczynski seconded it.

Albany City Council Regular Session
October 28, 2015

Konopa pointed out that the Owner of Record on Exhibit A should be "ARRT," not "AART." The Exhibit will be revised (see agenda file). Inclusion of the revised Exhibit was acceptable to the motion maker and the seconder.

VOTE: A vote was taken on the motion to approve the ordinance with the revised Exhibit A and it passed 5-0, and was designated Ord. No. 5860.

Adoption of Resolutions

Approving an exemption from the competitive bidding process and authorizing the purchase of Zoll Defibrillators through National Purchasing Partners.

Fire Chief John Bradner said the Fire Department is requesting the exemption to purchase Zoll heart monitor/defibrillators. The Fire Department has used these for approximately the last 14 years. The crews are familiar with the brand, and the Department gets interoperability with their current system.

Johnson asked if the old defibrillators can be refurbished. Bradner said the monitors currently in use on ambulances will go to the engines. Those on the engines, which are now 14 years old, are not serviceable any more. They will be traded in for \$1,500 per monitor.

MOTION: Kopczynski moved to adopt the resolution and Coburn seconded it. The motion passed 5-0 and was designated Resolution No. 6456.

Appropriating funds for the purchase of a replacement Type 6 Fire Engine.

Bradner read from the staff report. The Fire Department is asking to appropriate \$118,200 from the General Fund for a Type 6 engine. The Type 6 is a smaller engine with four-wheel drive and a water tank. It can go off road to access portions of North Albany or Knox Butte that a city-use engine can't get to. This summer's wildfire issues highlighted the need for this equipment. Fortunately there were no major incidents in Albany, but the Fire Department responded to Scio, Brownsville, Lebanon, and other districts on mutual aid. This engine is needed to replace one that went out of service a few years ago.

Coburn asked who the Department will buy the engine from. Bradner said they will buy the chassis from Roberson Ford.

Kopczynski remembered that some time ago, the City purchased a repurposed EMT vehicle with the same box on a different frame. He asked if we can do that in this case. Bradner said no, because brush engines are overweight. Their current vehicles are from the early 1990s on F350 chassis, and subject to mechanical failure. This summer, the Fire Department had to choose between maintaining a brush engine with a full water tank, or partly draining the tank so they could use it to tow the boat. They need a brush engine with a larger chassis.

MOTION: Kopczynski moved to adopt the resolution and Coburn seconded it. The motion passed 5-0 and was designated Resolution No. 6457.

Approving an Extended Property Tax Abatement Agreement with SnoTemp Cold Storage.

MOTION: Kopczynski moved to adopt the resolution. Coburn seconded the motion.

Kellum asked if this had been through the City's Planning process. John Pascone, of the Albany Millersburg Economic Development Corporation (AMEDC), said SnoTemp has to apply before they start construction. They've been planning this project for about a year, and have been through the land use channels already. They can start site work as long as they don't put footings in before the application comes in.

VOTE: A vote was taken on the motion and it passed 5-0, and was designated Resolution No. 6458.

Adoption of Consent Calendar

- 1) Approval of Minutes
 - a) August 18, 2015, Joint Albany City Council and Benton County Board of Commissioners minutes.
 - b) August 24, 2015, Work Session minutes.
 - c) September 21, 2015, Work Session minutes.
- 2) Readopting an Investment Policy and repealing Resolution No. 6374. RES. NO. 6459
- 3) Readopting the Risk Management Policy and repealing Resolution No. 6375. RES. NO. 6460
- 4) Readopting Financial Policies and repealing Resolution No. 6376. RES. NO. 6461
- 5) Declaration and disposal of surplus personal property.

MOTION: Johnson moved to adopt the Consent Calendar as presented. Kopczynski seconded the motion and it passed 5-0.

Albany City Council Regular Session
October 28, 2015

BUSINESS FROM THE COUNCIL

Johnson asked if the City has set business hours for marijuana dispensaries. City Manager Wes Hare said no. Johnson said one dispensary is putting signs in the right-of-way.

Kellum referred to discussion of a parking structure downtown. He thinks Water Avenue might not be the best place for one. He suggests considering the parking lot at Two Rivers market instead. Konopa said that site has been part of the discussion for years. She said she recently talked to someone who loved the changes in downtown. They suggested blocking off First and Second Avenues and Broadalbin Street, to make them pedestrian malls.

Konopa reminded the Councilors that they would be riding on the trolley in the Veteran's Day parade.

Hare said he met with some South Koreans at Portland State University last week, and they will be touring the area tomorrow. Today the City had visitors from China to look at the wastewater treatment plant. Hare said there are also members of the Fellows program visiting from Cambodia and the Philippines.

Public Works Engineering and Community Development Director Jeff Blaine gave an update on the Crocker Lane street project. The City will be required to get wetland permits from both the Department of State Lands and the Army Corps of Engineers. Getting the permits is a long process, so construction will probably start in the summer of 2017. The issue is connectivity between drainage ditches and regulated wetlands. Four criteria trigger the need for permits, and the project met three of the four.

RECESS TO EXECUTIVE SESSION TO DISCUSS REAL PROPERTY TRANSACTIONS IN ACCORDANCE WITH
ORS 192.660 (2)(e)

The Regular Session recessed to the Executive Session at 7:39 p.m.

RECONVENE

The Regular Session reconvened at 7:46 p.m.

MOTION: Kellum moved to authorize the City Manager to conclude the sale of the City of Albany Police Station and the Albany National Guard Armory properties in the appropriate manner. Koczynski seconded the motion and it passed 5-0.

Councilor Dick Olsen asked if the armory building was considered in the City's earthquake plans. Hare said Linn County has a more direct link to the armory, but the City would be looking at all public resources in that event.

NEXT MEETING DATE: Work Session: November 2, 2015, and Regular Session: November 4, 2015.

Konopa said the 4:00 p.m. Work Session on November 2 won't take long, and the second meeting is at 7:15 p.m.

ADJOURNMENT

There being no other business, the meeting was adjourned at 7:50 p.m.

Respectfully submitted,

Mary A. Dibble, MMC
City Clerk

Reviewed by,

Wes Hare
City Manager

CITY OF ALBANY
CITY COUNCIL WORK SESSION
Municipal Court Room
Monday, November 2, 2015
4:00 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 4:00 p.m.

ROLL CALL

Councilors present: Councilors Rich Kellum, Bill Coburn, Ray Kopczynski, Dick Olsen, Bessie Johnson, and Floyd Collins.

Councilors absent: None.

Councilor Floyd Collins arrived at 4:05.

BUSINESS FROM THE PUBLIC

None.

OSU EXTENSION BOND MEASURE

Derek Godwin, Regional Administrator of Oregon State University (OSU) Extension Service for the mid-Willamette Valley, gave his address as 2630 Summer Street SE, Salem, 97302. He distributed several handouts (see agenda file). He said Extension in Oregon began as a partnership with county funding to bring OSU faculty into the counties. The Extension Service went national in 1914, with state and federal funding matches of county dollars. State and federal funds now make up about 80% of funding, going mostly to faculty. County funds pay for offices and support staff, rent, utilities, and travel. State and federal funding has not kept up with expenses. In the last decade, Extension has lost about 20% of faculty around the state, and more than that in the Willamette Valley. Most of Oregon's counties have created special service districts for Extension and 4H. Linn County established a service district in 2008. Local districts give counties local control of priorities.

Godwin said Benton County Commissioners are "very supportive" of creating an Extension service district in Benton County. The next step is to ask each city in the county to pass a resolution stating that they would be part of such a district. An amended copy of the resolution in the agenda packet was on the dais (see agenda file).

Maggie Livesay, 4H Youth Development Agent and County Leader for OSU Extension, Benton County, gave her address as 7580 NW Mountain View Drive, Corvallis, 97330. She described Extension and 4H in Benton County. They currently have about 480 adult volunteers. Benton County has about six hundred students involved in 4H year-round, and over a thousand students in school enrichment programs. Extension faculty is spread thin. The proposed service district would provide stable funding for program assistants to manage volunteers. Livesay described participation in some of the programs provided to North Albany residents. Several of the programs are grant-funded, and would benefit from the stable funding of a service district.

Godwin called attention to his handout on compression (see agenda file), which came from Benton County's budget analyst. The proposed service district rate of eight cents per \$1,000 would lose about \$18,600 to compression.

Councilor Bill Coburn asked why the City of Corvallis does not appear on the list of jurisdictions on the compression handout. Godwin said the City of Corvallis is not under compression. Coburn noticed that the change in compression is similar in the City of Albany and in Benton County. Konopa said that would be the unincorporated area of Benton County, which is smaller than Linn County.

Councilor Dick Olsen asked if Corvallis is up against their taxable limit. Godwin said Corvallis has a \$3.97 gap. He said that compression loss does not come from a city's general fund. City Manager Wes Hare explained that compression occurs on a property-to-property basis. A taxing district loses revenue on any property that reaches the \$10 cap on taxes. Compression losses come first out of local option levies, then out of the base. Corvallis' gap is related to its higher property values.

Councilor Rich Kellum asked if Lane County has an Extension service district. Godwin said no. It's harder in Lane County's government system to do a service district than it is for Benton County. Kellum asked if Lane County receives Extension services. Derek said no county Extension funds go to Lane County. One faculty member serves Linn, Benton, and Lane counties. Her salary is paid by the state. Kellum asked if Benton County's proposed service district would be established at the same rate as Linn County's. Godwin said Linn County's rate is seven cents; Benton County's would be eight cents.

Councilor Bessie Johnson asked if Linn County's service district keeps Linn County Extension solvent. Godwin said yes. Without the service district money, Linn County would not have an Extension program.

Albany City Council Work Session
November 2, 2015

MOTION: Coburn moved to approve the amended resolution on the dais. Kellum seconded the motion, and it passed 6-0, and was designated Resolution No. 6462.

COUNCILOR COMMENTS

None.

CITY MANAGER REPORT

There was no City Manager Report.

ADJOURNMENT

There being no other business, the meeting was adjourned at 4:28 p.m.

Respectfully submitted,

Reviewed by,

Allison Liesse
Accounting Specialist

Stewart Taylor
Finance Director

CITY OF ALBANY
SPECIAL CITY COUNCIL WORK SESSION
Council Chambers
Monday, November 2, 2015
7:15 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 7:15 p.m.

ROLL CALL

Councilors Present: Councilors Bill Coburn, Floyd Collins, Bessie Johnson, Rich Kellum, Ray Kopczynski, and Dick Olsen

Councilors Absent: None

MARIJUANA REGULATIONS AND LOCAL OPTIONS

City Attorney Sean Kidd said the City Council previously directed that staff come back with options on marijuana regulations. He gave a presentation on Local Options Regarding Marijuana Regulations (see agenda file), which included the following options as well as maps showing how each option would affect areas of the City.

Option A: Do nothing and leave the ordinance the way it currently exists. The City could place additional time, place, and manner regulations such as not allowing facilities to be located within 1,000 feet of each other. The City would share in tax revenue but that amount is yet to be determined. Cities are allocated 10% of all taxes collected based on population, and counties are allocated an additional 10%. Council could adopt an ordinance referring the question of banning recreational facilities to a future ballot and, if that passed, facilities would not be grandfathered in. The Council could also choose to put an additional 3% tax on the November 2016 ballot. Discussion followed regarding the amount of tax revenue that might be expected to be received.

Option B: Rescind the current ban on medical dispensaries selling recreational marijuana and then adopt an ordinance banning recreational facilities. This option would allow the City to evaluate how early sales impact the City related to enforcement. The City would not be able to share in the tax revenue generated from early sales. If voters adopt the ban in November 2016, all recreational marijuana sales would end by December 31, 2016. If voters reject the ban, recreational facilities would be able to open as soon as applications were processed by the state.

In response to questions from the Council, Kidd said none of the proposed actions would affect current medical dispensaries which are grandfathered in. Council could ban any new medical dispensaries, wholesalers or producers; however, medical grow sites were left off of uses that can be outright banned. The Council can regulate any of these uses in time, place, and manner. The state will allow medical dispensaries to sell recreational marijuana until the end of 2016, at which point recreational facilities are expected to be up and running and medical facilities will go back to being dispensaries. The Council could also choose to rescind the ban on dispensaries but not adopt an ordinance banning recreational facilities.

Option C: Amend the Albany Municipal Code (AMC) to remove the Industrial zone exception, thereby requiring facilities to be located at least 300 feet from any residential zoning, and/or further define residential zoning, define how property lines are measured, and/or regulate time, place, and manner. Uses would have to meet the criteria of the zone which could further restrict some uses in some zones. The Council could choose to regulate hours of operation, odor, noise, background checks, security, signs, inspection, and more. Under this option, the City would share in the early sales tax revenue and could choose to put the 3% tax on the November 2016 ballot.

Brief discussion followed and staff provided additional information about where uses could be located currently and potentially under this option. Kidd noted that the Oregon Liquor Control Commission (OLCC) will review applications according to the state rules. City regulations, including Sign Code, would not trump state law. Some cities have adopted the state rules into their development code or municipal code.

Option D: Adopt a ban on recreational facilities (or other uses) and do not rescind the ban on medical dispensaries selling recreational marijuana. This option would result in no recreational marijuana sales anywhere in the City prior to the November 2016 election. OLCC has said it will not process applications in cities or counties that have adopted a ban until the vote. The Council would want to begin working on time, place, and manner restrictions in case the ban is voted down. Under this option, the City would not be able to share in the tax revenue generated from early sales. The preliminary interpretation from the League of Oregon Cities (LOC) is that once the ban is placed, a City would be unable to receive the tax revenue even if they later rescind the ban.

Option E: Refer the question to the voters in November 2016 and not adopt a ban now. Under this option, sales would likely not occur in Albany prior to the late summer or early fall 2016. The City would share in tax revenue from early sales until and unless the ban was passed by the voters. The Council would want to consider time, place, and manner regulations immediately as the City would be receiving requests from OLCC regarding land use and zoning compliance for incoming applications. If voters reject the ban, all marijuana businesses would be required to cease operations with the exception of any medical dispensary or producer already licensed. People who spend money to make improvements for recreational sales would do so at their own risk and those businesses would not be

grandfathered in if voters approve the ban. There is risk of the City being sued; however, the chances of success by a negatively impacted facility is low.

Kidd said the City Attorney's Office doesn't have an opinion on the morality of marijuana. In his opinion, Option E has a lot of unanswered questions that could put the City in an awkward predicament. As the bill is being interpreted by most attorneys, there must be a ban in place in order for it to go to the voters in 2016. The LOC also has expressed concern about giving people of the City a second vote on an issue already voted on by the state. If the Council is interested in pursuing Option E, staff would need to look into it further with the state. Brief discussion followed.

BUSINESS FROM THE PUBLIC

Michael Parten, from West Coast Alternative Medicine, said the option to rescind the ban on medical facilities selling recreational marijuana and then move to a vote to ban recreational facilities would provide a few months of a track record for recreational sales and how much tax revenue would be generated for our City. Establishing a ban right away would cut the City out of that tax revenue.

Casey Costa, P.O. Box 1305, said she has been watching both sides of the disagreement of whether to have recreational sales. One side is determined not to have recreational marijuana sales in Albany despite the fact that it is now legal, and the other side is determined to have recreational marijuana in Albany without knowing the full impacts. What each side doesn't seem to appreciate is the additional commerce that recreational marijuana would bring to Albany. While the City Council has said tax revenue would be negligible, that is an unknown at this time. There is a unique situation of having a substance that is widely available illegally and tax free, and the public stating that they want to buy it legally and pay tax. She asked that the City Council realize that recreational marijuana is in Albany and has been for a long time, and that they encourage commerce and start the sale of legal recreational marijuana as soon as possible.

Trey Mork, 2614 Prairie Place, asked for clarification on an earlier statement, and Kidd explained that as it now stands, the City would not be able to receive tax revenue if they enacted a ban that the voters later rescinded; however, he expects that to be changed by the legislature going forward.

Rhonda McNeal, 687 Tangent Street, Lebanon, reviewed her personal history and her efforts to get off of disability insurance, renew her independence, and all that comes with holding a paycheck. She is having trouble finding a job because of the laws on THC, and her only real hope of employment is in the green community which has been accepting and nonjudgmental. She asked that consideration be given to lifting the ban to help people like her who are trying to get off of assistance.

Steven Reynolds, P O Box 1420, said he owns property in the Light Industrial zone and he is concerned about private property rights and what he would be losing if these uses are grandfathered in. His perspective is that this is still illegal under federal law and the government could decide to enforce federal laws next year. The laws may change in the future and marijuana might be treated like alcohol or wine grapes; however, at this point, people who don't want to be involved until it becomes legal are being pushed to the side. These property rights have value to him.

Sher Fenn, 2601 Salem Avenue SE, said the current ban has negatively affected medical marijuana sales in Albany, which has affected her family members who are employed in that business. She noted that people who go to Corvallis to buy marijuana are also shopping and eating there, so Albany is losing revenues in other ways as well. She asked that Council think about how the ban is hurting businesses.

BUSINESS FROM THE COUNCIL

Councilor Ray Kopczynski asked if the maps could be refined to show where retail shops could potentially locate. Konopa noted that staff has provided a lot of information and she suggested that it would be appropriate to have Council consensus on whether additional information is needed. City Manager Wes Hare said that retail sales would most likely be located in Community Commercial and Regional Commercial zones. City Attorney Jim Delapoer added that, although the discussion has been focused on recreational sales, there could also be processing and distribution which might occur in Industrial zones.

Councilor Rich Kellum said he would like to adopt Option D and begin working to ensure regulations would be in place if the ban isn't passed by the voters. Delapoer said the usual process would be to direct staff to draft an ordinance.

MOTION: Kellum moved to have staff to bring back an ordinance based on Option D of the Local Options Regarding Marijuana Regulations. Councilor Bessie Johnson seconded.

In response to a question from Johnson, Kidd clarified that if a ban is put on the November 2016 ballot, the state would not begin to process applications until after that date.

Councilor Bill Coburn asked if it is Kellum's understanding that if the ban does not pass and recreational sales begin, Albany would not get any tax revenue. Kellum said the tax revenue can only be used for the regulation of marijuana. He said many other jurisdictions have banned commercial recreational marijuana activities and he can't imagine that

legislators won't change that provision in the future. Kidd agreed, noting he has already seen signs of that change.

Konopa said there are a lot of details that are yet to be worked out at the state level and she fully expects that the tax revenue limiting provision will be changed.

Kopczynski said the City of Albany has already voted to legalize recreational marijuana and he takes exception to asking them to vote again. Kellum said he doesn't think people understood the amount of tax revenue that would be received and limitations on its use. Konopa said additional information has come out and going to a vote would give people the ability to make a decision based on what is in place. Kopczynski questioned what information has come out that would materially change how things would happen in Albany. Konopa said she has seen information that the black market is growing because they can sell it cheaper; she thinks this option will provide time to see if the black market will grow or decline. Councilor Dick Olsen said that marijuana is Oregon's biggest cash crop and he thinks that allowing recreational sales would help to curb the black market element. Kopczynski said we have been talking about this for a long time and he feels that some on Council are on the wrong side of history. Kellum noted that the ban would not keep anyone from personal use. Additional discussion and deliberations followed.

VOTE: A vote was taken on the motion and it passed 4-3 with Olsen, Kopczynski, and Coburn voting no.

The requested ordinance will come back for a first reading on December 2, 2015.

MOTION: Olsen moved to have staff bring back an ordinance with the 3% tax. The motion died for lack of a seconder.

Kidd noted that the 3% tax issue could be decided anytime between now and next November.

ADJOURNMENT

There being no other business, the meeting was adjourned at 8:58 p.m.

Respectfully submitted,

Reviewed by,

Teresa Nix
Administrative Assistant

Wes Hare
City Manager

CITY OF ALBANY
CITY COUNCIL WORK SESSION
Municipal Court Room
Monday, November 9, 2015
4:00 p.m.

MINUTES

CALL TO ORDER

Mayor Sharon Konopa called the meeting to order at 4:00 p.m.

ROLL CALL

Councilors present: Councilors Rich Kellum, Bessie Johnson, Ray Kopczynski, Floyd Collins, and Dick Olsen (arrived at 4:02 p.m.)

Councilors absent: Councilor Bill Coburn (excused).

BUSINESS FROM THE PUBLIC

None.

TRANSIENT LODGING TAX

Councilor Dick Olsen arrived at 4:02 p.m.

Economic Development & Urban Renewal Director Kate Porsche introduced the item to the Council. She explained that there was discussion at a previous Budget Committee meeting about changes to the existing Transient Lodging Tax (TLT) policy; however, no specific action was taken at that meeting. Staff is looking for clarification and direction as to which body the Council wants to appoint to review the current TLT policy. The current policy outlines a TLT Advisory Committee and the details of who that would include are noted in the staff report. Other options for a review body would include a TLT ad hoc group, some other workgroup of the Council's choosing, or that Council review the policy themselves. Porsche added that staff would recommend that the Council review the policy themselves and that she believes that could be done in three special meetings. She believes that having Council review the policy would result in a better understanding of the policy by the Councilors and would better engage the various stakeholders. Porsche sees this as a process that would include interaction with stakeholders as well as recipients of the TLT; and that staff would assist with a road map of questions to look at, some of which she has already included in the staff report.

Konopa said she likes the idea of having the City Council as the review body. She believes it would be more efficient and save on staff time; that if it were opened up to a new committee, it would take a lot more preparation to get everyone up to speed on all of our tourism activities. She pointed out that Council has the final say anyway so in her opinion, that would be the best option to expedite the process before budget time.

Councilor Rich Kellum said he is of the opinion that they need to have input from others. He added that people have their opinions and attitudes; we need to first have facts, without any emotion added. To that end, the ad hoc committee would be a group of people, none of which are a provider or a receiver of TLT funds, to bring forward the facts. The people that were on the committee before, for example, Bill Draper from *Democrat Herald*, someone who knows a lot about marketing; Betsy Penson from Heritage Mall; Mitch Langjahr, a guy who's run a multi-billion dollar corporation and whose job has been marketing. Kellum referred to a few people in the audience and said, we need people who don't have a vested interest; someone who has the background, knows the history, and knows how to conduct a meeting. Then if there are other people that don't have an ax to grind but have the expertise, include them as well. Kellum believes that the pertinent expertise among the Councilors is limited and that the only expertise lies with him having been in business for 32 years and with Councilor Ray Kopczynski working for JC Penny for a long time. Kellum pointed out that he is on the board for the Albany Millersburg Economic Development Corporation (AMEDC) and Kopczynski is on the board of Albany Visitors Association (AVA). He said that even if they're trying not to, it's hard not to have an ax to grind. Kellum's preference would be to have an ad hoc committee.

Kopczynski agrees with Kellum that outside influence is needed to create the policy, especially if they're going to be considering changing it. To that end, he believes Kellum is correct, but he also reiterated that the Council is the body that makes the final determination as to what actually happens. He can see outside people making recommendations to the Council at the various meetings but since Council is the one that has to make the final decision, he would go along with Porsche's recommendation to have Council review the policy.

Konopa pointed out that all the people that Kellum mentioned could provide input at meetings but the Councilors are the ones that look at all programs and the overall funding needs of the City.

Kellum replied, bottom line, there's no one here that has the expertise; and we're talking about whether or not somebody's expertise applies to this issue. He continued, we don't really have much expertise on the Council and it

should be driven by expertise. He asked, do you really want to spend the time for Council to get all the info they need?

Konopa stated that the people with the overall knowledge of City programs and the needs of the City would be the Council.

Kellum responded that the discussion is not about whether things are working, it's about whether it's effective. He asked whether the money that they're doling out is being used effectively and he believes only people with expertise have the ability to say. He added that people without the expertise are making a guess as to whether the money is being well spent and the Council hasn't always asked the right questions.

Councilor Bessie Johnson understands there have been questions raised as to whether the Albany Downtown Association (ADA) or the AVA are making good use of their TLT funds. She agrees with Kellum and would like to see a fair and unbiased panel that could look at each situation and see if that's really the outcome that was intended. She said that it may come back that the results are what was desired and the Council should continue with the current process. She feels that there's no way of saying that the Council or the recipients of the funds are not prejudiced in some way or another, even though they don't want to be. She would like to see some other work group appointed by the Council; that a fair and unbiased panel would be a good way of determining whether the funds are being spent appropriately and would also be a good means of keeping things transparent. She said she's glad that Scott Pierson from Wood Castle was mentioned, she believes he's a good example of someone that would be a good fit for the policy review board. Pierson is also a member of the Budget Committee.

Konopa believes that if Council went this route, then each Councilor should make an appointment, otherwise it would be too difficult to make a collective Council decision about who to appoint. Johnson suggested a panel of seven and Kellum added that it could be seven individuals who do not either receive or appropriate TLT funds, to which Johnson agreed.

Kopczynski is concerned that Council will be getting filtered information and that they won't be hearing from the people that are directly impacted by the decisions of those appointees. Kellum asked what would keep them from giving an editorial view of what came out of the discussions.

Konopa believes that if they want to assemble a group that's totally unbiased, that would mean that they don't know anything about the current City tourism programs or what's currently being funded out of TLT, which would mean those individuals would have to be brought up to speed on every program and every organization that's currently being funded. Konopa could see this taking an entire year.

Johnson believes this could work similarly to the Public Safety Facilities Review Committee. She said they didn't have all the information or know everything to begin with and it took some time, but she believes this process needs to be done right. She suggested that the panel give their reports to the Council and then the Council makes the final decision.

Olsen asked for an update on the Expo Marketing Contractor's report as to what's needed for the Expo. Council hasn't seen that report yet. He would like to see a report from the marketing contractor before they start forming yet another committee. He believes that Council should consider the option that Porsche has suggested and that Council should make the decisions, rather than have a committee report to them and then they make the decision, which just seems like an extra step to him. He added that he may not be a marketing expert but he has many years of experience on the Council reviewing reports.

Kellum stated that this is his point exactly; they get reports about how much money is received from TLT, and there are people who are ecstatic about how much money is coming in. He said, receiving the money is great, but what are we getting the money for? The money is to promote Albany and generate business for Albany from outside the community. If you listen to the hoteliers, they would point out that it is the over-nighters, not those staying for multiple nights and spending time in Albany, that are paying most of those taxes. The people who know that are the ones that need to provide input on the policy. It's not the money we're after; it's the economic activity that we're after and that's what the funds are supposed to support.

Porsche responded to Olsen's question concerning the Expo and agreed that staff can have the contractor come to give a report to the Council. Olsen said he would appreciate that as he has asked for this report before.

Porsche weighed in on the discussion of a subcommittee versus having the Council reviewing the policy themselves and explained that staff could bring in a professional to help facilitate meetings and gave an example of who might be able to assist. Porsche is concerned about the timing of this, however. There are a number of current recipients who are getting ready to build their budgets for the next year and need to determine whether they will receive these funds. She was hoping to have these meetings in January and complete this process expeditiously. Olsen asked, are they worried about whether they will receive the tax money? Porsche explained, as an example, part of her Economic Development work for the City is funded by TLT funds. Whether or not they can expect to receive those funds will affect how they build the budget. The sooner those recipients have that information, the better. Konopa said for the timing issue, it would be better to have the City Council be the review body. Olsen added that everyone's welcome to come and provide input.

Porsche explained that City Manager Wes Hare had requested to know what other communities of similar size to Albany, receiving a similar amount of funds, are doing with their TLT funds. Economic Development/Urban Renewal Coordinator Nathan Reid has begun to compile that information. Konopa noted that the City is restricted with what they can do; a certain percentage is required to be spent on tourism by state law.

Hare commented that a lot of time is spent talking about a budget that is less than one million dollars per year and there's been a formula for this budget for years. A portion of the funds go to the Albany Visitors Association (AVA), an organization that's been around for many years working to promote the community. The City hears from them at least twice per year, receives reports that are fairly comprehensive, and they give presentations to Council on the work they're doing. The City also gives money to the ADA, which is also a well-established group that's been around for a long time, as well as AMEDC. All of those bodies report back to the City Council and explain how they're using the money that's given for that purpose. Hare continued, it seems that every year we have this debate and one of the problems is not having an agreed upon metric for determining success. One of the big metrics in other communities across the state is how the TLT itself is doing, is it robust, is it shrinking, or are you looking at a declining business, if you will. We all know that's not a particularly accurate gauge because there's volatility in the industry. It's certainly climbed back since the recession; each hotel has had an all-time record month recently so from that perspective, it's been pretty robust. The reason staff has recommended that Council deal with this is how much time and effort to you want to invest in a budget that is small by comparison. We've had a long established method for distributing the funds that looks very similar to what other communities do. It seems that we discuss this issue a lot but don't seem to reach a conclusion.

Konopa agreed that it is beneficial to have a metric to follow. She said that for twenty years of budget processes, it used to be that TLT was the only discussion item they had and it took a great amount of time listening to those presentations for requests for TLT funds from outside agencies. When they transitioned from the old program to the new program, with simple allocations, they didn't have to spend that time at the budget level. She said that it's just been in the last year or two that this conversation has started again and the question is whether we want to spend our entire budget season discussing one source of funds, or just continue with the way things are being done currently. She feels that if they need to tweak that plan, in order to expedite the process, the Council will need to do this.

Olsen stated that he doesn't mind discussing this at budget time but in reviewing the report the Council received recently, he noted the revenues for June are up by 48.6%. He sees those numbers and wonders if there are that many more people coming to stay in Albany or if the rates have increased that much. Olsen said he's amazed at that many people wanting to come to Albany to stay the night, which in his mind means that Albany is doing something right.

Porsche added that staff would bring that sort of information forward and include it as part of the policy discussion.

MOTION: Kopczynski moved to have the City Council, as the body that directs the TLT funding, review the policy (the fourth option listed in the staff report) and Olsen seconded.

Kellum said they've been doing this for several years and it's never been investigated properly and each time people are giving reports on how the money was spent but not what the outcome was from spending that money. He believes that if they don't know how it worked out, maybe it needs investigating and feels that the fact that Olsen is ecstatic about 48% goes to a point. He stated, people in marketing won't just be happy with the increase, they'll ask, why the increase.

Konopa pointed out that the Council has received input from several individuals, ad hoc committees, and the like over the years. Kellum responded that every time this comes up they're under the gun and have to do something right away so the response is that they better not investigate because of time constraints.

Olsen commented that the last request was to get someone into the Expo to help with that.

Collins said that at the time they established the current policy, part of the commitment was that there would be a routine review and report that would come back to the Council about how effective the program was. Johnson stated that this never happened.

Collins explained that the Council's agreement to the current policy was all predicated on the fact that there would be a review of whether the expenditure of funds was effective. He stated that Council has to deal with the limitations they have; the statute requires that a certain percentage go to tourism but they have flexibility beyond that. He believes there are two components, one being the effective use, but they shouldn't ask a group to look only at the continuation of the allocation of the gross revenue without asking these questions: Given the percentage that has to be allocated for tourism, what's the most effective use of that percentage; and what about the balance, should that also then be reinvested in tourism, or some other City program? He feels the second question has to be decided by Council but the effective use component, they can get that information from talking with the recipients, the people who are in the business. He said he doesn't see Council shirking that ultimate responsibility and that when they're looking at tightening the financial picture for the entire City, they need to maintain that flexibility and only Council can do that. Collins is looking for a hybrid of Konopa's position on this issue and Kellum's position. He wants to look at the effective use of the allocation they currently have, the policies that guide that distribution, what is the correct disbursement of the entire fund, and look at the policy that guides the automatic increases. He doesn't believe that agencies should receive automatic increases; they should be required to show their return on investment.

Konopa added that with a motion to have Council be the reviewing body, a component of that can be to bring in an advisory body to advise Council, such as Travel Oregon, as well as hearing from recipients and other people in the business, and researching what other cities are doing with tourism funds.

Collins remarked that it's not going to be done by the end of January, by the time they get everybody together and receive input.

Konopa reaffirmed that there is a motion on the floor to have the Council as the review body, with that they can determine what the process will be.

Johnson agrees with Collins that the time needs to be taken to do this correctly, put in the effort, and review things thoroughly. She is concerned that with Council as the review body, they will bring their existing views to the table and it won't be anything new or different.

VOTE: A vote was taken on the motion and it failed 3-2 with Kellum and Johnson voting no. It takes four votes of the Council to make any decision. This item will come back to the next meeting that a full Council is present.

Collins added that he would still like to have staff make arrangements for the Expo Contractor to come back with a report for Council, as Councilor Olsen requested.

Konopa would also like staff to develop a plan and timeline for the review process, in the event that Council is going to be the review body.

REQUEST FOR PROPOSAL SEWER ECONOMIC DEVELOPMENT FUNDS

Assistant Public Works Director/City Engineer Jeff Blaine gave a brief explanation of the request for funds. As part of SnoTemp's expansion project, with their land use decision, they have a condition to extend 275 feet of sewer line in Lochner Road. SnoTemp is requesting the use of Sewer Economic Development funds to complete that project. With Council approval, staff would design and bid the project and once constructed, it would satisfy that condition for SnoTemp. The City has both Sewer and Water Economic Development Fund accounts which act as reserve funds when an economic development project comes up, or if there's some sort of business retention activity, people in the community that are looking to develop can come to Council and make a request to use those funds to construct improvements. Blaine explained that there are adequate reserves currently in the Sewer Economic Development Fund to construct this project. Staff came before Council a few months ago to discuss using some of those funds to construct the Cox Creek Interceptor improvements which have been delayed. He said that even with those improvements, there would be adequate funds to construct the requested improvements for SnoTemp.

Jason Lafferty, 3815 Marion Street SE, of SnoTemp, updated the Council on their 95,000 square foot expansion project. They will be applying for permits within the next week and have hopes for quick review so that they can start construction in January and catch next summer's harvest season. SnoTemp will not be tying into this sewer extension; it will benefit a neighboring property. Their engineers estimate the project at \$150,000 and they're requesting that the Council support this request for funds to complete the sewer extension project.

Konopa asked Lafferty how many years their family has been in business. Lafferty said that his Grandfather started the business in Eugene in 1957 and moved to Albany in 1974.

Kopczynski would like to know if this item will be going out for a Request for Proposal (RFP); and whether there's a "not to exceed" amount as part of the approval.

Blaine replied that Council could approve the request however they'd like, but the staff recommendation would be to approve the request and let staff take care of seeing the project through. He does not anticipate any dramatic increase in construction costs that would cause a budget issue.

Collins asked whether this is a 12 or 8-inch sewer line. The staff report and Lafferty's letter have different information. Lafferty clarified it is a 12-inch sewer line. Collins also asked, with a 12-inch line, whether there are System Development Charges (SDCs) involved with funding this project, either in addition to, or as an offset to, Sewer Economic Development Funds.

Utility Services Manager Mark Yeager is not certain whether Sewer SDCs are a part of the project.

Collins asked, since SnoTemp is not going to be connecting to the line for service and it's just a matter of extension of the line to and through that property, with future development to the East side making connections to the line, whether the project is subject to a subsequent developer reimbursement agreement so that the City receives part of that money back to put into the Economic Development Fund.

Blaine responded that the City would receive connection fees associated with each parcel that connected, which would go back to the Sewer Fund.

MOTION: Collins moved to approve the requested action with receiving clarification on the SDC funding and with the future direction that any subsequent developer reimbursement be returned to the Sewer Economic Development line item and Kellum seconded.

Konopa asked whether there is Local Improvement District (LID) that is associated with this property on the South end.

Yeager replied that the LID has now expired; it was only good for ten years.

VOTE: A vote was taken on the motion and it passed 6-0.

STORMWATER DISCUSSION, PART 5: FUNDING STRATEGIES

Blaine explained to Council that staff will be looking for direction regarding Stormwater funding methods going forward. He gave a brief summary of the discussions and presentations that have taken place over the last year and reminded Council that the Stormwater program hasn't changed much over the past few decades.

Blaine gave a Power Point presentation (see agenda file).

Blaine explained that he spent a bit more time reviewing the pipe maintenance portion of the presentation than any other item because he believes it's important to note that the City is spending between \$1 million and \$1.5 million per year to replace failing sewer lines. For stormwater, the City is spending nothing and is not making any headway on a worsening problem. Blaine further explained that the common theme of all presentations to date is the lack of stormwater funding. Albany does not have a dedicated source of funding for stormwater; when the City performs even limited stormwater activities, it's taking funds away from the Water and Sewer Funds, which already have limited budgets. He would like to hear from Council what funding options they may be willing to consider.

Blaine pointed out that this is not a new issue at all. He reviewed a discussion paper and a memo to Council that was written by Collins when he was the Public Works Director in 2001. Blaine noted that Collins' materials mirror the current discussions taking place. He said that although this is not a new issue, it is new with respect to the permitting requirements that Albany is facing. He believes that Albany needs stormwater funds to fully develop programs including regulatory compliance under the upcoming stormwater permit requirements, to conduct basic operation and maintenance activities, and to perform condition assessments that help identify which pipes are failing. He explained that once those pipes are identified, the City will need to have funds available to replace those pipes through a perpetual life replacement program. Lastly, there may be other pipe projects to be considered from a capacity standpoint to address issues like street flooding.

Blaine continued with the presentation slide title "Stormwater Funding Needs" and gave funding options for Council to consider. He said that if we continued with the current practice of using sewer and water funds to deal with infrastructure issues, it takes funds away from the needs within those systems. With the new requirements the City is facing for stormwater, the City would have to look at raising sewer rates in order to fund those additional expenditures. Customers would be paying based on their use of the sewer system and therefore, costs would be disproportionate to the customers' use of the stormwater system. Council could look at using General Fund monies but this would take funding from Police, Fire, Parks & Recreation, or the Library. With the General Fund being tax-based revenue, this would again be potentially disproportionate with a property's impact to or use of the stormwater system. Council could establish special districts or local improvement districts, neither of which would be practical from an administrative standpoint, or they could make use of general obligation bonds or operating levies, which again would be based on property taxes and may not be a legally defensible funding method.

Blaine stated that staff would recommend creating a stormwater utility and using revenues generated from stormwater-user fees, in combination with SDCs and permit fees, to cover the cost of a full stormwater program. He explained the benefits of a stormwater utility: fairness and equity – it can be designed to be proportionate to the customer's impact on the drainage system; a dependable revenue source – the City can have dedicated revenues designed to fund only stormwater management activities, just like we do for water and sewer; lastly, it is a legally defensible method of charging fees. There would be a site-specific fee for service or use of the system; therefore it's not technically a tax which is why it's defensible. He added that another benefit is having understandable rates for users.

Blaine provided a handout (see agenda file) titled "2015-16 Stormwater Rates in Oregon Cities by Fees", of Oregon cities, many of which are cities that the Councilors are used to seeing utility comparisons with, with the information sorted by population. Blaine noted that every community that is at least as large as Albany already has a stormwater fee and many communities that are smaller than Albany also have a stormwater fee. He pointed out that the handout is not an exhaustive list of the cities that have a stormwater utility. Of the 29 communities listed on the handout, only Albany and four other communities don't currently have a stormwater utility. Of those four cities that don't, one is in the process of developing a fee and one is getting ready to take the information to their Council for direction since they are being pulled into the stormwater permit requirements by the Department of Environmental Quality (DEQ) as well.

Blaine explained that the values shown on the handout are the rate for a single-family residence per month per lot. Olsen asked to clarify whether the fee is the same regardless of the size of a lot or residence. Blaine explained that

this is a boilerplate type rate structure where residential lots pay one rate and another rate is charged for commercial or industrial lots.

Blaine distributed a second handout (see agenda file) titled "2015-16 Stormwater Rates in Oregon Cities by Population", and explained that it contains the exact same information as the first handout, just sorted differently. It has the communities sorted based on the fee amount, with Sweet Home on the low end at \$1.00 per month for a single-family residence all the way up to \$27.00 per month for someone living in Portland. The average fee for the communities shown on the handout is \$9.00 per month.

Blaine explained that there is a lot of work to be done when considering what an appropriate fee for Albany might look like. Staff is looking for direction now as to what funding options Council would consider. If Council were interested in considering a stormwater utility, staff would come back to Council in a work session to define what the evaluation and development process would look like; so they're not being asked to make any rate decisions but rather deciding a path forward.

Kopczynski asked, since a lot of this is defined by impervious versus pervious surfaces, and using his home as an example, whether it is practical to consider removing the driveway and putting in gravel to make it pervious. He asked if there are any ways to incentivize people to do things like that. Blaine replied that there are ways to incentivize people to reduce the square footage of their impervious surfaces. Blaine remembers Yeager saying previously that in order to have a truly defensible stormwater program, the City would need to have a means to provide credits to people for those efforts.

Kellum wants to make sure this is part of the discussion from the beginning, not waiting until way down the road to decide what credits may be provided. He said that he would hate to see citizens take proactive measures to reduce their impervious spaces and then find out that the utility program doesn't allow for that and it's not going to benefit them.

Konopa pointed out that regardless of what a person may do with their own property, even if they don't have any stormwater drainage coming from their property, there's still the issue of stormwater drains underneath one hundred plus miles of roadway that need to be maintained or replaced. All storm drains need improvement and there needs to be some sort of funding mechanism to be able to maintain the entire system. Konopa added that she is proud of the fact that Albany has been able to hold off on having any utility fees up to this point and hold back the costs and utilize limited funds well, but it's gotten to a point where they have to question how much longer they can keep waiting. She stated that the City has a stormwater system that's deteriorating and they're going to have to somehow maintain it, and she doesn't see in the future that they're going to have another funding source to keep that type of program sustainable. Konopa would like for staff to identify on the list of cities listed in Blaine's handout, how many of those cities have more than one utility fee, noting that Corvallis has a few and other cities have other fees aside from a stormwater fee.

Collins added that the City has avoided this issue for many years but the driver now is not the City, the driver is the state and the permit that's coming down the road. He stated that they're not going to be able to avoid it for another five to ten years so the question then becomes, what's feasible to have a reasonable program. He doesn't believe any of the first four options are reasonable funding options because of the competing nature of the General Fund, the limited ability for taxation, and many of the points that Blaine made. He believes Council must look at developing a storm water utility, which can include credits, base fees, etc. He stated that staff needs to have the direction to come back to Council and provide the framework of what the utility would look like. He added that staff will need to perform budget projections; how much revenue does the City need to collect per year and then they can start to develop a user fee based on that. He explained that generally you try to treat user groups somewhat systematically. He pointed out that if they attempt to manage each residence independently, the program management costs are going to go up dramatically, so while they can have some forms of credits, they have to be very explicit, very direct, and not-necessarily always equitable. He said that if they want to be equitable, the operational costs will go way up and probably disproportionately so. He wants to make sure that they are reasonable, that they provide some flexibility, but don't go crazy over credits. He sees incentives coming into effect more with large commercial or industrial parcels.

MOTION: Collins moved to direct staff to begin work to develop the formation of a stormwater utility and bring it back to Council and Kopczynski seconded.

Johnson asked, on the lists of cities that have been handed out, whether the amounts listed are a flat fee, or if they're based off of sewer usage. Blaine replied that they are stormwater-only flat fees for single-family residential customers; there would be other charges for different user groups. Johnson said she agrees with Collins, they need to keep it simple and have a flat fee, and that she hates to see any sort of fee imposed, people are getting driven out of their homes by fees. Collins then commented that whatever the fee is there has to be some sort of link back to a reasonable level of discharge from a typical single-family residence. He stressed that the City can't individually manage twenty to twenty-five thousand single-family lots. Once a homeowner makes a change, they receive their credit, and then they can change back to what they were doing before, and the City loses control of the billing system very quickly. He reiterated that they're not going to answer those questions today; it's just a matter of directing staff to begin the process to develop the utility.

Kellum asked about a recent Supreme Court ruling against the Environmental Protection Agency (EPA) brought by the State of Virginia in which the EPA claimed that stormwater was a pollutant and Virginia disputed that claim. Blaine believes this was an issue relating to volume as a contaminate, not the water itself, and that the claim by EPA was overruled. Yeager explained to Council that he doesn't know the final outcome of the lawsuit but Blaine is correct in saying it was not related to the quality of the stormwater but rather the volume. The EPA was attempting to try to force control of volume. He stated that this sort of issue is happening all around the Nation but again, it was volume related; the assertion that volume alone was a pollutant, rather than any constituents that were inside the rainwater.

VOTE: A vote was taken on the motion and it passed 6-0.

FINANCIAL NEEDS ASSESSMENT

Assistant City Manager/Chief Information Officer Jorge Salinas provided an updated copy of the spreadsheet that was included in the packet (see agenda file). He explained that Council requested a few months ago that staff provide a projection of the current financial needs in order to evaluate distribution of funds from the General Fund in addition to potential funding sources in the future. Salinas explained that this is a projection for the next ten years with input from Directors in terms of what they see as a priority or risk. Based on feedback from Council, staff will go ahead and prepare a similar report to provide to the Budget Committee at their first meeting.

Salinas gave a Power Point presentation (see agenda file).

Discussion followed. Police Chief Mario Lattanzio explained to Council that he did not project personnel needs for the next ten years; only the next three to four years. He believes that Albany is currently short eight officers but being realistic, he figured in adding one officer per year. Konopa asked Fire Chief John Bradner for clarification whether his figures were a ten-year projection. Bradner explained that his projections for the Fire Department were for eight years. Konopa asked for Lattanzio to update the Police Department figures to reflect eight years as well. Lattanzio asked for clarification as to whether he should figure the true number of officers he really believes we need, because that will dramatically increase the numbers.

Collins commented that there needs to be some guidance as far as population growth and whether they look at the national average or something else. Then they can ask the general question of whether staff is headed in the right direction; pulling that information together first and then they can define some of those boundaries by which Council wants information included. Collins said that he would agree that some things need to be updated but encouraged the Councilors not get bogged down with those details right now. Collins believes that this financial needs assessment is absolutely essential to be able to make decisions about staffing needs, set priorities with projects, and look at the overall needs of the City in a comprehensive picture. Collins envisions this as a communication tool with the public, to be able to show the citizenry what the City is anticipating one year, five years, and ten years out. He said that part of needing this report is to address the common complaint of "the City never has enough money and they're always asking for more." He feels that if Council sees there's a need now, they need to be telling the public what it is now, not when they're scrambling. Collins also believes this will be an important part of outlining community priorities and working with neighboring communities to establish what they should be doing regionally. He explained that Council needs to provide guidance to staff as to what the expectations are, they need to be consistent with growth projects and percentages, and they need to tie this back to some level of indexing so they know how the expenses can be calculated in future years. He said the question is whether they think that staff is headed in the right direction to provide them with a product that they think will be beneficial to the community.

Kopczynski said he agrees completely with Collins and that because some of these items are being driven by things they don't have any control over, they need to keep their lines of communication open and stay in a positive relationship with legislators, considering these things are coming from the top down.

Collins added that having a document like this would help with those discussions with legislators, to be able to sit down and say, look what Albany is facing financially. He feels that if they don't have a document like this, they're just whistling in the wind. He wants to be able to present to legislators their best assessment of all the things they're asking the citizenry for. Collins added that when they get down to looking at personnel, for example, is a second firefighter equal to one Parks employee; they need this information to be able to make those choices. He recognizes that they're going to be facing this question year after year and this report at least gives Council the ability to condense, bring everything together, and have a common point of discussion for the future. He feels it also gives them some common understanding and a common decision-making approach.

Kellum said he would like to see the estimated personnel costs prioritized and color-coded. He believes this would be helpful in the long run to know the highest priorities and the color-coding would help for those people that are visual.

Konopa would like this presentation brought back to the Budget Committee in January. Hare said that based on some of the feedback received, the hope is that staff will be able to come back to that meeting with an updated report.

Collins asked whether the growth-related projects will be paid for by SDCs or other sources and whether it's already in the methodology. He would like to have that question answered.

COUNCILOR COMMENTS

Olsen said he received an email from a citizen about several trailers being moved onto their neighbor's lot.

Management Assistant/Public Information Officer Marilyn Smith explained to Council that a Community Resource Officer checked on the issue that day and the person who owns one of the motor homes came in to apply for a temporary RV parking permit on that property as well. The applicant claims this is supposed to be a very temporary situation. The application for a temporary parking permit requires that the applicant get approval from all of the adjacent property owners, which includes the claimant, so the application may not go very far. Smith added that the property is in foreclosure but the County won't take possession until 2017, according to the tax records. The City received a tall grass complaint two years ago and another complaint this August, for which a letter was sent, but she does not believe it generated a response. She said the situation with the motor homes is fairly new.

Olsen asked whether the motor home is on foreclosed property. Smith explained that it is on a property that is going through the foreclosure process and it is believed that the house is still occupied.

The Councilors confirmed details for the Veterans Day Parade.

CITY MANAGER REPORT

Hare wanted to apologize for giving the impression earlier the meeting that a one million dollar budget was not significant or important; that was not what he intended. He pointed out that the Council and staff members spend a lot of time discussing the one million dollars from TLT and a whole lot less time discussing the \$160 million that's in the City budget. He would just like to see the City have a process that gets to an outcome that everyone can hopefully agree on and they can move forward. The Councilors agreed and indicated that they understood his intent earlier in the meeting.

ADJOURNMENT

There being no other business, the meeting was adjourned at 5:58 p.m.

Respectfully submitted,

Holly Roten
Administrative Assistant I

Reviewed by,

Stewart Taylor
Finance Director



TO: Albany City Council
VIA: Wes Hare, City Manager
FROM: John R. Bradner, Fire Chief *JRB*
DATE: December 29, 2015, for the January 13, 2016, City Council Meeting
SUBJECT: Acceptance of the Fiscal Year 2015-2016 State Hazardous Materials Emergency Preparedness Grant on Behalf of the Mid-Valley Local Emergency Planning Committee

RELATES TO STRATEGIC PLAN THEME: ● A Safe City

Action Requested:

City Council approval by resolution to approve application and accept and appropriate funds for the Fiscal Year (FY) 2015-2016 State Hazardous Materials Emergency Preparedness (HMEP) Grant on behalf of the Mid-Valley Local Emergency Planning Committee (LEPC) for evacuation planning and a hazardous materials response exercise and responder training.

Discussion:

The City of Albany participates on the Mid-Valley LEPC. The State Fire Marshal's Office provides grant opportunities to Oregon LEPCs for hazardous materials emergency preparedness, and the City receives and distributes the grant funds on behalf of the LEPC.

The Mid-Valley LEPC has been notified by the State Fire Marshal's Office that they have been approved for the FY 2015-2016 HMEP grant for the following activities:

1. Mid-Valley LEPC Evacuation Plan, Segment 3
2. Hazardous Materials Tabletop Exercise
3. Hazardous Materials Awareness and Operations Training for Linn and Benton County Emergency Responders

Budget Impact:

\$26,000 - FY 2015-2016 Hazardous Materials Emergency Preparedness Grant (203-25-5089)

The grant requires an in-kind match of \$6,500, which would be satisfied through personnel participation in grant-supported activities.

JB:ljh

Attachment

RESOLUTION NO. _____

A RESOLUTION APPROVING APPLICATION AND ACCEPTING AND APPROPRIATING FUNDS FOR THE FISCAL YEAR 2015-2016 STATE HAZARDOUS MATERIALS EMERGENCY PREPAREDNESS GRANT ON BEHALF OF THE MID-VALLEY LOCAL EMERGENCY PLANNING COMMITTEE FOR EVACUATION PLANNING AND A HAZARDOUS MATERIALS RESPONSE EXERCISE AND RESPONDER TRAINING.

WHEREAS, the City of Albany participates on the Mid-Valley Local Emergency Planning Committee; and

WHEREAS, the Mid-Valley Local Emergency Planning Committee applied for funding through the FY 2015-2016 State Hazardous Materials Emergency Preparedness Grant; and

WHEREAS the State has notified the Mid-Valley Local Emergency Planning Committee that they will be awarded funding through the FY 2015-2016 State Hazardous Materials Emergency Preparedness Grant for evacuation planning, a hazardous materials tabletop exercise, and hazardous materials responder training; and

WHEREAS the FY 2015-2016 State Hazardous Materials Emergency Preparedness Grant requires an in-kind match which can be satisfied through member participation in grant-supported activities; and

WHEREAS, the State requires that a city or fire service agency be the pass-through for receipt and distribution of grant funds on behalf of the local emergency planning committees; and

WHEREAS, Oregon Local Budget Law provides that expenditures in the year of receipt of grants, gifts, bequests or devices transferred to the local government in trust for a specific purpose may be made after enactment of a resolution or ordinance authorizing the expenditure (ORS 294.326(3)).

NOW, THEREFORE, BE IT RESOLVED, the Albany City Council approves application for and authorizes the Fire Department to receive funds in the amount of \$26,000 from the FY 2015-2016 State Hazardous Materials Emergency Preparedness Grant on behalf of the Mid-Valley Local Emergency Planning Committee for evacuation planning and hazardous materials responder training.

BE IT FURTHER RESOLVED, the FY 2015-2016 State Hazardous Materials Emergency Preparedness Grant funds are hereby appropriated as follows:

Resources:	Debit	Credit
203-25-5089-42017		\$ 26,000
 Requirements:		
203-25-5089-60101	\$ 21,000	
203-25-5089-61024	\$ 5,000	

DATED AND EFFECTIVE THIS 13TH DAY OF JANUARY 2016.

Mayor

ATTEST:

City Clerk



TO: Albany City Council

VIA: Wes Hare, City Manager

FROM: John R. Bradner, Fire Chief *JRB*

DATE: January 7, 2016, for the January 13, 2016, City Council Meeting

SUBJECT: InterCommunity Health Network-Coordinated Care Organization (IHN) Funding for a One-Year Pilot Community Paramedic Program

RELATES TO STRATEGIC PLAN THEME: ● A Safe City

Action Requested:

City Council approval by resolution to enter into a contract with IHN and appropriate funds for a one-year pilot Community Paramedic Program.

Discussion:

The Fire Department responds to over 7,600 emergency calls for service annually, of which over 6,600 are medical related, resulting in over 4,400 transports to a hospital emergency room. The Department has experienced a 6.1 percent increase in emergency calls over the last year and a 24.6 percent increase over the last five years. The Fire Department has been looking at alternative ways to reduce the number of emergency transports and the impact on emergency service operations.

The Fire Department requested financial support from IHN to establish a Community Paramedic Program (CPP) to provide a new, transformational model of healthcare in our community. The CPP will connect at-risk populations to appropriate resources, including patients who frequently use emergency services. The funding would provide for the staff and equipment to fully operate the program, with no impact to current emergency response capabilities. IHN has approved funding for the CPP, pending approval from the IHN Regional Planning Council and the Albany City Council.

IHN received approval from the Centers for Medicare and Medicaid Services (CMS) for its Transformation Plan beginning July 1, 2013. This approval allows IHN to put pilot programs and projects in place that are aimed at improving local health and healthcare in Linn, Benton, and Lincoln Counties.

Community paramedicine is a model of community-based healthcare in which paramedics function outside their customary emergency response and transport roles in ways that facilitate more appropriate use of emergency care resources and enhance access to primary care for medically underserved populations. These programs are typically designed to address specific local problems. Nationwide interest in community paramedicine has substantially grown in recent years based on the belief that it will improve access to and quality of care while also reducing costs.

The Fire Department's CPP would include assessments, follow-up, and treatment to provide education and referrals in order to guide IHN patients and others toward health and wellbeing, connect them with available services, and intervene with those who are unable or unwilling to take an active role in the management of their healthcare. As a result, this will reduce healthcare

Albany City Council

Page 2

January 7, 2016, for the January 13, 2016, City Council Meeting

costs by providing a more appropriate level of care and reducing 9-1-1 calls and the Fire Department's response to medical emergencies, which helps address the overall increase in emergency responses.

Budget Impact:

Receipt of \$290,400 from IHN-CCO for a Community Paramedic Program (203-25-5117).

JB:ljh

Attachment

RESOLUTION NO. _____

A RESOLUTION TO ENTER INTO A CONTRACT AND APPROPRIATE FUNDING FOR A ONE-YEAR PILOT COMMUNITY PARAMEDIC PROGRAM FROM THE INTERCOMMUNITY HEALTH NETWORK-COORDINATED CARE ORGANIZATION.

WHEREAS, the Fire Department experiences a significant increase in emergency calls for service annually, but funding is not available to support a commensurate increase in emergency resources; and

WHEREAS, the Fire Department is evaluating programs and service delivery that can reduce the number of emergency calls for service; and

WHEREAS, approximately 66 percent of all emergency calls for service result in transport to a hospital emergency room; and

WHEREAS, a Community Paramedic Program can reduce the number of emergency transports to hospital emergency rooms by focusing on in-home care and referral to alternative healthcare options; and

WHEREAS, the InterCommunity Health Network-Coordinated Care Organization through the Centers of Medicare and Medicaid Services can put pilot programs and projects in place that are aimed at improving local health and healthcare in Linn, Benton, and Lincoln Counties; and

WHEREAS, with InterCommunity Health Network-Coordinated Care Organization funding, the Fire Department would initiate a one-year pilot Community Paramedic Program to provide transformative healthcare aimed at decreasing emergency calls and transports to hospital emergency rooms; and

WHEREAS, Oregon Local Budget Law provides that expenditures in the year of receipt of grants, gifts, bequests or devices transferred to the local government in trust for a specific purpose may be made after enactment of a resolution or ordinance authorizing the expenditure (ORS 294.326(3)).

NOW, THEREFORE, BE IT RESOLVED, the Albany City Council authorizes the Fire Chief to enter into a contract with InterCommunity Health Network-Coordinated Care Organization to receive \$290,400 for a one-year pilot Community Paramedic Program.

BE IT FURTHER RESOLVED, the funds are hereby appropriated as follows:

Resources:	Debit	Credit
203-25-5117		\$290,400
Requirements:		
203-25-5117	\$290,400	

DATED AND EFFECTIVE THIS 13TH DAY OF JANUARY 2016.

Mayor

ATTEST:

City Clerk



TO: Albany City Council

VIA: Wes Hare, City Manager
 Kate Porsche, Economic Development and Urban Renewal Director *KCP*

FROM: Staci Belcastro, P.E., City Engineer *SB*
 Chris Cerklewski, P.E., Civil Engineer III *CLC*

DATE: January 4, 2016, for the January 13, 2016, City Council Meeting

SUBJECT: Award of Bid for MS-16-01, Dave Clark Riverfront Trail Lighting

RELATES TO STRATEGIC PLAN THEME: • A Safe City

Action Requested:

Staff recommends that Council award this contract in the amount of \$298,008.10 to the low bidder, Crawford Electric of Stayton, Oregon.

Discussion:

On Tuesday, November 10, 2015, bids were opened for MS-16-01, Dave Clark Riverfront Trail Lighting. There were four bids submitted for this project, ranging from \$203,678.10 to \$373,586.00 for the Schedule A base bid, and ranging from \$298,008.10 to \$466,668.00 for the base bid plus additive alternate bid, Schedules A + B. The Engineer's estimate was \$215,000 for Schedule A and \$290,000 for Schedule A + B. A bid summary is provided as Attachment 1.

Project Description

This project will replace existing lighting in Monteith Park and along the Dave Clark Riverfront Trail from Monteith Park to Oak Street with new LED lighting. The existing lighting in these areas is either substandard or nonexistent, and is becoming difficult to maintain due to its age. The lack of good lighting has been identified by the Parks and Recreation and Police Departments as a major contributor to illicit activities along the riverfront. The improved lighting will make the riverfront more attractive to recreational use and make enforcement much easier. This project was authorized by the Central Albany Revitalization Agency Advisory Board on March 18, 2015.

Schedule Comparison and Award Recommendation

This project was bid with a base bid (Schedule A) and an alternate additive bid (Schedule B) to facilitate cost comparison of two potential purchasing methods for the lighting materials; owner furnished using the US Communities Government Purchasing Alliance or contractor furnished. The estimated cost for owner-furnished lighting materials purchased through the US Communities Government Purchasing Alliance is estimated to be approximately \$90,000. The cost for contractor furnished lighting materials based on bids submitted for this project is \$94,330. A summary of the total estimated cost of each purchasing option is provided in the table below.

Award Option	Base Bid	Lighting Materials	Total
Schedule A	\$203,678.10	\$90,000.00 <i>City-Purchased Lights</i>	\$293,678.10
Schedule A + B	\$203,678.10	\$94,330.00 <i>Contractor-Purchased Lights</i>	\$298,008.10

Staff recommends Council award Schedule A + B which provides for the Contractor supplying the lighting materials. The cost for this option is slightly higher when compared to the City purchasing the lighting materials through the US Communities Government Purchasing Alliance; however, the City furnishing materials for this contract will require additional staff time that would be charged to the project and the City is at risk being responsible for potential costs resulting from delays or errors in the delivery of the lighting materials.

Summary of Total Estimated Project Costs

Based on the project bid and anticipated related costs, a summary of the total estimated project cost is shown in the table below. The amounts have been rounded to the nearest \$100.

Project Components	Estimated Cost
I. Costs	
a. Engineering	\$15,000
b. Construction Inspection	\$10,000
<i>Engineering Subtotal</i>	\$25,000
II. Construction Costs	
a. Construction Contract	\$298,000
b. Construction Contingency (10%)	\$29,800
<i>Construction Subtotal</i>	\$327,800
<i>Total Estimated Project Cost</i>	\$352,800
<i>Project Budget</i>	\$352,800
<i>Under/(Over) Project Budget</i>	\$0

Budget Impact:

This project will be funded by the Central Albany Revitalization Agency (290-11-1102).

CC:kw

Attachment



CITY OF ALBANY, OREGON
Public Works Department
Construction Contract Bids

Project: MS-16-01, Dave Clark Riverfront Trail Lighting

Bid Opening: Tuesday, November 10, 2015

Engineer's Estimate	Crawford Electric (Stayton)	Benton Electric (Albany)	EC Company (Albany)	Tornado Soft Excavation (Independence)
\$290,000.00	\$298,008.10	\$356,611.67	\$450,113.00	\$466,668.000



TO: Albany City Council
FROM: Sharon Konopa, Mayor *Sharon (ed)*
DATE: January 7, 2016, for January 13, 2016, City Council Meeting
SUBJECT: Appointments to City Boards, Committees, and Commissions

RELATES TO STRATEGIC PLAN THEME: • An Effective Government

Action Requested:

Council approval of the following appointments to City advisory groups:

Airport Advisory Commission

Chuck Kratch (Councilor Collins' reappointment)

Albany Arts Commission

Jane Donovan (Mayor's reappointment)

Lynn Whitacre (Mayor's reappointment)

Albany Bicycle and Pedestrian Advisory Commission

Jim Lawrence (Mayor's reappointment)

Terry Virnig (Mayor's reappointment)

Bill Pintard *[application attached]* (Mayor's new appointment)

Robert Bornheimer *[application attached]* (Mayor's new appointment)

[fill vacant position due to resignation; term expires 12-31-2017]

Albany Budget Committee

Jeff Christman (Councilor Kellum's reappointment)

Colleen Keller (Councilor Johnson's reappointment)

Building Board of Appeals

Dan Watson (Mayor's reappointment)

City Tree Commission

Brenda Sterner (Mayor's reappointment)

James Smith *[application attached]* (Mayor's new appointment)

[fill vacant position due to resignation; term expires 12-31-2016]

Community Development Commission

Terry Knoll, social services representative (Mayor's reappointment)

Sharon Konopa, City Council representative (Mayor's reappointment)

Landmarks Advisory Commission

Cathy LeSeur (Mayor's reappointment)

Kerry McQuillin *[application attached]* (Mayor's new appointment)

Parks & Recreation Commission

Will Sheppy *[application attached]* (Mayor's new appointment)

Planning Commission

Dan Sullivan, Ward II (Councilor Kopczynski's reappointment)
Robert Schueller, Ward III [*application attached*] (Mayor's new appointment)

Public Safety Commission

Kevin Kreitman [*application attached*] (Mayor's new appointment)
Nick Fowler, rural fire districts' representative (Mayor & Council's reappointment)

Senior Center Endowment Committee

Connie Lanham (Mayor's reappointment)
JoAnn Miller [*application attached*] (Mayor's new appointment)

Traffic Safety Commission

Ron Green (Mayor's reappointment)
Chuck Kratch (Mayor's reappointment)
Will Sheppy (Mayor's reappointment)

Discussion:

Recommendations for any remaining vacancies on the City advisory groups will be submitted for approval at subsequent Council meetings.

Budget Impact:

None.

SK:ldh

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BOARD, COMMISSION, AND COMMITTEE APPLICATION

(Please print legibly or type)

RECEIVED
NOV 25 2015
City of Albany
City Manager's Office

AK2
LW

CITY HALL
333 Broadalbin Street SW
P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net

(541) 917-7500

Board, Commission, and/or Committee Preference:

I Bicycle and Pedestrian Advisory Commission

(list all for which you are applying)

Name: William Pintard

Preferred First Name: Bill

Residential Information:	
Home Address: <u>Albany, OR 97322</u>	Phone: <u>541-967-32955</u>
	Cellular: _____
E-mail: <u>b.pintard@comcast.net</u>	Fax: _____ (Optional)

Employment Information:	
Employer's Name: <u>Retired</u>	
Work Address: _____	Phone: _____
	Cellular: _____
E-mail: _____	Fax: _____ (Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

I served on the Bicycle and Pedestrian Advisory Commission for 10 years but decided to take a leave of absence. I would like to reapply for a position on the Commission.

<p>For City use only: Ward: I II <u>III</u> or Lives Outside City Limits. (Circle One)</p> <p>If lives outside city limits, does applicant meet special definition for the specific b/c/c for which applying?</p> <p>Yes No If yes, how? _____</p>



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

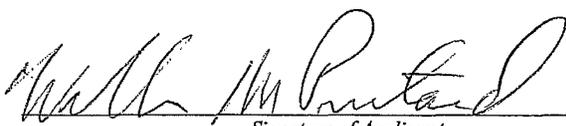
Served on the Bicycle and Pedestrian Commission for 10 years and the Landmarks Advisory Commission for 8 years. I am a member of Santiam Spokes Bicycle Club. I volunteered for SHIBA for 8 years and Call a Ride for 5 years. I have served on the I-5 Citizens Advisory Group.

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

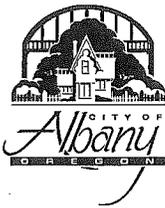
I have been an active cyclist for most of my life and have an interest in improving the cycling and pedestrian facilities in Albany.

- What contributions do you hope to make?

My prior experience on this Commission and my knowledge of the larger cycling community. I have contacts in both the Corvallis Bicycle and Pedestrian Commission, Travel Oregon and various other organizations involved in cycling advocacy.


Signature of Applicant

11/24/15
Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

(Please print legibly or type)

Handwritten initials

Board, Commission, and/or Committee Preference:

Bicycle and Pedestrian Advisory Commission

(list all for which you are applying)

CITY HALL
333 Broadalbin Street SW
P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net

(541) 917-7500

Name: Robert Bornheimer Preferred First Name: Rob

Residential Information:	
Home Address:	Phone: <u>541.967.0550</u>
<u>Albany OR 97321</u>	Cellular: _____
E-mail: _____	Fax: _____ <i>(Optional)</i>

Employment Information:	
Employer's Name: _____	Phone: _____
Work Address:	Cellular: _____
<u>Albany OR 97321</u>	Fax: _____ <i>(Optional)</i>
E-mail: _____	

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

Sales Manager for ATI in Millersburg (~7 yrs)

Leadership Albany 2015-2016

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If lives outside city limits, does applicant meet special definition for the specific b/c/e for which applying?

Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM
Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

AYSO Soccer Coach (4 Yrs)

Boys & Girls Club Coach (1 Yr)

North Albany City Cleanup Events

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

Having been a citizen of Albany for 40+ years, I have seen the need for continued safety for cyclist and pedestrians in our community. Improvements in signage, sidewalks, bike paths, and community awareness have encouraged me to get involved in the services provided and help educate others. I have grown to love recreational bicycling over the past three years and want to promote both safety and benefits of a healthy lifestyle.

PARENT & HUSBAND TO A FAMILY THAT ENJOYS BICYCLING!

- What contributions do you hope to make?

I HOPE TO OFFER A NEW SET OF EYES TO CURRENT CHALLENGES AND OPPORTUNITIES. I ENJOY RESEARCHING PROJECTS AND LOOKING FOR ALTERNATIVES THAT PROVIDE THE BEST VALUE AND LOWEST OVERALL COST AND RISK.


Signature of Applicant

11/10/15
Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

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City Manager's Office

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P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net

(541) 917-7500

Board, Commission, and/or Committee Preference:

Tree Commissioner

(list all for which you are applying)

Name: James D. Smith

Preferred First Name: Jim

Residential Information:

Home Address

Albany, Oregon, 97321

Phone: _____

Cellular: _____

E-mail: _____

Fax: _____

(Optional)

Employment Information:

Employer's Name: Currently not employed

Work Address: _____

Phone: _____

Cellular: _____

E-mail: _____

Fax: _____

(Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

Hewlett-Packard 1993 - 2009; Manufacturing Development Engineer, Research and Development Engineer. Hewlett-Packard 2012 - 2015; contract engineer.

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Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

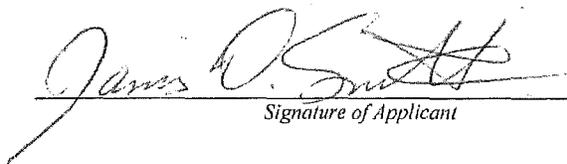
Active with identifying and working with the Albany Parks and Recreation maintenance to resolve acts of vandalism. I have volunteered to be their eyes and document vandalism in the Teloh-Catapooia Park area. I worked to provide improved communication to the public regarding the invasive weed spraying program that resulted in a change of protocol. I participated in the Calapooia River Water shed evaluation this year. I gave a talk at the Arbor Day festival on "Birds of the Teloh-Calapooia Area". I have been a participant in the Albany Strings Orchestra since its beginning and am presently the Principle Second Violin. The Albany Strings Orchestra is primarily an adult community band that gives free concerts.

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

I have found compelling evidence that this area is an unrecognized area of biodiversity with birds from the interior US mixing with the birds of the Pacific coast. This area has a great deal of potential and I would like to preserve what we have and to promote increased efforts on conservation and restoration. I decided to redo my city yard landscaping from "typical" to using plants, in order of priority in the decision matrix: Oregon native, US native, and non-invasive-anywhere alien. It wasn't easy as the plan provided by a "local expert" was flawed and a surprising amount of information on the "net" is either incorrect or conflicting. The increase in biodiversity in my yard has been tremendous and the effect could be extended to City of Albany, area business, and residential landscaping.

- What contributions do you hope to make?

The biggest contribution I would like to make is to make folks realize that this area isn't just a city of people. It is an area of potentially high biodiversity that brings in birds, and likely animals from the interior US as well as the Pacific coast. Evidence already is present based upon the birds seen in the valley. If we can implement programs that will enhance the conservation and parks areas, we can change the "potential" to reality and make this area beneficial to all species. Keeping what we have is important, improving what we have is the next step. A combination of native plant propagation in conjunction with invasive plant removal can boost the viability for all species. A case in point; I requested the City Parks group to remove a pile of litter from behind a bench; that spring a beautiful giant Western Trillium grew up in the location of the debris.


Signature of Applicant

12/13/2015

Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

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AM

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333 Broadalbin Street SW
P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net

(541) 917-7500

Board, Commission, and/or Committee Preference:

Landmarks Advisory Commission :

(list all for which you are applying)

Name: Kerry McQuillin

Preferred First Name: Kerry

Residential Information:

Home Address:

Albany, OR 07321

Phone: _____

Cellular: _____

E-mail: kerry.mcquillin@gmail.com

Fax: _____

(Optional)

Employment Information:

Employer's Name: _____

Work Address:

Corvallis, Oregon 97331

Phone: _____

Cellular: _____

E-mail: _____

Fax: _____

(Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

I work in the Office of Equity and Inclusion at OSU. My work focuses primarily on data cleaning and processing for the affirmative action plan. I also work with cognitive bias training and systems development and revision in accordance with new regulations.

My master's degree is in exercise psychology and prior to my current work, I worked in wellness education.

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Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

I am embarrassed to have nothing to list here. My husband and I just moved to Albany (back to Albany for me) and in our previous locations, we struggled to commit to being involved because we knew they were temporary. As I was growing up, I was very active in the community, but that fell by the wayside with college, graduate school, and not feeling rooted. We hope that Albany will be our home for the rest of our lives and I am eager to start really settling in by getting engaged in the community I love again.

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position

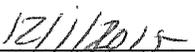
For the Landmarks Advisory Commission (LAC), I simply bring passion. I don't have any formal education that would apply. I grew up in the Woodworth house on 6th and Walnut and grew to have a great appreciation for older homes and maintaining their character. That home has what some might describe as "torture chamber" wrought iron light fixtures and with each new owner (it's for sale now), I worry that the light fixtures will be thrown out. I can give many examples of my worries about the interiors of homes in this town that I feel personal attachment to. I know the LAC does not regulate interiors, as perhaps that is too invasive, but I am grateful that they work to preserve exteriors. I now live in a 1941 home that was misidentified as a 1960 home, and in my neighborhood there are many beautiful older homes, but a few were converted in the 1970s and the impact on the neighborhood is noticeable. *See below for Parks & Rec

- What contributions do you hope to make?

For the Parks & Recreation Commission, I have studied Exercise Psychology and have some formal awareness about how the built environment impacts motivation for physical movement. I also was very active growing up and benefitted from the parks system in Albany and would be pleased to give back.

Because I am new to commission work, I think my most important first step would be to listen and learn in order to determine what contributions I could offer that would be most helpful. I know I am good at hearing multiple perspectives, enhancing communication, developing systems and policies, interpreting rules and laws fairly and consistently, and thinking creatively. All of these things would create opportunities for me to contribute.


Signature of Applicant


Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

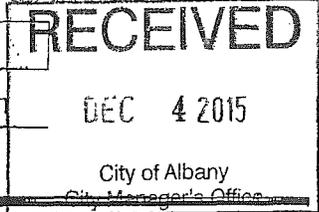
(Please print legibly or type)

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Albany, OR 97321-0144
www.cityofalbany.net
(541) 817-7500

Board, Commission, and/or Committee Preference:

Parks & Recreation Commission

(list all for which you are applying)



*AKK
JAC*

Name: Willard Sheppy

Preferred First Name: Will

Residential Information:

Home Address: 3306 SE Columbus ST Phone: 541-760-9670
Albany, OR 97322 Cellular: 541-760-9670

E-mail: WillSheppy@gmail.com Fax: _____
(Optional)

Employment Information:

Employer's Name: Valley Health Acupuncture

Work Address: 220 5th Ave SW Phone: _____
Albany, OR 97322 Cellular: _____

E-mail: _____ Fax: 541-926-5540
(Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____
Owner and Acupuncturist at Valley Health Acupuncture LLC.

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If lives outside city limits, does applicant meet special definition for the specific b/c/e for which applying?
 Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

Traffic Commission / City of Albany 10'14 - Present
 Student Council / Oregon College of Oriental Medicine 6'06 - 9'09
 Volunteer Instructor/ Shin's Martial Arts 6'06 - Present
 Team Liberation Human Relations Facilitator / Oregon State University 9'01 -- 9'06
 Study Circle Facilitator/ Community Alliance for Diversity 10'03 -- 9'05
 Judicial Board Student Government / Oregon State University 9'01 - 9'03

Please See Resume

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

Albany is a fun and vibrant community that has so much to offer and it takes everyone working together to enhance it.

I enjoy working with and learning from groups of people and engaging in dialogue that is both informative and motivating. I have a strong desire for building connections within my community and supporting the growth of Albany.

I consistently seek out new challenges for myself and provided support for the growing needs of organizations I have worked with. At Albany Acupuncture Clinic I started a Saturday community clinic, built a website and made more efficient its office and herbal dispensary. In my martial Arts

* continued on next page.

- What contributions do you hope to make?

Parks and Recreation

My Environmental Science education combined with my mediation and facilitation work has given me a unique passion for sustaining collaboration between community stakeholders and natural resources.

Traffic Safety

Maintaining public safety is the foundation of the city. If the citizens of Albany do not feel safe to live and travel. It will not matter how nice our parks or business.

Wells

Signature of Applicant

12/4/15

Date

BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM
Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

Traffic Commission / City of Albany 10'14 - Present
Student Council / Oregon College of Oriental Medicine 6'06 – 9'09
Volunteer Instructor/ Shin's Martial Arts 6'06 -Present
Team Liberation Human Relations Facilitator / Oregon State University 9 '01 — 9'06
Study Circle Facilitator/ Community Alliance for Diversity 10'03 — 9'05
Judicial Board Student Government / Oregon State University 9'01 - 9'03
Please See Resume

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

* → clinic, built a website and made more efficient its office and herbal dispensary. In my martial Arts career I founded a Portland branch of the school. Working at Linn Benton Mediation Service I enhanced the volunteer program and established new training opportunities. At Oregon State University I helped found Team Liberation, a human relations facilitation team.

I believe the aforementioned examples prove my dedication to promoting diverse community, increasing the quality of life and improving the economy of Albany.

- What contributions do you hope to make?

Parks and Recreation

My Environmental Science education combined with my mediation and facilitation work has given me a unique passion for sustaining collaboration between community stakeholders and natural resources.

Traffic Safety

Maintaining public safety is the foundation of the city. If the citizens of Albany do not feel safe to live and travel. It will not matter how nice our parks or business.

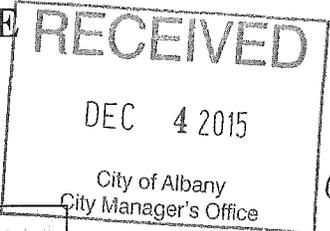
Signature of Applicant

Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

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CITY HALL
333 Broadalbin Street SW
P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net
(541) 917-7500

Board, Commission, and/or Committee Preference:

Planning

(list all for which you are applying)

Name: Robert Schueller

Preferred First Name: Bobby

Residential Information:	
Home Address: _____ <u>Albany, OR 97322</u>	Phone: _____ Cellular: _____
E-mail: _____	Fax: _____ <i>(Optional)</i>

Employment Information:	
Employer's Name: _____	Phone: _____
Work Address: _____ <u>Salem, OR 97301</u>	Cellular: _____
E-mail: _____	Fax: _____ <i>(Optional)</i>

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

Chief of Staff - State Senator Sara Gelser

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Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

Intern - City of Albany Department of Economic Development
Deputy Field Director - 2014 Senate District 8 campaign

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

I am in hopes to bring a younger perspective from a resident in Ward III, which seems to be
under-represented in both the planning and budget committees. I am specifically interested in

these two committees because I believe they play a significant role in Albany's development. I
want to ensure that Albany provides the same (or better) opportunities for my children that it did

for me, and having a voice in the conversation that shapes those opportunities is the best way to
do that.

- What contributions do you hope to make?

I hope to bring the insights, concerns, and opinions of my neighbors forward to ensure they are
being heard at the committee level. I hope to be a valuable piece of the conversation, whether

that conversation is the building of a new neighborhood or the management of budget items. I
hope to use my experiences and insight to help make Albany a better place to live. Most of all, I

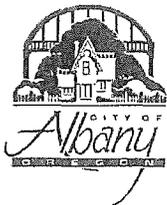
hope to learn a great deal about our local government.



Signature of Applicant

11/30/2015

Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

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P.O. Box 490
Albany, OR 97321-0144
www.cityofalbany.net
(541) 917-7500

Board, Commission, and/or Committee Preference:
Public Safety Commission
(list all for which you are applying)

Name: Kevin Kreitman Preferred First Name: _____

Residential Information:
Home Address: 4105 Moose Run Dr. SW. Phone: 541-981-2389
Albany, OR 97321 Cellular: 530-921-1711
E-mail: kevinlkreitman@outlook.com Fax: _____
(Optional)

Employment Information:
Employer's Name: Retired
Work Address: _____ Phone: _____
_____ Cellular: _____
E-mail: _____ Fax: _____
(Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____
Formerly Fire Chief City of Redding, previously with the Albany Fire Department for 27 years.

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Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

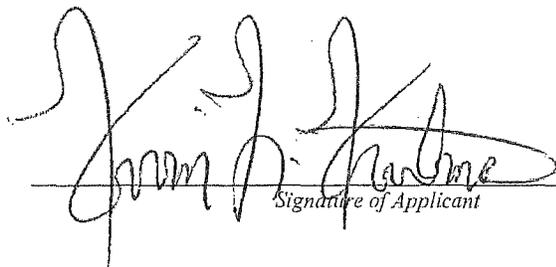
Serve on National Fire Protection Association (NFPA) committees as a member on their Combustible Metals Standard and Hazardous Material Code. I also Chair NFPA's correlating committee which oversees all of NFPA's dust standards. Member of International Titanium Association's safety committee.

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

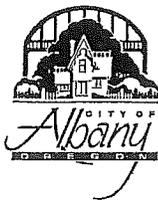
I have over 36 years experience in both local and national fire service, which provides me a unique experience of challenges the City faces with regards to public safety, including 26 years as Chief Officer, and extensive budgeting and performance measurement background.

- What contributions do you hope to make?

I have experience with the challenges facing emergency services on a local, state and national level. I also have extensive experience with the major industries in our community. I believe my experience can help in providing feedback and information for policy decision makers.


Signature of Applicant

12/10/15
Date



BOARD, COMMISSION, AND COMMITTEE APPLICATION

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Albany, OR 97321-0144
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(541) 917-7500

Board, Commission, and/or Committee Preference:

(list all for which you are applying)

Senior Center Endowment

Name: JoAnn Miller

Preferred First Name: JoAnn

Residential Information:	
Home Address:	Phone:
<u>Albany, OR 97322</u>	_____
E-mail: _____	Cellular: _____
	Fax: _____
	(Optional)

Employment Information:	
Employer's Name: _____	
Work Address:	Phone: _____
<u>Corvallis, OR 97330</u>	Cellular: _____
E-mail: _____	Fax: _____
	(Optional)

Please provide information as requested below to describe your qualifications to serve on this City of Albany Board, Commission, or Committee. Feel free to provide additional information that you may wish to share with the City.

- List current or most recent occupation, business, trade, or profession: _____

Director of Community Health Promotion - 2008 to present

Director of Benton County Commission on Children and Families - 1995 to 2008

Resume is attached

For City use only: Ward: I II III or Lives Outside City Limits (Circle One)

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Yes No If yes, how? _____



BOARD, COMMISSION, AND COMMITTEE APPLICATION FORM

Page 2

- List community/civic activities. Indicate activities in which you are or have been active:

I have been active in Project Homeless Connect, Linn County Extension Association Board Member, Linn County Oral Health Coalition, Linn County Childhood Obesity Prevention Committee.

- Indicate why you are interested in serving on this board, commission, or committee and what other qualifications apply to this position.

I am interested in serving on either of these boards to get involved in my community. I am committed to working to improve the overall health of Albany by utilizing my experience with city, state and federal government processes. I would also like to give back to my community by volunteering my time to serve in a capacity that will make a difference.

- What contributions do you hope to make?

I will contribute my knowledge and experience of working with city, state and federal government agencies through advocacy and policy development. I can also contribute to the committees because of my knowledge of the community needs. I have conducted or led focus groups, facilitated meetings and participated in local forums that have provided me with an understanding of what other residents of Albany view as a priority to improve the city. My experience with developing local budgets based on rules, regulations, administrative restrictions, and federal guidelines will be a great contribution to the budget committee. I have experience with community planning and development through research and data can contribute to the committee efforts.


Signature of Applicant

12/4/2015
Date

JoAnn R. Miller
2925 Westpark Court SE
Albany, OR 97322
541.704.0462

PROFESSIONAL OBJECTIVE

To become a leader in the education, business, community, or social environment.

EDUCATION

Masters Interdisciplinary Studies
Oregon State University
June 2003

Bachelor of Science
Southern Illinois University
Education, Training & Resource Development, 1991

Associate of Arts
Olympic College
Business, 1990

SPECIALIZED SKILLS

Substance Abuse Prevention Specialist
Substance Abuse Prevention Trainer
Public Health Community Development Trainer
Interdisciplinary Education Facilitator

LEADERSHIP TRAINING

Cultural Competency Trainer –March 2006, Warm Springs
Oregon Pacific Program – Catalytic Leadership and Development for Upper Level Managers
October 2004, Warm Springs Oregon.
Safe Streets Gang Prevention - 1993– Juvenile Justice, Kitsap County

ADDITIONAL SKILLS

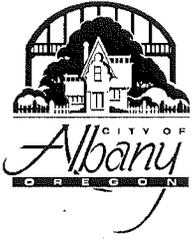
Crisis Prevention Interventionist
Diversity Counselor
EEO/Affirmative Action Trainer
Mediation Training
Juvenile Crime Prevention Training

RELEVANT EXPERIENCE

Community Benefit Director
Supervises 3.0 FTE employees and advise 36 advisory board members from five hospitals on community benefit. Plan, develop, coordinate and oversee the Samaritan Health Services community benefit initiative. Work closely with Chief Executive Officers of five hospitals to develop and implement a Community Benefit Plan. Develop and implement policies, procedures, and guidelines related to community benefit. Lead collaborative initiatives in partnership with other staff involved in community benefit activities. Train and educate executives, managers, staff and board members about community benefits. Train staff and

managers on Community Benefit Inventory for Social Accountability (CBISA) software. Monitor and maintain CBISA system. Develop annual community benefit report. Work closely with accounting department to develop annual reports for tax statements. Support efforts to identify and seek external funding to support community benefit activities. Write and edit local, state, and federal grants for various departments. Oversee and monitor grant funded projects. Conduct annual employee evaluations. Oversee department budget over \$1,000,000 per year. Samaritan Health Services, Inc., Corvallis, OR
11/2008 – present

Commission on Children and Families - Director
Supervises 3.5 employees and 26+ community volunteers. Worked closely with the Board of County Commissioners to coordinate support for the Benton County Commission on Children and Families. Develops and executes a comprehensive service delivery plan. Analyzes information, issues, and policy options. Have a thorough knowledge of local, state, and federal child and family rules, regulation and policies. Have a thorough knowledge of local and state services that support children and families. Promote community awareness of, and interest in, children and family needs, issues, strategies and goals. Facilitates consensus among local commissioners, board members, and other representatives of groups involved in planning for the delivery of services to children, families, consumers, juvenile justice professionals, child and family advocates, elected officials, business leaders, and educators who represent diverse interests in Benton County. Serve as liaison with media and other community groups; coordinates commission advocacy efforts and networks with statewide children and families representatives. Conduct research on issues, assesses needs, and analyzes data concerning children and family issues. Provide technical assistance and training to local commission members, service providers, public agencies, and the general public. Develop and write reports both state and local. Oversee county budget of \$1,000,000 year. Reviewed federal grants for Department of Justice, Department of Education, and Center for Substance Abuse Prevention. Benton County Commission on Children and Families, Corvallis, OR
10/95 – 11/2008



TO: Albany City Council

FROM: Sharon Konopa, Mayor

Sharon Konopa

DATE: January 7, 2016, for January 13, 2016, City Council Meeting

SUBJECT: Appointments to the Council Audit Committee

RELATES TO STRATEGIC PLAN THEME: • An Effective Government

Action Requested:

Council action to fill the two vacant positions for terms that expired December 31, 2015, by:

- Reappointing Sharon Konopa;
- Reappointing Sue Folden;
- Appointing a new member from the Council; and/or
- Appointing a new member from the Budget Committee.

Discussion:

Mayor Sharon Konopa's and Budget Committee member Sue Folden's terms on the Audit Committee expired December 31, 2015. A copy of the Audit Committee roster ending December 31, 2015, is attached.

Per Resolution No. 5350 (copy attached) creating the Audit Committee, the Committee shall consist of three serving City Councilors and/or Mayor and one serving member of the Budget Committee (copy attached).

Budget Impact:

None.

SK:ldh

Attachments 3

G:\Administrative Services\City Manager's Office\Boards-Commissions\2016 Recruitment\^2016 Audit Committee Appointments-msk.doc

Audit Committee

Term of Office: 2 years

Name	Business Information	Home Information	Position Information
Sue Folden* Chair Ward I Position: Budget Committee Member	Albany, OR 97322 Phone Cell Fax E-mail	Albany OR 97321 Phone Cell Fax E-mail suefolden@comcast.net	Original Appointment Date: 11/14/07 Full Consecutive Term: Fifth Current Position Term Begins: 01/08/14 Expires: 12/31/15
Sharon Konopa Position: Mayor	Phone Cell Fax E-mail	632 35th Avenue SE Albany OR 97322 Phone 541-926-6812 Cell 541-990-4098 Fax E-mail sharon.konopa@cityofalbany.net	Original Appointment Date: 10/25/06 Full Consecutive Term: Fifth Current Position Term Begins: 01/08/14 Expires: 12/31/15
Bessie Johnson Position: Councilor (Ward III-a)	Phone Cell Fax E-mail	2911 Geary Street SE Albany OR 97322 Phone 541-791-2494 Cell 541-619-0652 Fax E-mail bessie.johnson@cityofalbany.net	Original Appointment Date: 01/09/13 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/16
Ray Kopczynski Vice Chair Position: Councilor (Ward II-a)	Phone Cell Fax E-mail	1303 Tamarack Court SW Albany OR 97321 Phone 541-917-0490 Cell Fax E-mail ray.kopczynski@cityofalbany.net	Original Appointment Date: 08/24/11 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/16

*By Council action on November 14, 2007, Sue's past participation in past meetings was ratified.

= Not Public Information

- a) meets as necessary
- b) Resolution No. 5350, dated November 15, 2006, established this committee; committee to be comprised of three serving City Councilors and/or the Mayor and one serving Budget Committee Member
- c) appointed by the City Council
- d) staff liaison: N/A
- e) City's Finance Director serves as a nonvoting, ex officio member

A RESOLUTION CREATING THE ALBANY AUDIT COMMITTEE

WHEREAS, upon the recommendation of the City Manager, the Albany City Council finds it desirable to create an Audit Committee; and

WHEREAS, the purpose of the Audit Committee will be to aid City staff in the selection of highly qualified City auditors and to assure clear and complete transmittal of audit results to the City Council and the community.

NOW, THEREFORE, BE IT RESOLVED by the Albany City Council as follows:

Section 1: Establishment of City Audit Committee. The Albany Audit Committee is hereby established. The committee shall consist of four voting members, three of whom shall, at the time of their appointment, be serving City Councilors and/or the Mayor, and the fourth shall, at the time of appointment, be a currently serving member of the Albany Budget Committee. The Finance Director of the City of Albany shall serve as a nonvoting, ex officio member of the Committee.

Section 2: Term – Vacancies. The voting members of the Committee shall be appointed for a two- (2) year term. Terms of appointment shall be staggered so that two (2) positions will become vacant each year. The initial appointees to the Audit Committee are Councilor Jeff Christman, whose term shall expire December 31, 2008, Councilor Sharon Konopa, whose term shall expire December 31, 2007, Councilor Ralph Reid, whose term shall expire December 31, 2008, and (Budget Committee member), whose initial term shall expire December 31, 2007. Appointees serve at the pleasure of the Council and may be replaced with or without cause. Vacancies during any term shall be filled by vote of the Council.

Section 3: Quorum and Rules. Three (3) voting members of the Committee shall constitute a quorum. The vote of three (3) members of the Committee shall be required to take any action. Notwithstanding the foregoing, the Chair of the Audit Committee, or in the Chair's absence the Vice Chair, may establish the date and time of the next Committee meeting.

Section 4: Responsibilities. The responsibilities of the Audit Committee shall be as follows:

1. Review with the Finance Department and/or staff those firms or individuals proposed for appointment as City auditors in succeeding years.
2. Recommend to the City Council of the appointment of those certified public accountants whom the Committee deems best suited to perform the annual City audit.
3. Discuss, as necessary, with the City auditors, any additional or particular areas of inquiry or audit focus which they deem desirable or appropriate.
4. Receive, consider, and transmit to the Albany City Council the results of the annual City audit.

Section 5: Reports. The Audit Committee shall periodically submit copies of the minutes of its meetings to the City Council. Reports or recommendations of the Committee shall be considered advisory in nature and shall not be binding on the Mayor or City Council.

DATED AND EFFECTIVE THIS 15TH DAY OF NOVEMBER 2006.


Mayer

ATTEST:


City Clerk

Albany Budget Committee (lay members)

ALBANY BUDGET COMMITTEE = Lay members plus City Council members

Term of Office: 3 years

Name	Business Information	Home Information	Position Information
Jeff Christman Vice Chair  Ward III Appointed By: Ward III-b	Albany OR 97321 Phone 541-928-0528 Cell 541-990-7447 Fax E-mail jblle75@msn.com	Albany OR 97322 Phone 541-928-0528 Cell 541-990-7447 Fax E-mail jblle75@msn.com	Original Appointment Date: 01/09/13 Full Consecutive Term: First Current Position Term Begins: 01/09/13 Expires: 12/31/15
Colleen Keller Secretary  Ward III Appointed By: Ward III-a	Albany OR 97321 Phone 541-926-4856 Cell Fax E-mail sueccd@aol.com	3201 18th Avenue SE Albany OR 97322 Phone 541-926-4856 Cell Fax E-mail sueccd@aol.com	Original Appointment Date: 01/10/07 Full Consecutive Term: Third Current Position Term Begins: 01/09/13 Expires: 12/31/15
Dick Conolly Ward I Appointed By: Mayor	Albany OR 97321 Phone 541-791-7173 Cell Fax E-mail rconolly@aol.com	1221 Washington Street SW Albany OR 97321 Phone 541-791-7173 Cell Fax E-mail rconolly@aol.com	Original Appointment Date: 01/12/11 Full Consecutive Term: Second Current Position Term Begins: 01/08/14 Expires: 12/31/16
Scott Pierson Ward I Appointed By: Ward II-b	Albany, OR 97321 Wood Castle Mfg. 29855 Hwy 34 Phone 541-757-6404 Cell Fax 541-757-6362 E-mail scott.pierson@woodcastle.com	Albany OR 97321 1908 Cascade Heights NW Phone 541-812-2222 Cell Fax E-mail dscott.pierson@gmail.com	Original Appointment Date: 01/08/2014 Full Consecutive Term: First Current Position Term Begins: 01/08/2014 Expires: 12/31/16
Michael Thomson Ward I Appointed By: Ward I-a	Albany OR 97321 Phone 541-971-0456 Cell Fax E-mail goeeyduck94@yahoo.com	Albany OR 97321 1291 Elm Street SW Phone 541-971-0456 Cell Fax E-mail goeeyduck94@yahoo.com	Original Appointment Date: 01/12/11 Full Consecutive Term: Second Current Position Term Begins: 01/08/14 Expires: 12/31/16
Sue Folden Chair Ward I Appointed By: Ward I-b	Albany, OR 97322 Phone 541-967-4292 Cell Fax E-mail suefolden@comcast.net	Albany OR 97321 Phone 541-967-4292 Cell Fax E-mail suefolden@comcast.net	Original Appointment Date: 01/08/03 Full Consecutive Term: Fourth Current Position Term Begins: 01/14/15 Expires: 12/31/17
Will Summers Ward I Appointed By: Ward II-a	Albany, OR 97321 Phone 541-967-4292 Cell Fax E-mail	Albany OR 97321 Phone 541-967-4292 Cell Fax E-mail	Original Appointment Date: 01/11/12 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/17

 Scheduled for reappointment on 1-13-2016

= Not public information

- a) meets when necessary
- b) ORS 294.336 provides for the Budget Committee
- c) Resolution No. 3539 provides for appointment procedures. [Must be residents of the city of Albany; Councilors' appointments not required to be from their respective wards.]
- d) Staff liaison: City Manager Wes Hare, 541-917-7505; or Finance Director Stewart Taylor, 541-917-7521
- e) Serves as the ARA Budget Committee per Resolution No. 2002-3



TO: Albany City Council
FROM: Sharon Konopa, Mayor *Sharon Konopa*
DATE: January 7, 2016, for January 13, 2016, City Council Meeting
SUBJECT: Hospital Facility Authority (HFA)

RELATES TO STRATEGIC PLAN THEME: • An Effective Government

Action Requested:

Council action to fill the two vacant positions for terms that expired December 31, 2015, by:

- Appointing a new City Council member; and
- Appointing a new Budget Committee member

Discussion:

City Councilor Bill Coburn's and Budget Committee member Colleen Keller's terms on the HFA expired December 31, 2015. Because they have served two consecutive terms, **they are not eligible for reappointment to the HFA.**

ORS 441.535 requires that at least one director be a member of the governing body. By Council policy, the other four members of the HFA are from the Albany Budget Committee. No member can serve more than two consecutive terms.

A copy of the HFA roster ending December 31, 2015, is attached as well as a copy of the Budget Committee roster.

Budget Impact:

None.

SK:ldh

Attachments 2

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Hospital Facility Authority of the City of Albany

Board of Directors

Term of Office: 3 years

Name	Business Information	Home Information	Position Information	
Bill Coburn (Council representative) <i>Ward II</i>	EC Company 32758 Old Highway 34 P.O. Box 925 Albany OR 97321 E-mail	Phone 541-926-4266 Cell Fax 541-926-4268 E-mail	6317 Chapman Court SW Albany OR 97321 Phone 541-928-0649 Cell 541-936-0396 Fax E-mail bcoburncc@gmail.com	Original Appointment Date: 01/27/10 Full Consecutive Term: Second Current Position Term Begins: 01/23/13 Expires: 12/31/15
Colleen Keller <i>Ward III</i>	Phone Cell Fax E-mail	3201 18th Avenue SE Albany OR 97322 Phone 541-926-4856 Cell Fax E-mail sneccd@aol.com	Original Appointment Date: 02/11/09 Full Consecutive Term: Second Current Position Term Begins: 01/23/13 Expires: 12/31/15	
Sue Folden <i>Ward III</i>	Phone Cell Fax E-mail	Albany, OR 97322 Phone Cell Fax E-mail suefolden@comcast.net	Original Appointment Date: 01/23/13 Full Consecutive Term: First Current Position Term Begins: 01/22/14 Expires: 12/31/16	
Dick Conolly <i>Ward I</i>	Phone Cell Fax E-mail	1221 Washington Street SW Albany OR 97321 Phone 541-791-7173 Cell 541-981-9903 Fax E-mail rconolly@aol.com	Original Appointment Date: 01/26/11 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/17	
Michael Thomson <i>Ward I</i>	Phone Cell Fax E-mail	1291 Elm Street SW Albany OR 97321 Phone Cell 541-743-5508 Fax E-mail goeeyduck94@yahoo.com	Original Appointment Date: 01/25/12 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/17	

= Not public information

- a) meets when necessary
- b) created by Albany City Council by Resolution #1900, dated 8/10/77 pursuant to ORS 441.525 to 441.595, Financing of Hospital Construction. Resolution #3143, dated 2/26/92, amended Resolution #1900 so that for terms expiring 7/1/92, 7/1/93, and 7/1/94, new appointments or reappointments shall commence on the date of appointment and will expire December 31 following the three-year appointment. Thereafter, terms shall commence on January 1 or date of appointment and shall expire on December 31 following the three-year appointment. Resolution #5016, dated 8/9/04, clearly defined term limits of new appointments.
- c) No member shall serve more than two consecutive terms per Resolution No. 1900.
- d) ORS 441.535 requires that at least one director shall be a member of the governing body (City of Albany)
- e) by Council direction (July 12, 2004), four directors shall be lay members of the Albany Budget Committee
- f) appointed by the City Council
- g) staff liaison: Finance Director Stewart Taylor, 541-917-7521

Albany Budget Committee (lay members)

ALBANY BUDGET COMMITTEE = Lay members plus City Council members

Term of Office: 3 years

Name	Business Information	Home Information	Position Information
Jeff Christman Vice Chair  Ward III Appointed By: Ward III-b	Phone 541-928-0528 Cell 541-990-7447 Fax Albany OR 97321 E-mail jbllic75@msn.com	Phone 541-928-0528 Cell 541-990-7447 Fax Albany OR 97321 E-mail jbllic75@msn.com	Original Appointment Date: 01/09/13 Full Consecutive Term: First Current Position Term Begins: 01/09/13 Expires: 12/31/15
Colleen Keller Secretary  Ward III Appointed By: Ward III-a	Phone 541-757-6404 Cell 541-757-6362 Fax Albany, OR 97321 E-mail scott.pierson@woodcastle.com	3201 18th Avenue SE Albany OR 97322 Phone 541-926-4856 Cell Fax E-mail sueccd@aol.com	Original Appointment Date: 01/10/07 Full Consecutive Term: Third Current Position Term Begins: 01/09/13 Expires: 12/31/15
Dick Conolly Ward I Appointed By: Mayor	Phone 541-757-6404 Cell Fax Albany OR 97321 E-mail scott.pierson@woodcastle.com	1221 Washington Street SW Albany OR 97321 Phone 541-791-7173 Cell Fax E-mail rconolly@aol.com	Original Appointment Date: 01/12/11 Full Consecutive Term: Second Current Position Term Begins: 01/08/14 Expires: 12/31/16
Scott Pierson Ward I Appointed By: Ward II-b	Wood Castle Mfg. 29855 Hwy 34 Albany, OR 97321 Phone 541-757-6404 Cell Fax 541-757-6362 E-mail scott.pierson@woodcastle.com	1908 Cascade Heights NW Albany OR 97321 Phone 541-812-2222 Cell Fax E-mail dscott.pierson@gmail.com	Original Appointment Date: 01/08/2014 Full Consecutive Term: First Current Position Term Begins: 01/08/2014 Expires: 12/31/16
Michael Thomson Ward I Appointed By: Ward I-a	Phone 541-967-4292 Cell Fax Albany, OR 97322 E-mail suefolden@comcast.net	1291 Elm Street SW Albany OR 97321 Phone 541-971-0456 Cell Fax E-mail goeeyduck94@yahoo.com	Original Appointment Date: 01/12/11 Full Consecutive Term: Second Current Position Term Begins: 01/08/14 Expires: 12/31/16
Sue Folden Chair Ward I Appointed By: Ward I-b	Phone 541-967-4292 Cell Fax Albany, OR 97322 E-mail suefolden@comcast.net	Albany OR 97321 Phone 541-967-4292 Cell Fax E-mail suefolden@comcast.net	Original Appointment Date: 01/08/03 Full Consecutive Term: Fourth Current Position Term Begins: 01/14/15 Expires: 12/31/17
Will Summers Ward I Appointed By: Ward II-a	Phone 541-967-4292 Cell Fax Albany, OR 97321 E-mail suefolden@comcast.net	Albany OR 97321 Phone 541-967-4292 Cell Fax E-mail suefolden@comcast.net	Original Appointment Date: 01/11/12 Full Consecutive Term: Second Current Position Term Begins: 01/14/15 Expires: 12/31/17

 Scheduled for reappointment on 1-13-2016

= Not public information

a) meets when necessary
 b) ORS 294.336 provides for the Budget Committee
 c) Resolution No. 3539 provides for appointment procedures. [Must be residents of the city of Albany; Councilors' appointments not required to be from their respective wards.]
 d) Staff liaison: City Manager Wes Hare, 541-917-7505; or Finance Director Stewart Taylor, 541-917-7521
 e) Serves as the ARA Budget Committee per Resolution No. 2002-3



TO: Albany City Council
FROM: Sharon Konopa, Mayor *Sharon (ed)*
DATE: January 7, 2016, for January 13, 2016, City Council Meeting
SUBJECT: City Council Representatives

RELATES TO STRATEGIC PLAN THEME: • An Effective Government

Action Requested:

Council approval of the following reappointments:

Albany Area Metropolitan Planning Organization (AAMPO) Policy Board

Floyd Collins [term expires: 12-31-2016] (Mayor's reappointment)
Ray Kopczynski (alternate) [term expires: 12-31-2016] (Mayor's reappointment)

Cascades West Area Commission on Transportation (CWACT)

Dick Olsen [term expires: 12-31-2017] (Mayor's reappointment)
Ron Irish (alternate) [term expires: 12-31-2017] (Mayor's reappointment)

Discussion:

None.

Budget Impact:

None.

SK:ldh

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TO: Albany City Council
VIA: Wes Hare, City Manager
FROM: Chris Bailey, Public Works Operations Director *CB*
DATE: January 4, 2016, for the January 13, 2016, City Council Meeting
SUBJECT: National Guard Armory Property

RELATES TO STRATEGIC PLAN THEME: ● Great Neighborhoods

RELATES TO: ● An Effective Government

Action Requested:

Staff requests the City Council discuss the sale of the City property currently leased to the National Guard Armory with the Airport Advisory Commission.

Discussion:

The City Council recently voted to sell to Linn County a City parcel currently leased to the National Guard Armory. This parcel was purchased by the City in the 1929 as part of a larger property used as the original Albany Municipal Airport. The Airport Advisory Commission discussed the sale of this property at their November meeting. Because it was originally acquired by the City for use as part of the municipal airport, the Commission recommends any funds received from the sale of this parcel be directed to the Airport Capital Projects budget where they would be used for future capital improvements.

The Airport Advisory Commission has provided the attached letter further explaining their concerns, and will be present at the Council meeting for additional discussion as necessary.

Budget Impact:

Any funds directed to the Airport Capital Projects budget have the potential to leverage Federal Aviation Administration funds by providing the required ten percent match for qualifying projects.

JG:CB:kw
Attachment

Albany City Council,

The Airport Advisory Commission was established in 2010 to advise the Albany City Council on the efficient management, operation, and development of the airport and its related operations, facilities, and assets. One of the main reasons for the formation of the Airport Commission was the dire economic condition of the Airport. The Airport had a capital reserve of \$200,000.00 and was losing approx. \$50,000.00 per yr. With the support and commitment of the City Council, together the Airport Commission and City Staff were able to stem the economic decline. Today the Airport, while not very profitable, does not expend any City general funds.

The past economic conditions at the Airport were ameliorated by the sale of property. At one point the Albany Airport consisted of over 200 acres, it is now somewhere around 80 acres. This contraction of usable ground at the Airport has come in several ways, most recently through the gift and sale of property. Before the formation of the Airport Commission, the City traded Airport property to Linn County for the construction of the Fairgrounds. 34.91 acres valued today over \$9 million. The City sold two parcels for the construction of two hotels. 4.36 acres valued today at \$1.7 million. Lastly, the City sold 1.08 acres, valued today at \$455,570, to a developer for the construction of a restaurant. In total, over 40 acres valued over \$11 million. As these sales occurred before the formation of the Commission, the amount the Airport received from these sales is uncertain, but budget evidence would indicate around \$250,000.

Now, the City would like to sell the National Guard Armory property which is land purchased as Airport property. The Airport has many needs and few assets. We have just about finished our new Airport Master Plan which will allow the Airport to make many improvements. New taxiways, an automated weather station, and an obstruction survey needed to increase the inclement weather usability of the Airport, are only a few of the projects needed to improve the Airport. We are all committed to making the Albany Municipal Airport a valuable and unique asset to the City and Citizens of Albany. The City Staff and Commission members have worked hard to bring that value to the City and not be a financial burden, but the continued reduction of the Airport asset base increases the difficulty of that mission.

The Airport Commission understands that the City has the right and responsibility to care for all the assets of the City, the Airport included. That said, the Commission feels that the if Airport property is sold, the value received should reflect the real market value and that the income received should be used for the improvement of the Airport. An airport is a valuable asset to any city including Albany. That asset needs to be protected and nurtured.

The Albany Airport Commission